



Kearns On-Street Parking Study

November 2022



Prepared for:



*Wasatch Front Regional Council
41 North Rio Grande Street
Salt Lake City, Utah 84101*



G R E A T E R S A L T L A K E
**Municipal Services
District**

*Greater Salt Lake Municipal Services District
2001 South State
#N 3-600
Salt Lake City, Utah 84190*

Prepared by:

Kimley»Horn



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Final Report

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Prepared for:



WASATCH FRONT REGIONAL COUNCIL

*Wasatch Front Regional Council
41 North Rio Grande Street
Salt Lake City, Utah 84101*



GREATER SALT LAKE
**Municipal Services
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November 2022
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Executive Summary

The purpose of the Kearns On-Street Parking Study Final Report is to provide an overview of the technical analysis, public engagement efforts, and recommendations that were developed as part of this study. The study area for

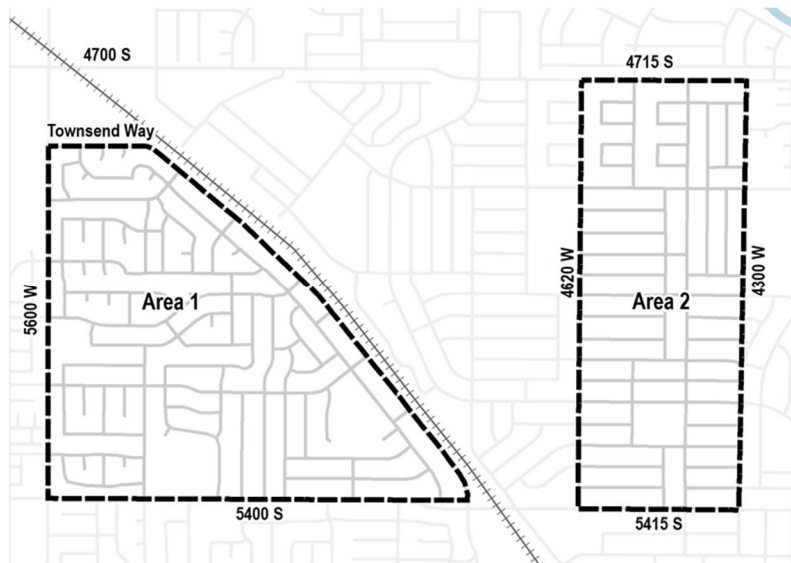


Figure E1 – Study Areas

the Kearns On-Street Parking Study consists of two residential neighborhoods as seen in the Figure E1. The intent of selecting these two neighborhoods was to capture the diversity found in the Metro Township.

The existing land use pattern in the study areas consists mainly of single family residential making up approximately 91% of the existing land use within the study areas. In general, the land use pattern in and around the study areas is planned to remain the same as the existing land uses

The technical analysis included a literature review of previous plans and studies, a safety review of historical crash data, and a detailed parking supply and demand analysis. The following are the key observations and takeaways identified from the safety review conducted for the study areas:

- The study areas have a higher percentage of fatal crashes and crashes with parked vehicles when compared to Salt Lake County crash data for similar roadways.
- 14% of all crashes within the study areas involved parked vehicles.
- Pedestrian related crashes could be considered slightly higher than normal within the study areas.
- The majority of crashes occurred along the exterior roadways where on-street parking is minimal or not permitted. These are also the higher volume and higher speed roadways within the study areas.
- The majority of pedestrian related crashes occurred along 5400 South in Area #1 which has no on-street parking.

Data was collected within the identified study areas for the project. Collection included two components, inventory data collection and parking occupancy data collection. Data was collected to observe typical weekday and weekend conditions. Weekday parking occupancy was collected on December 07, 2021, and December 14th, 2021. Weekend occupancy was collected on December 11th, 2021. All collection captured various times of day, collecting for early morning, afternoon, evening, and late evening. The following are the key observations and takeaways identified from the parking supply and demand analysis conducted for the study areas:

- Parking trends for weekday and weekend peak occupancy during the various time periods are similar with the majority of roadway segments being underutilized, with peak occupancy reaching 28%. Peak occupancy on both weekday and weekend collection was observed at 5:00 PM to 7:00 PM.



- Observed parking conditions during the weekend peak occupancy had more segments above 50% occupancy than observed during peak occupancy on weekday collection.
- Common higher occupancies are observed at the following locations:
 - Study Area #1:
 - 5240 West from 5200 South to 5400 South
 - Trident Drive from 5600 West to 5420 West
 - Study Area #2:
 - 5055 South from 4620 West to 4300 West
 - 4380 West from 4745 South to 4835 South
- Vehicles parked on the street are commonly intersecting with the sidewalk, blocking active transportation and ADA uses. Of the observed periods, 25% to 35% of vehicles parked on the street were interfering with the sidewalk.
- Corner parking is most common in Study Area #1, specifically throughout the northern half of the study areas. These roadways are typically more winding than Study Area #2, making corner parking a larger impact on sight distance.
- Many vehicles were observed to park in yards rather than utilizing on-street parking. During all of the observed times, over 2,000 vehicles were parked in yards. If vehicles parked in yards were relocated to street parking, on-street parking occupancy would increase but still be underutilized, with a peak occupancy of 38% and 40% for weekday and weekend collection, respectively.

Public engagement efforts were comprised of two separate phases. The first phase was a stakeholder meeting with key representatives from various entities or agencies. The second public engagement phase involved community outreach to communicate the results of the technical analysis and allow the community the opportunity to provide input and feedback. The online survey received 229 comments across the six survey questions from 108 respondents. In addition to the survey responses, an additional 114 comments were left on the Facebook posts advertising the survey. Based on the survey responses and comments, the following key observations and takeaways were found and were considered when developing recommendations related to on-street parking changes in the Kearns Metro Township.

- 62% of respondents do not park in their yards.
- 75% of respondents were in favor of eliminating on-street parking that impedes views near intersections.
- Respondents left 32 comments expressing interest in stronger enforcement of existing parking rules, 13 of which were requests for stronger enforcement of winter parking rules. This included better signage to advertise the parking rules.
- Respondents left 86 comments across the survey expressing safety concerns related to on-street parking and neighborhood intersections in the Kearns Metro Township.
- 60% of respondents are in favor of eliminating on-street parking on one side of the roadway for narrow roads.
- Multiple comments expressed the need to limit the types of vehicles that can park on the street including RVs, large trucks with trailers, semi-trucks, and inoperable vehicles.



Based on the technical analysis and public engagement performed as part of the Kearns On-Street Parking Study, a list of recommendations was developed to be considered for implementation in the Metro Township. Included with each recommendation is a description of the recommendations, the purpose behind the recommendation, items considered the in the development of the recommendation, the specifics of the recommendation, performance measures to track the effectiveness of the recommendation, cost of implementation, community support, and potential barriers to implementation. The proposed recommendations are as follows:

- No parking clear zones at residential intersection
- On-street parking allowances based on street widths
- Increase signage related to winter parking regulations
- Eliminate rolled curbs
- More accessible parking regulations
- Increase parking enforcement efforts
- Parking code changes
- Monitoring of parking performance measures



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1. Introduction

The purpose of the Kearns On-Street Parking Study Final Report is to provide an overview of the technical analysis, public engagement efforts, and recommendations that were developed as part of this study. Included in the Final Report is an overview of the study areas, literature review of previous plans and studies, land use analysis, review of historical crash data, parking supply and demand analysis, public engagement efforts, public comments based on an online survey, and recommendations.

The study area for the Kearns On-Street Parking Study consists of two residential neighborhoods as seen in Figure 1. These two neighborhoods were intentionally selected because together they are representative of the diverse housing types, right-of-way (ROW) widths, and street patterns (grid system and cul-de-sac) found throughout the rest of Kearns Metro Township (Metro Township). The intent of selecting these two neighborhoods was to capture the diversity found in the Metro Township. The Kearns General Plan has also identified these neighborhoods to connect to the community land use centers by way of non-motorized transportation, which could be hindered by excessive or unsafe on-street parking.

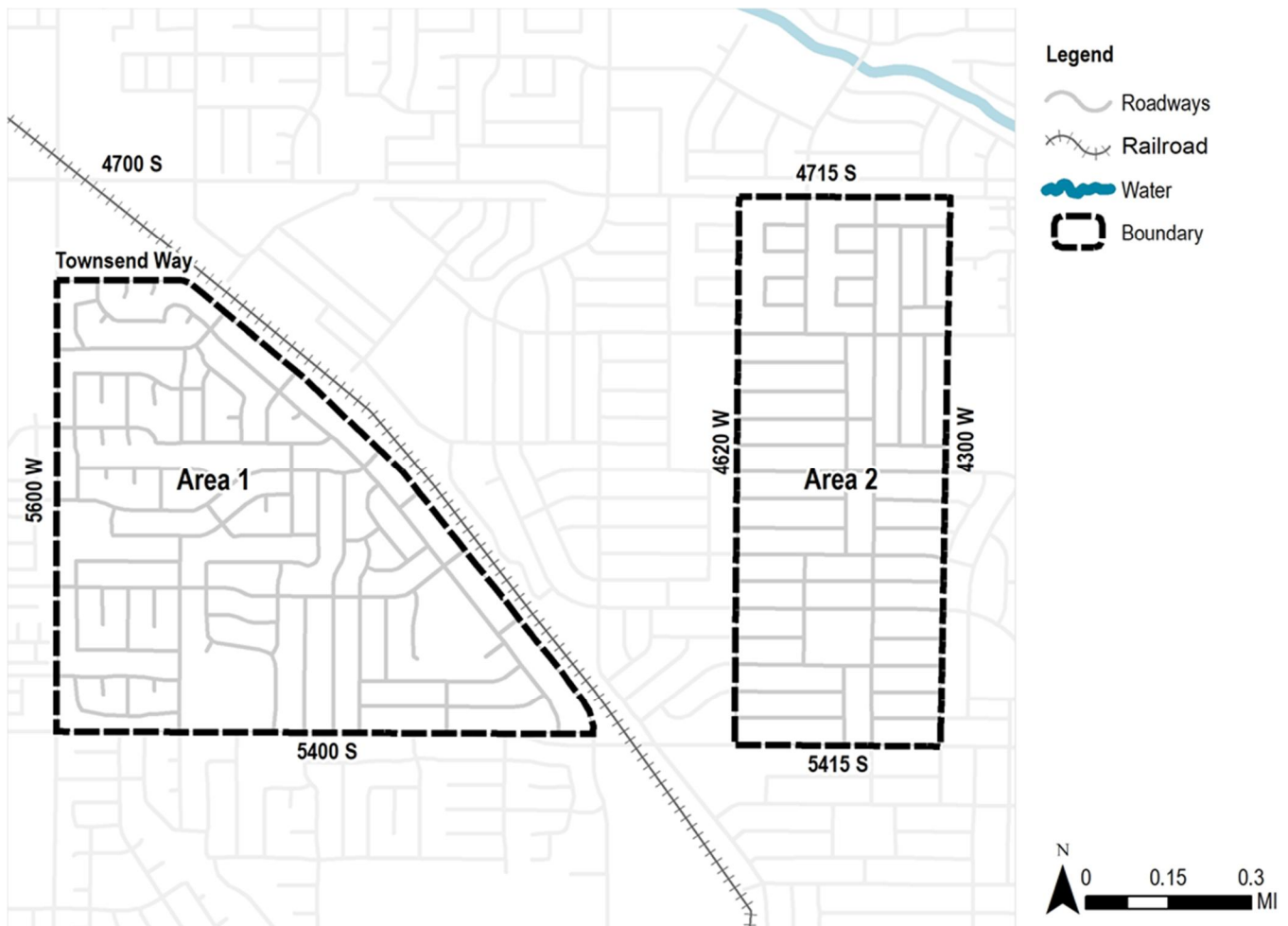


Figure 1 – Study Areas



2. Previous Plans and Studies

This section of the report provides a brief introduction along with key takeaways related to this project from the following previous plans, studies, and other sources:

- Kearns General Plan (2020)
- Kearns Master Transportation Plan (2020)
- Greater Salt Lake Municipal Services District (MSD) Citizen Problem Reporter
- Utah Parking Modernization Guide – Phase 1 (2021)
- Kearns & Magna Active Transportation Plan (2022 Draft)
- Bird E-Scooter Program and Data Dashboard
- Kearns Town Center Master Plan (2019)
- Winter Parking and Snow Removal Guide (2020/2021)

Along with providing a brief introduction and key takeaways for each of the previous plans and studies reviewed, the summaries also identified items related to proposed roadway, transit, active transportation, and micro-mobility concepts or projects. On-street parking is one part of a complete transportation system and is directly connected to roadway projects, transit improvement, and accessibility to active transportation along with access to micro-mobility options.

2.1. Kearns General Plan (2020)



The Kearns General Plan, adopted in 2020, addresses challenges and future goals for the Metro Township by assessing existing conditions. The plan aims to provide a framework for future planning that promotes growth and development aimed to enhance air and water quality, provide access to a healthy and safe environment, celebrate Kearns' history, promote education and economic opportunity, and redevelop to enhance quality of life for residents. The General Plan identifies existing and planned character areas, identifying challenges and visions for the key areas of development.

The General Plan process prompted the development of this parking study. While the General Plan focuses on the community's land use vision, it also discussed transportation opportunities and challenges, including parking. The challenge of connected transportation, especially for pedestrians and bicyclists, was a key discussion within assessed character areas. Parking needs and access was identified as an additional challenge throughout

some of the character areas, including the Warehouse District and the Commercial Redevelopment Opportunity area.

Transportation goals for the Metro Township were developed within the plan, emphasizing the need for a safe and connected transportation network, increased access to the public transit system, increased walking and biking infrastructure, accessibility, safety, and the need for integrated transportation planning and design decisions.

A few key takeaways from the Kearns General Plan related to transportation and the study areas are summarized in the following list:



- Roadway Expansion Projects
 - 4700 South
- Major Transit Investment Corridor
 - 5400 South
- Transit Line Projects
 - 5600 West
 - 5400 South
- Active Transportation Projects
 - 4700 South
 - 4980 West

2.2. Kearns Master Transportation Plan (2020)

The Kearns Master Transportation Plan analyzed current and future conditions for the Metro Township, aiming to improve safety and accommodation for all modes of transportation. The master plan noted that Kearns is not experiencing significant changes in population or housing, but traffic is growing exponentially. The transportation system needs to be improved to accommodate the increase in motorists as well as improve active transportation services.

The master plan identified areas of challenge within the transportation network. Northwest Avenue, from Salem Avenue and 4280 West, has a high speed limit, resulting in unsafe conditions for the surrounding neighborhoods and development, requiring traffic calming measures. Thomas Jefferson Junior High school needs safe active transportation facilities, especially sidewalks connecting the school to the surrounding area. Kearns High School also is missing connectivity for pedestrians and bicyclists to the surrounding area. The Plan recommends that active transportation design concepts be evaluated along the Union Pacific Line to increase safety for students.



The key takeaways from the Kearns Master Transportation Plan related to the study areas include the following:

- 10-Year Sidewalk Maintenance Plan
- Additional Bike Lanes, Buffered Bike Lanes, and Neighborhood Byways
 - Heath Avenue
 - 4820 West
 - Pieper Boulevard/4980 West
 - 5015 South
 - 4420 West
- Future Transit Facilities
 - 5600 West Express Service
 - 5400 South
 - Pieper Boulevard/4980 West
 - 4420 West



- 4700 South Roadway Widening Project
- Pieper Boulevard/4980 West Traffic Calming Project

2.3. MSD Citizen Problem Reporter

The problem reporter allows residents to report a variety of issues, including animals, building department, code enforcement, garbage collection, grading and storm water, health department, illegal parking, land use and zoning, parks and recreation, and public works.

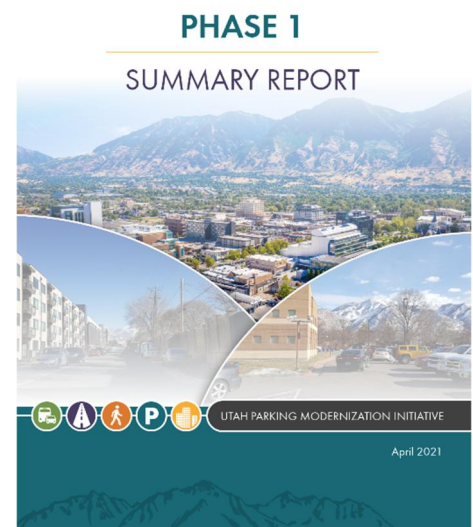
At the time of this report, there were no existing problem reports within the project study areas.

2.4. Utah Parking Modernization Guide – Phase 1 (2021)

The Utah Parking Modernization Guide was created to integrate parking and identify strategies to modernize the approach to parking management for cities within the Wasatch Front. The partnership study assessed the cities of Ogden and South Salt Lake to examine local parking practices, travel patterns, and to develop parking management strategies for cities within the region.

The parking studies performed for Ogden and South Salt Lake found that existing parking supply is underutilized overall, indicating parking codes for land use requires developers to build an excess of parking. From conclusions drawn from the parking study, strategies were developed to aid cities within the Wasatch Front in modernizing their parking management approach.

The guide promotes assessment of parking requirements, planning for mobility hubs, implementation of parking enforcement and time limit restrictions, transit station parking planning, curb lane management, data-based decision making, implementation of flexible parking code, repurposing of underutilized parking, implementation of parking permit programs and paid parking programs, and plan for transportation demand management and mobility.



2.5. Active Transportation Plan (2022 Draft)

KEARNS & MAGNA

Active Transportation Plan

The Kearns and Magna Active Transportation Plan is in the drafting phase, as of 2022. The Plan is establishing a vision of connecting destinations with accessible infrastructure for all ages and abilities. The Active Transportation Plan aims to encourage healthy lifestyles, increase comfort and amenities to create a sense of place, promote equity by ensuring all neighborhoods, ages, and abilities have access to active transportation opportunities, enhance connectivity to local and regional destinations, and connect to transit through establishing the first and last-mile connections from the recommended active transportation systems to the existing transit stops.

The Active Transportation Plan draft has released an existing conditions assessment for the Metro Township. The assessment of existing conditions shows that the project study areas have sidewalks on all local and major roadways. Although there are no bus stops within the study areas, the UTA Bus Route 35 and associated stops are



surrounding the study areas, along 5600 West, 4700 South, 5400 South, and 4420 West. The existing conditions assessment also summarizes the existing and planned active transportation facilities as found in previous plans.

As of September 2022, no additional information related to the Kearns and Magna Active Transportation Plan was available to the public.

2.6. Bird E-Scooter Program and Data Dashboard

The Kearns Metro Township implemented a Bird E-Scooter program in July of 2021, allowing the community to utilize micro-mobility for intra-community trips and recreation. The E-Scooter program aims to reduce carbon emissions and traffic congestion.

A variety of locations throughout Kearns may be prohibited on-street E-Scooter use due to safety and traffic conditions, including 4700 South. All roadways within the study areas either permit on-street riding or are UDOT-owned and do not have an e-scooter policy put in place. The map of permitted and prohibited on-street riding locations is shown in Figure 2. The E-Scooter program does not prohibit riding E-Scooters on sidewalks within the Metro Township.



Kearns Guide to E-Scooter Riding 2021

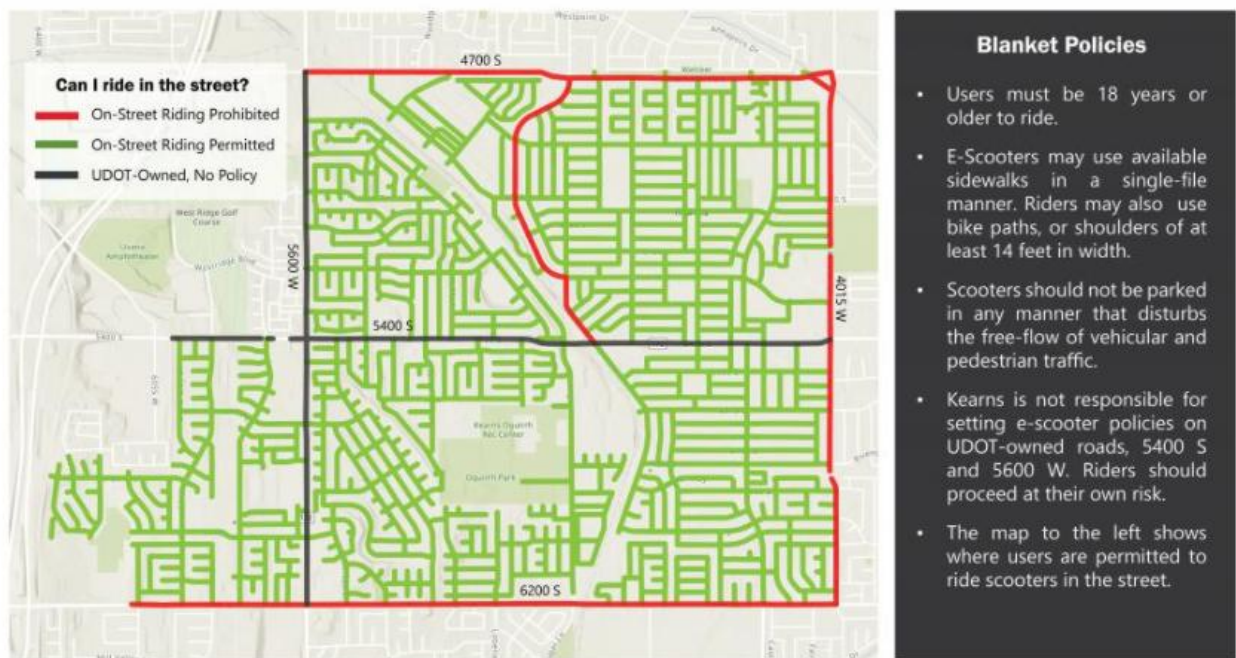


Figure 2 – Example of the Kearns E-Scooter Riding Guide



2.7. Kearns Town Center Master Plan (2019)

The Kearns Town Center Master Plan was implemented to restore the Town Center area's role as the commercial and community center of the area by improving the pedestrian environment, connectivity throughout the area, and creating opportunities for life and work in the Town Center. The Master Plan identified part of the east portion of the project study areas as falling within the 1-mile walk access to the Town Center.

The Master Plan resulted in a preferred Town Center Concept Plan. Key concepts identified within the plan include:

- Increased connectivity through commercial properties to encourage active transportation
- Investment in a transit hub
- Creation of multi-use streets
- Concentration of transit routes, including the addition of new routes
- Opportunities for development of multi-family housing, office space, and other employment types



2.8. Winter Parking and Snow Removal Guide 2020/2021

The Winter Parking Enforcement and Snow Removal Guide provides guidelines on winter parking and needed reminders for residents. The document determines that when vehicles are parked on roadways when it is currently snowing or there is snow on the roadway, snowplows cannot properly clear the roads. Per Ordinance 11.20.130, it is illegal to park on the street during the months of November through April when there is an active snowstorm or there is snow on the street.

The document also identifies first priority roadways where plowing is most commonly going to occur to ensure that driving conditions are safe as seen in Figure 3. These include various roadways within the study areas, including:

- Heath Avenue
- Mountain Men Drive
- Hoopes Street
- 4620 West
- 4865 South
- 4715 South

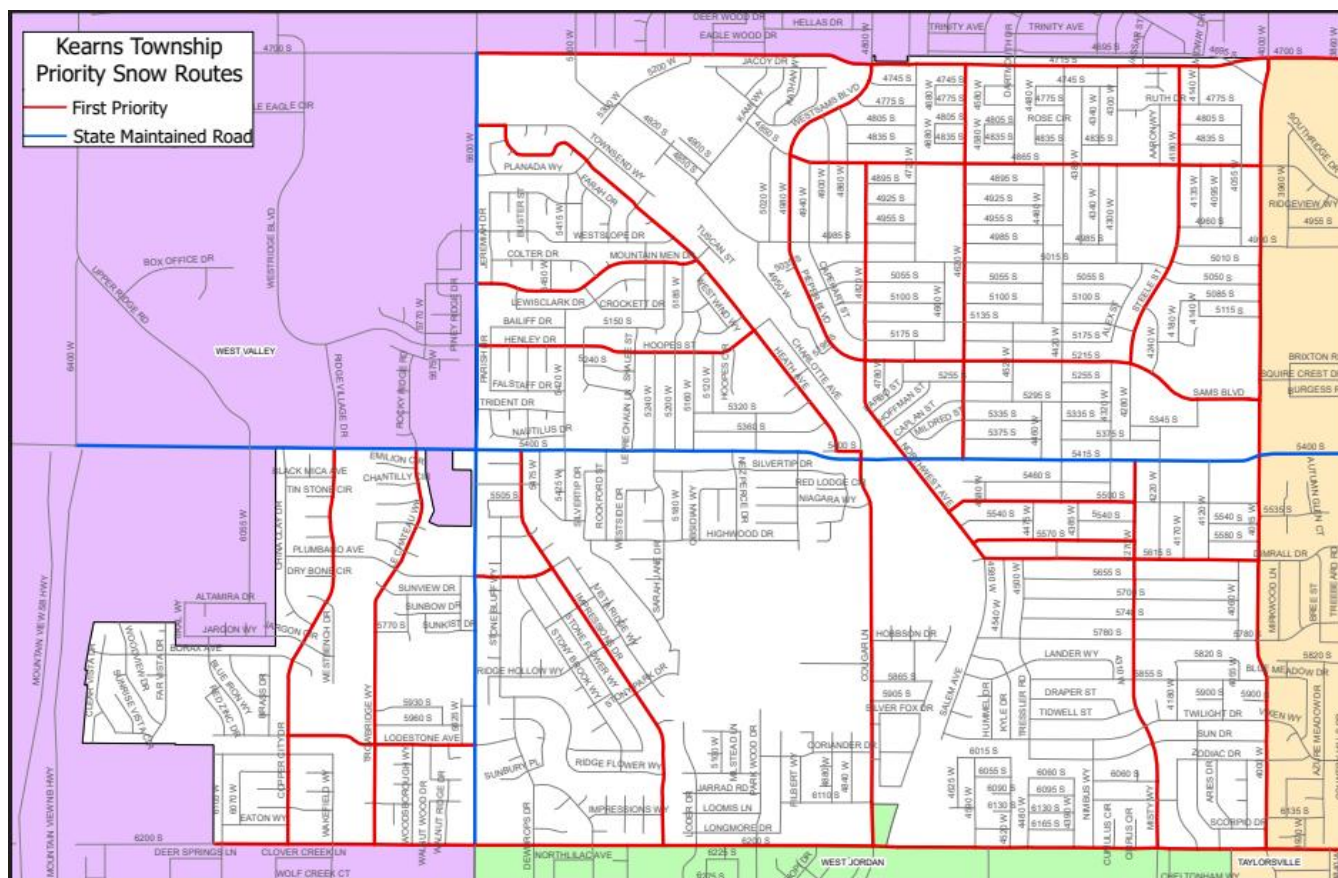


Figure 3 – Kearns Metro Township Priority Snow Routes



3. Land Use Analysis

This section of the report provides an overview of land uses in the study areas, existing land use patterns, land uses neighboring the study areas, and future land uses.

3.1. Land Use in the Kearns On-Street Parking Study Areas

Land use is a primary driver of parking patterns. To determine land use's influence on parking, there are a range of considerations: most fundamentally, who is being drawn to a given site, and how are those people accessing that site? If a person accessing a site is driving and needs to store his or her vehicle, considerations include for how long and for what purpose? Finally, what are the amount, nature, and regulations of the parking space that is available?

The study areas contain almost completely residential land uses, with a few schools, parks, and places of worship mixed in. This generally low-density single family land use pattern presents a simple parking situation: Most people parking are doing so at their homes and storing vehicles there for long amounts of time, with some light visitation. Most homes have off-street parking in driveways, and the low-density residential land use pattern does not typically lead to constraints in on-street parking space. Parking is free and lightly regulated. There are just a few destinations that attract higher visitation, such as St. Francis Xavier Catholic Church and Western Hills Elementary School, and these sites generally have off-street parking.

However, a few factors could put additional stress on the on-street parking. First, demographic information indicates that the study areas have significantly larger households and higher rates of automobile ownership than Salt Lake County as a whole. Second, the surrounding area contains a mix of uses that includes uses such as Kearns' emerging Town Center, Kearns High School, and USANA Amphitheater, which could affect on-street parking.

We address land use in on-street parking in Kearns by looking at three topics:

- The general land use pattern: Who is parking at what uses?
- Broader characteristics and considerations of land use in the broader Kearns community that could influence parking.
- Are there any changes planned or trending that may change the parking demand?

3.2. Existing Land Use Patterns

As Figure 4 shows, the existing land use pattern in the study areas is straightforward. Single family residential, at a relatively low density, makes up approximately 91% of the existing land use within the study areas. Analysis of a sample of the study areas showed a gross density of 4.6 units per acre (6.0 units per acre netting out the streets), a middle-of-the-road suburban density in the Wasatch Front.

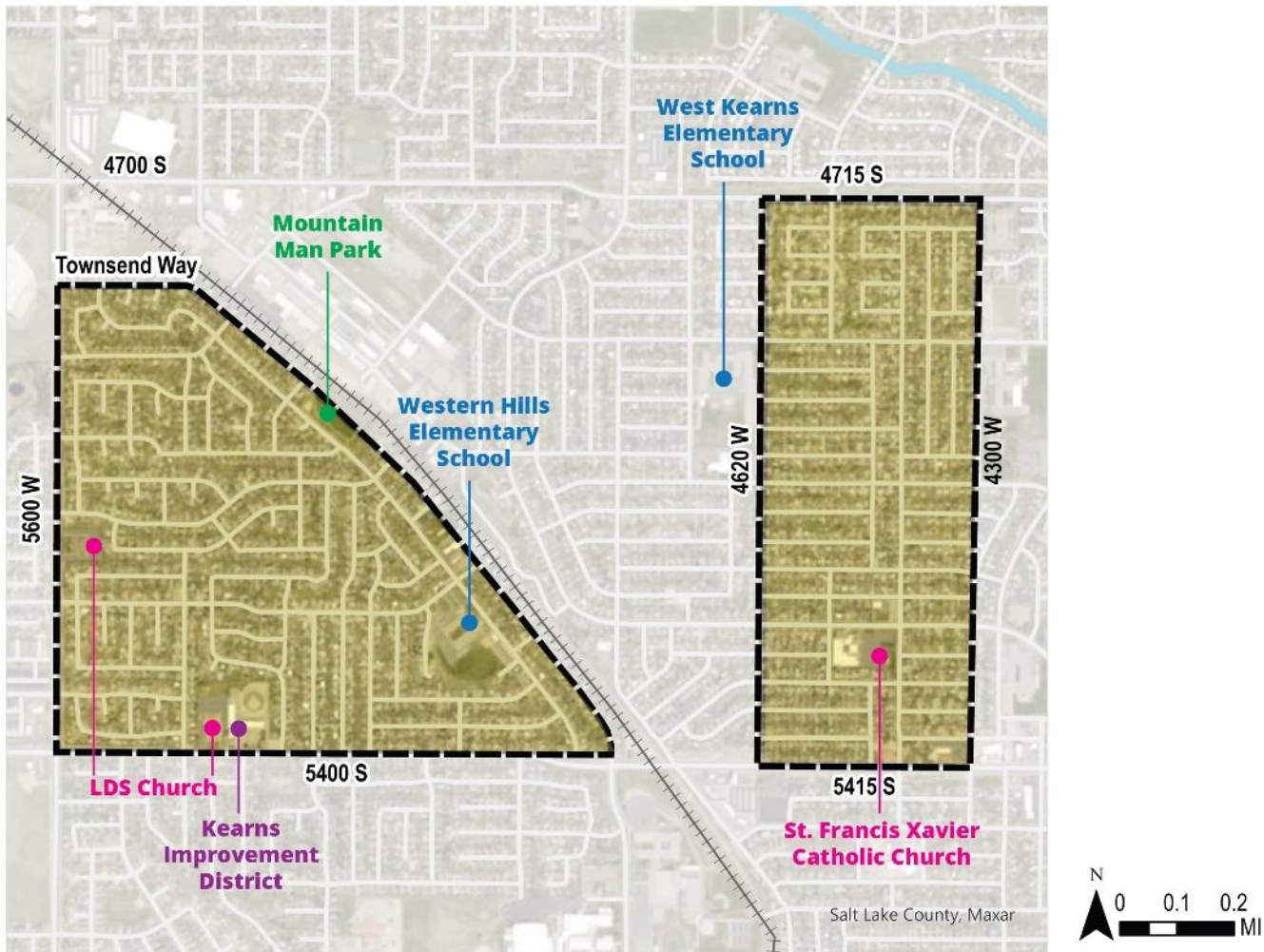


Figure 4 – Existing Land Use in the Study Areas

Physically, most of the housing stock is similar in that homes generally have driveways to accommodate off-street parking. Many, but not all, of these driveways are large enough to fit two cars.



House with Off-Street Parking in the Study Area

There are a few non-residential uses within the study areas. They include:

- Western Hills Elementary School
- St. Francis Xavier Catholic Church
- The Point Church
- Kearns Improvement District
- Mountain Man Park
- Church of Jesus Christ of Latter-day Saints (two locations)
- West Kearns Elementary School (just outside the border of the eastern study area)

Most of these uses have off-street parking or are ostensibly in walking or biking distance of most of the users. None is the type of major destination that makes a large parking impact on a neighborhood, except potentially at certain hours, such as school pickup and church service times.

However, looking at the demographics of the area shows a more interesting picture. The two study areas fall into two separate Census tracts. American Community Survey Data indicates that these Census tracts have significantly larger households, and these households have (and must park) significantly more vehicles than Salt Lake County as a whole.

As Figure 5 shows, nearly half (49%) of the western study area's households have four or more people. This rate is 65 percent higher than Salt Lake County as a whole. The eastern study area also has a higher rate of these large households (37%) but not as high as the western area.

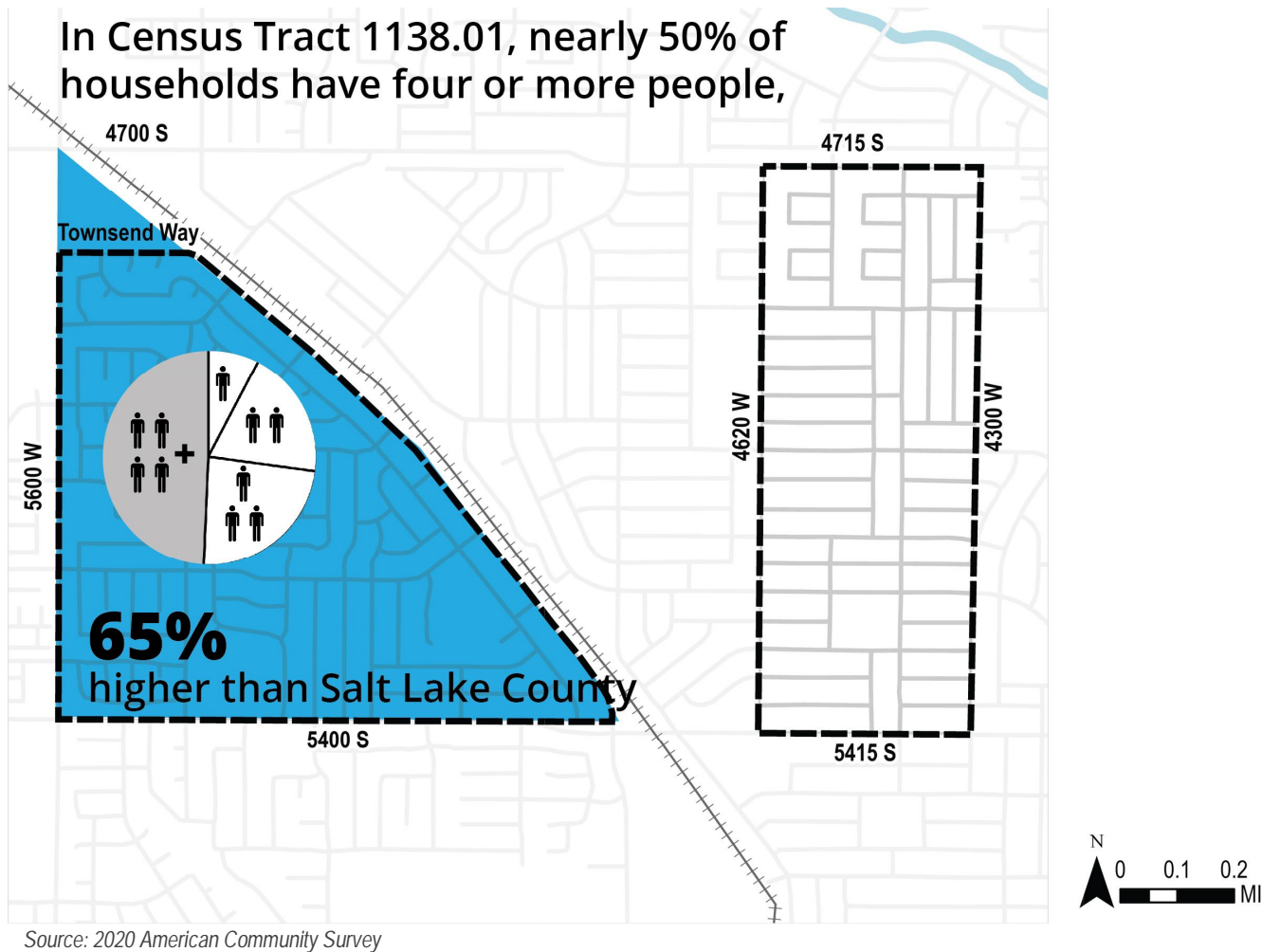
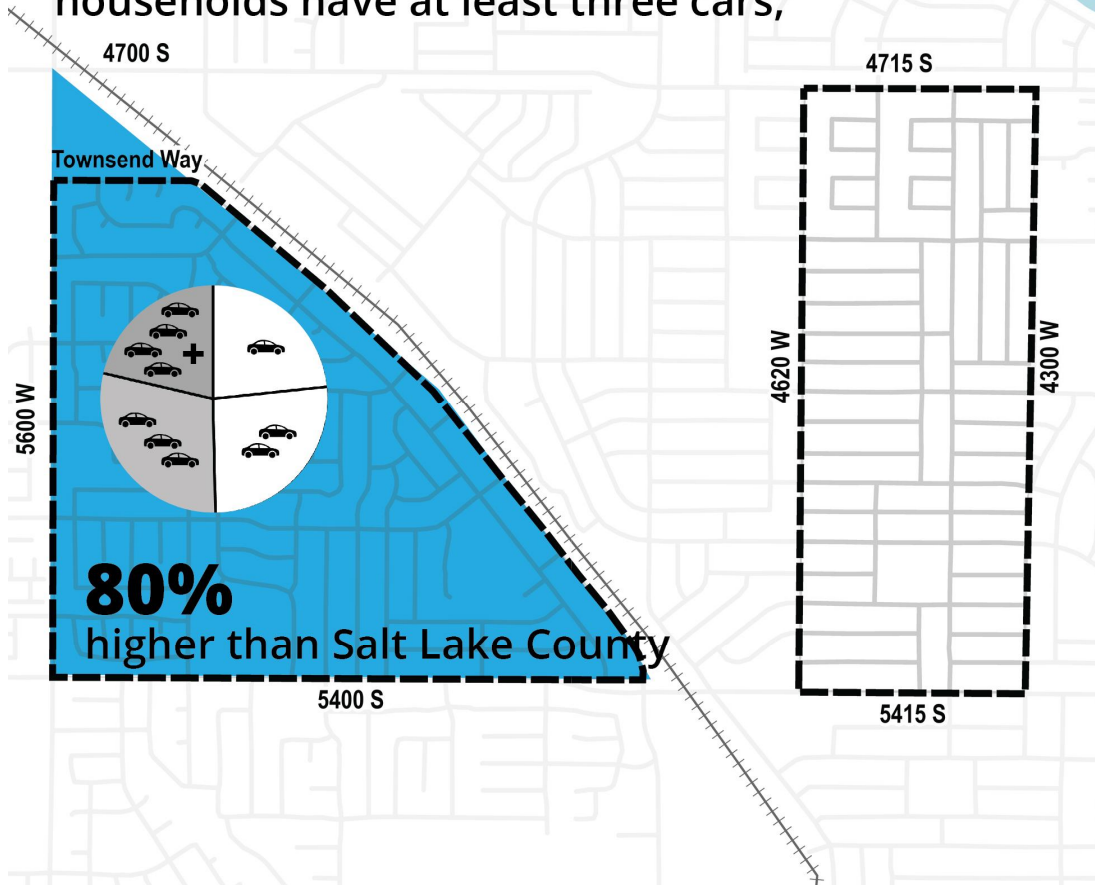


Figure 5 – Household Size in the Study Area

Relatedly, households in the study area also have high rates of vehicle ownership. As Figure 6 shows, nearly half (49%) of the western study area's households have three or more vehicles. And 21% have four or more vehicles. The rate for three or more vehicles is 80% higher than Salt Lake County as a whole. The eastern study area also has a higher vehicle ownership rate (33% have three or more vehicles) but not as high as the western area.

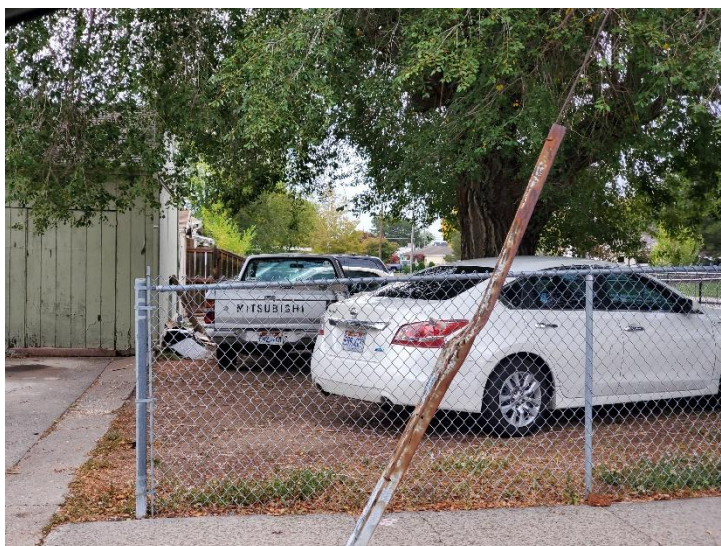


In Census Tract 1138.01, nearly 50% of households have at least three cars,



Source: 2020 American Community Survey

Figure 6 – Vehicle Ownership in the Study Area



Cars Parked in a Yard in the Study Area

These larger household sizes and vehicle ownership rates put more parking pressure on the streets, since many of these vehicles can't fit in driveways or elsewhere off-street.



3.3. Broader Kearns Land Use Considerations

The land use in the immediate study area doesn't necessarily tell the whole land use story. While the study area proper is all suburban density single family residential with complementary neighborhood land uses, the surrounding Kearns and West Valley City areas include a range of non-residential land uses with some key destinations that could influence parking in the area. Figure 7 shows these uses. They include the Kearns Town Center, Kearns High School, the Kearns/Oquirrh Park Fitness Center, and USANA Amphitheater. However, with the busy streets between most of these larger uses and the study areas, it's not likely – apart from a few peak times at, for example, USANA – that these uses are a major factor in on-street parking issues in the study areas.

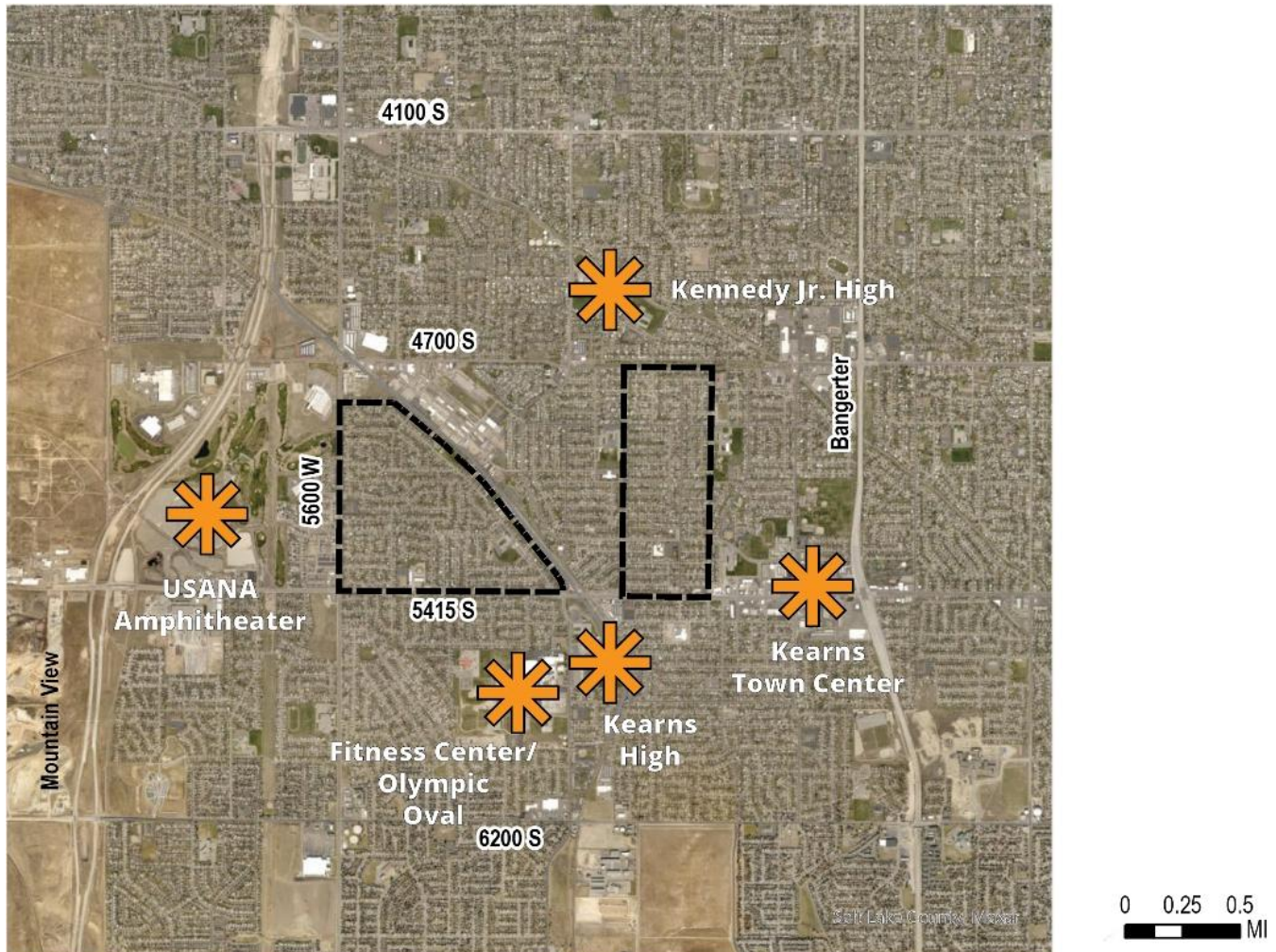


Figure 7 – Major Land Uses in the Surrounding Areas



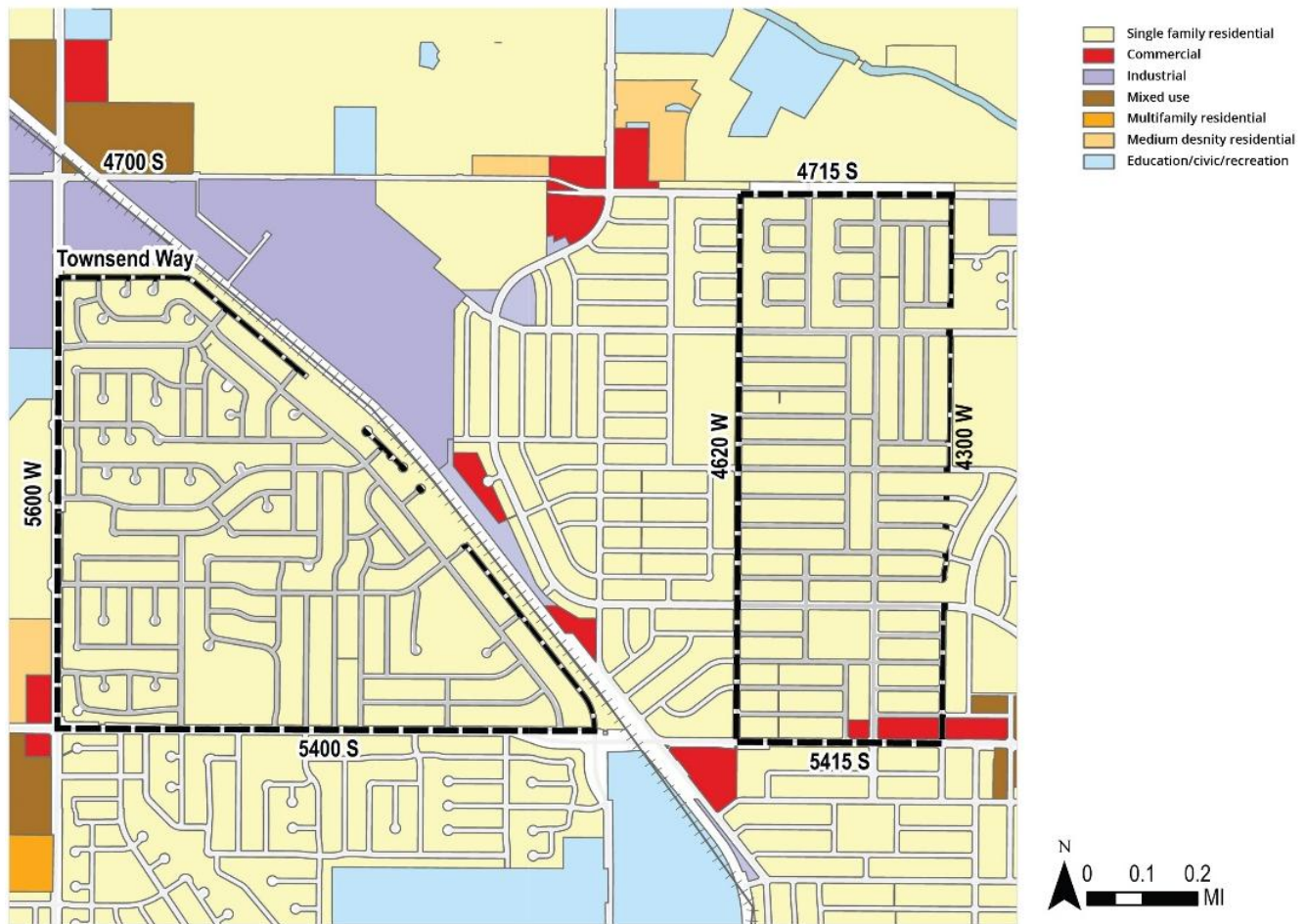
3.4. Future Land Use



Sign dissuading USANA attendees from parking on neighborhood streets.

In general, the land use pattern in and around the study areas is planned (Figure 8) to remain the same as the existing land uses. In the Kearns General Plan, the western study area is part of the Edgemont Park residential character area, while the eastern study area is part of the Camp Kearns residential character area. But they are both envisioned to remain similar in character as today. For both character areas, while the density could potentially increase through accessory dwelling units, the target density in both areas, 6-7 units/acre, resembles the existing density. No major developments are currently in the pipeline in and immediately around the study areas. In the areas around the study area, the land uses most likely to change

are in the Kearns Town Center, where the land use intensity of redeveloped sites could increase, and the largely commercial uses could turn over to a broader range of uses. The more impactful issue to watch will be how household size and vehicle ownership trend into the future – this seems to be the land use issue most influential to on-street parking in the study areas.



Source: WFRC

Figure 8 – Planned Future Land Use



4. Safety Review

The safety review and analysis for the Kearns On-Street Parking Study is summarized in this section. The safety review analyzed the most recent complete five years of crash data within the study areas, from 2017 to 2021. Crash data was sourced from the Utah Department of Transportation's crash database.

The two study areas had similar safety trends observed from 2017 to 2021. A summary of the crash analysis for the two study areas is as follows:

- Study Area #1
 - 583 Total Crashes
 - 5 Fatal Crashes (0.9%)
 - 354 Intersection Related Crashes (60.7%)
 - 12 Pedestrian Related Crashes (2.1%)
 - 77 Crashes Involving Parked Vehicles (13.2%)
- Study Area #2
 - 205 Total Crashes
 - 1 Fatal Crashes (0.5%)
 - 112 Intersection Related Crashes (54.6%)
 - 5 Pedestrian Related Crashes (2.4%)
 - 33 Crashes Involving Parked Vehicles (16.1%)

For reference purposes a crash search was performed on similar roadway types for all of Salt Lake County to see how the study areas compare to the rest of the County. A brief summary of this crash analysis for Salt Lake County is as follows:

- Salt Lake County
 - Fatal Crashes – 0.28%
 - Intersection Related Crashes – 56.63%
 - Pedestrian Related Crashes – 1.98%
 - Crashes Involving Parked Vehicles – 7.29%

Based on the crash analysis it was determined that the two study areas in the Metro Township have a higher percentage of fatal crashes and crashes involving parked vehicles when compared to Salt Lake County crash data on similar roadways. Pedestrian related crashes could be considered slightly higher than normal.

4.1. Crashes Over Time

Crashes within the study areas from 2017 to 2021 account for a total of 788 crashes, with an average of 157.6 crashes per year. 2018 experienced the highest number of crashes, exceeding the average by 20 crashes. The years 2020 and 2021 saw a drop in crashes, likely due to travel pattern impacts seen by the COVID-19 Pandemic. Crashes by year are shown in Figure 9.

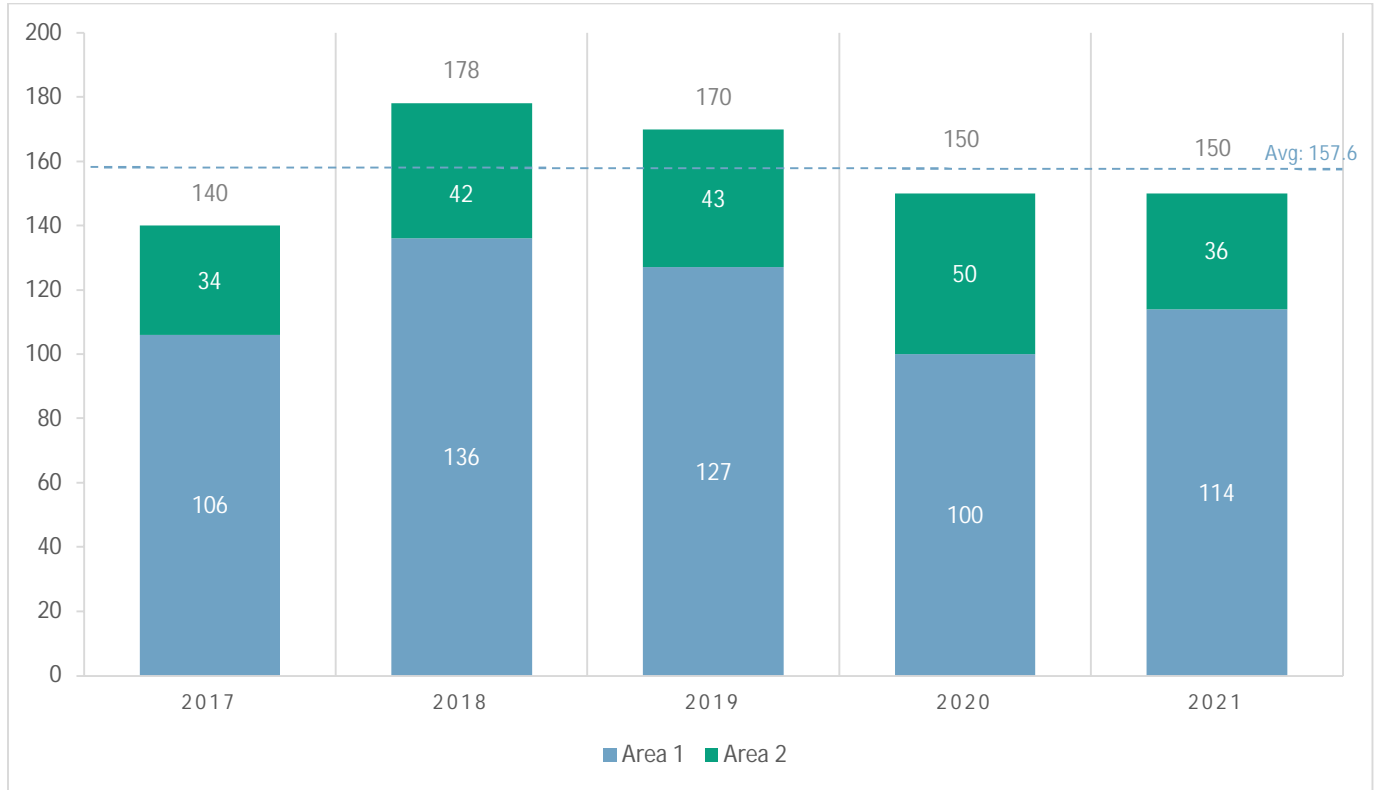


Figure 9 – Crashes by Year

4.2. Crashes by Severity

Crash severity denotes the highest level of injury resulting from the recorded crash. Table 1 shows the crash severity over the five-year period examined. Injuries observed over the five-year period were often minimal, with approximately 73% of all crashes resulting in no injury. Fatal injuries were least common, with approximately 1.2 fatal crashes occurring per year within the study areas.

Table 1 – Crashes by Severity

Severity	Number of Crashes	Percent of Crashes
No Injury	572	72.59%
Possible Injury	123	15.61%
Suspected Minor Injury	73	9.26%
Suspected Serious Injury	14	1.78%
Fatal	6	0.76%

Crashes by severity are displayed in Figure 10. Fatal and suspected serious injury crashes are most common at or near intersections throughout the study areas. A high density of fatal crashes were observed within Study Area #1 at intersections along 5400 South. Similarly, the fatal crash observed in Study Area #2 was at the intersection with 4620 West and 5215 South. Fatal crashes are most common at intersections along major roadways intersecting within the study areas.



Figure 10 – Crashes by Severity

4.3. Crash by Type

Crash types are shown in Table 2. Angled crashes are most common within the study areas, accounting for 33% of all crashes seen over the five-year period. Parked vehicle crashes account for 14% of all crashes as seen in Figure 11. Crashes involving parked vehicles are especially common throughout Study Area #1, especially along Heath Avenue. Study Area #2 has a variety of parked vehicle involved crashes, especially along 4420 West.

Table 2 – Crashes by Type

Crash Type	Number of Crashes	Percent of Crashes
Angle	258	32.7%
Front to Rear	197	25.0%
Parked Vehicle	110	14.0%
Sideswipe	76	9.7%
Head On	31	3.9%
Rear to Side	3	0.4%
Rear to Rear	1	0.1%
Other	112	14.2%

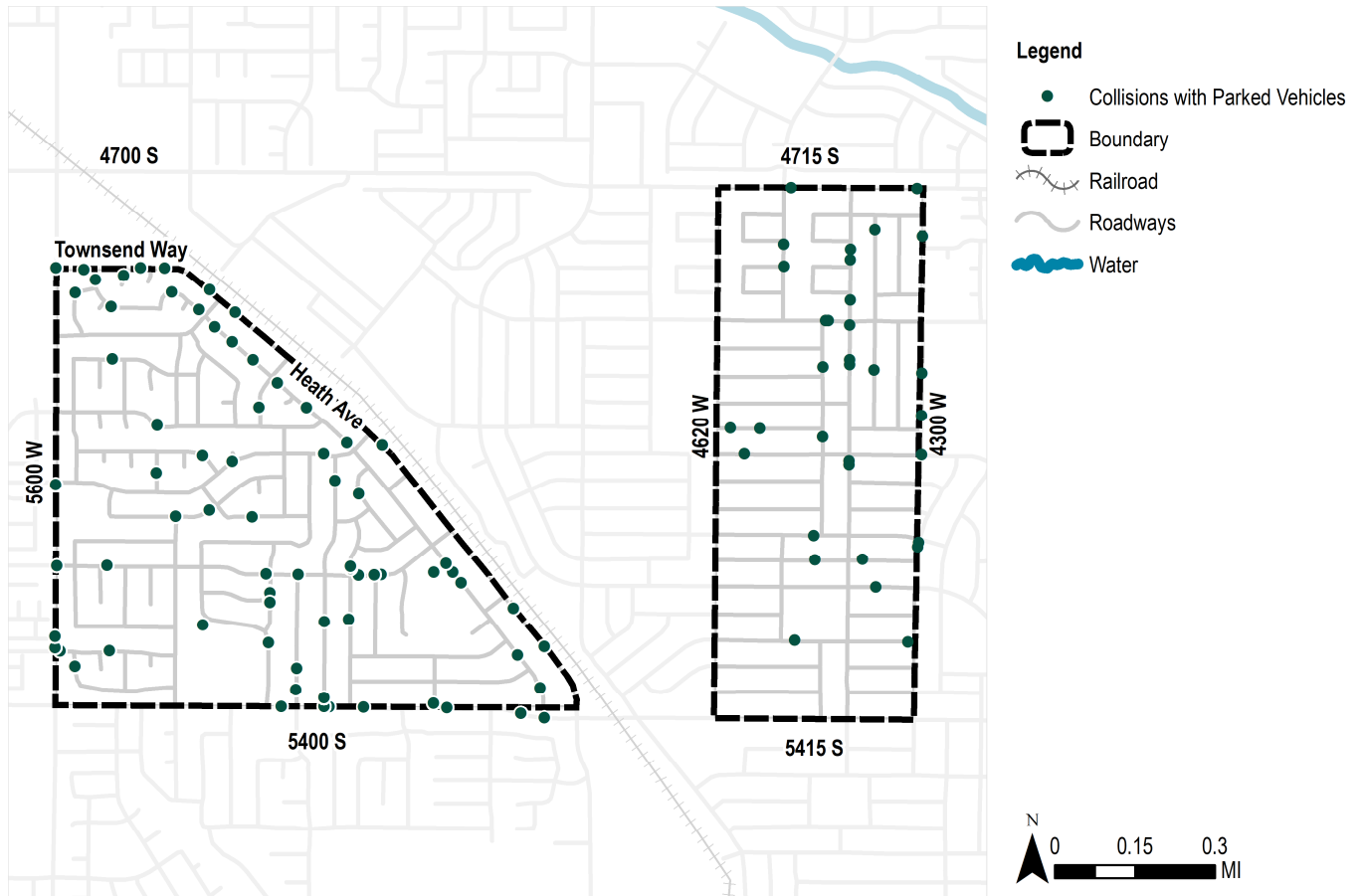


Figure 11 – Crashes Involving Parked Vehicles

4.4. Crash by Location

A total of 464 crashes occurred at an intersection within the study areas from 2017 to 2021. This accounted for 58.9% of all crashes observed. Intersection crash locations are displayed in Figure 12. Intersection crashes are most common along the perimeter roadways of the study areas.

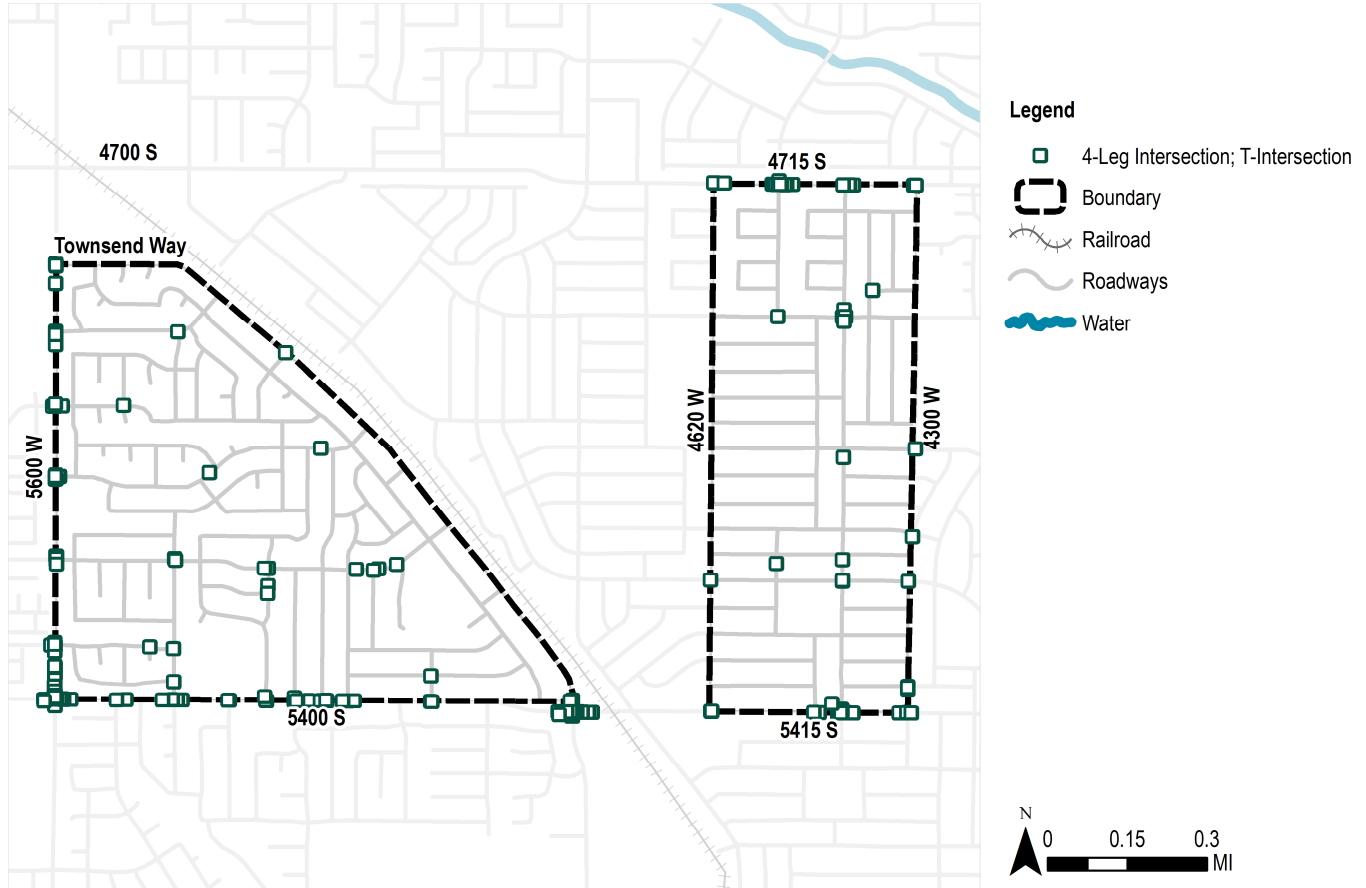


Figure 12 – Intersection Crashes

4.5. Pedestrian Crashes

Due to the pedestrian related crashes being slightly higher within the study areas, when compared to Salt Lake County, it is important to analyze crashes involving pedestrian. 17 crashes over the five-year period involved pedestrians, accounting for an average of 3.4 pedestrian-involved crashes per year. Of pedestrian involved crashes, 3 crashes resulted in a fatality. This higher than normal pedestrian related crashes might indicate unsafe conditions within the neighborhoods. One contribution factor to the pedestrian related crashes could be due to the number vehicles parking on the sidewalks causing some pedestrians to walk in the street.

2.16% of crashes
involved a pedestrian

Pedestrian-involved crashes locations are shown in Figure 13. All fatal pedestrian-involved crashes occurred on 5400 South. Pedestrian-involved crashes are typically found near intersections or where roadways bend throughout the intersection. They are most commonly along the perimeter of the study areas, with crashes occurring along 5400 South, 5600 West, 4715 South, 5415 South, and 4620 West.

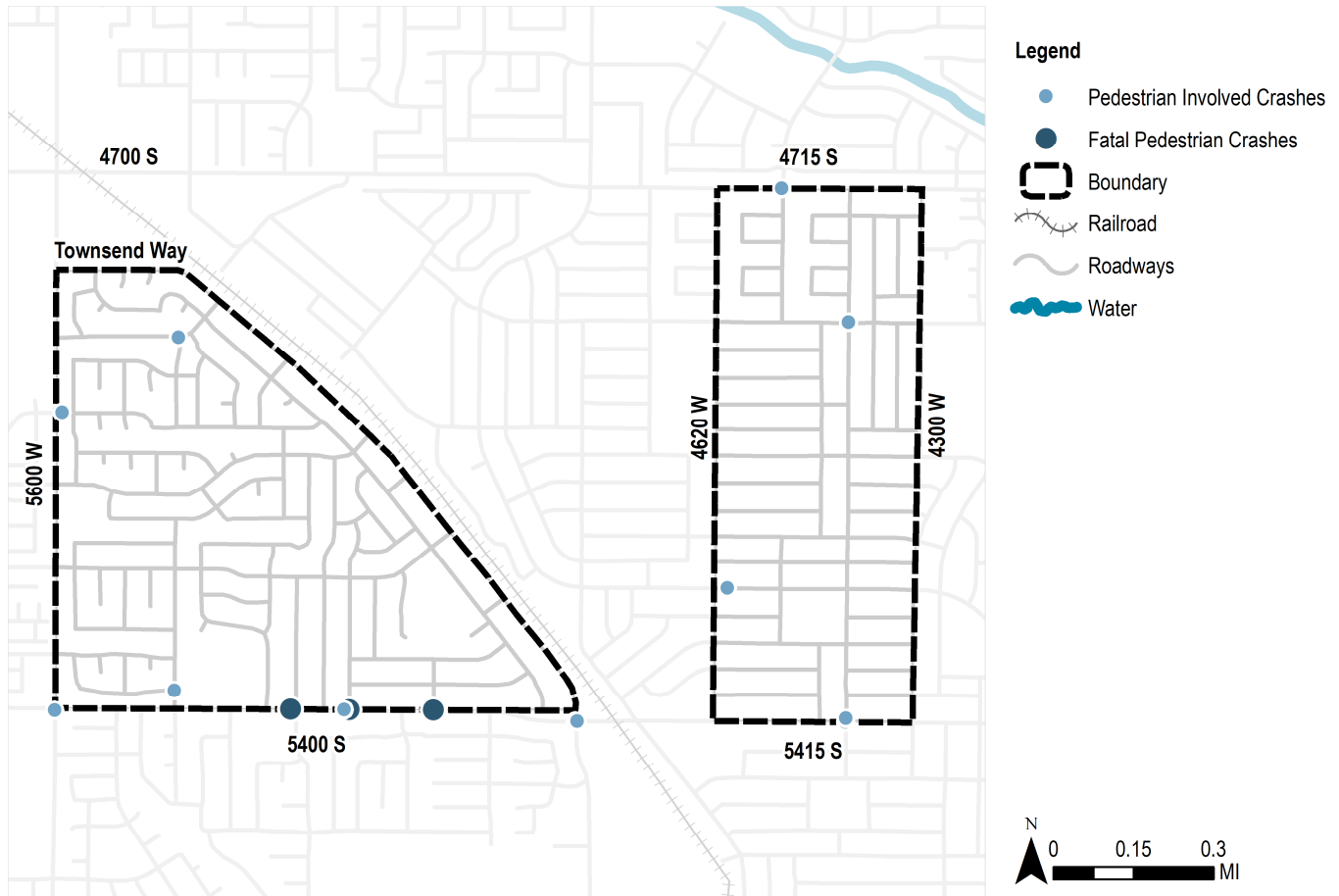


Figure 13 – Pedestrian Crashes

4.6. Key Observations, Takeaways, and Considerations

The following are the key observations and takeaways identified from the safety review:

- Based on the crash analysis it was determined that the two study areas in the Metro Township have a higher percentage of fatal crashes and crashes involving parked vehicles when compared to Salt Lake County crash data on similar roadways. Pedestrian related crashes could be considered slightly higher than normal.
- 14% of all crashes within the study areas involved parked vehicles.
- The majority of crashes occurred along the exterior roadways of the study areas where on-street parking is minimal or not permitted. These are also the higher volume and higher speed roadways within the study areas.
- Generally, crashes occurred at intersections within the study areas.
- The majority of pedestrian related crashes occurred along 5400 South in Study Area #1 which has no on-street parking

Based on the analysis, observations, and an additional safety focus field visit the following items were considered when developing recommended improvements for the study areas. These considerations were evaluated in conjunction with all other analyses performed.



- Replace rolled curb to reduce sidewalk parking
- Determine ideal on-street parking configuration based on typical roadway widths
- Consider eliminating parking on one side of the roadway for narrow roads
- Determine sight distance requirements for vehicles stop at intersection and eliminate on-street parking impeding a vehicles sight distance
- Consider eliminating semi-truck parking on neighborhood streets
- Consider eliminating on-street parking along higher volume and higher speed roadways



5. Parking Supply and Demand

This section of the report provides an overview of the parking supply and demand analysis performed for the study areas.

5.1. Data Collection

Data was collected within the identified study areas for the project. Collection included two components, inventory data collection and parking occupancy data collection. The purpose of data collection is to better understand the parking behaviors and patterns within the study areas, to assist future parking management initiatives and recommendations for the study. Following collection, data was analyzed using Microsoft Excel and ArcGIS to determine the parking system's behavior within the study areas.

Data was collected to observe typical weekday and weekend conditions. Weekday parking occupancy was collected on December 07, 2021, and December 14th, 2021. Weekend occupancy was collected on December 11th, 2021. All collection captured various times of day, collecting for early morning, afternoon, evening, and late evening. The parking occupancy data collection times for both the weekday and weekend are displayed in Table 3. The collected data has been analyzed to identify key trends of parking patterns within the study areas.

Table 3 – Park Occupancy Data Collection Times

Weekday	Weekend
7:00 AM – 9:00 AM	8:00 AM -10:00 AM
2:00 PM – 4:00 PM	12:00 PM – 2:00 PM
5:00 PM – 7:00 PM	5:00 PM – 7:00 PM
8:00 PM – 10:00 PM	9:00 PM – 11:00 PM

Other key measures and observations were collected to determine the utilization and habits of on-street and off-street parking vehicles within the study areas. In addition to parking occupancy of on-street vehicles, vehicles parked on the sidewalk, corner, and yard were recorded to determine where parking is being utilized outside of use of personal driveways.

5.2. Parking Occupancy by Time of Day

Parking occupancy was analyzed from the observed data collection time periods. Of the observed time periods, the weekend observation periods observed higher occupancy at all collection times compared to the weekday, as expected within a residential area. Both weekday and weekend collection observed peak occupancy from 5:00 PM to 7:00 PM, at 24% occupied and 28% occupied, respectively. Parking occupancy by time of day for both weekday and weekend are summarized in Figure 14 and Figure 15, respectively.

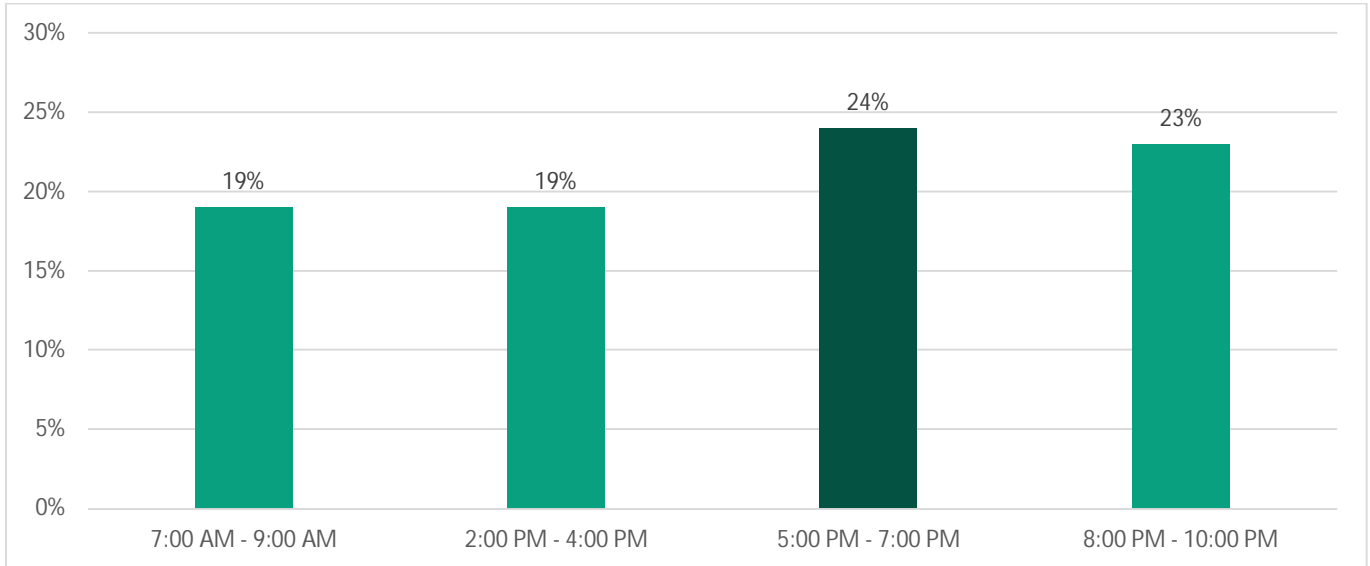


Figure 14 – Weekday Occupancy by Time of Day

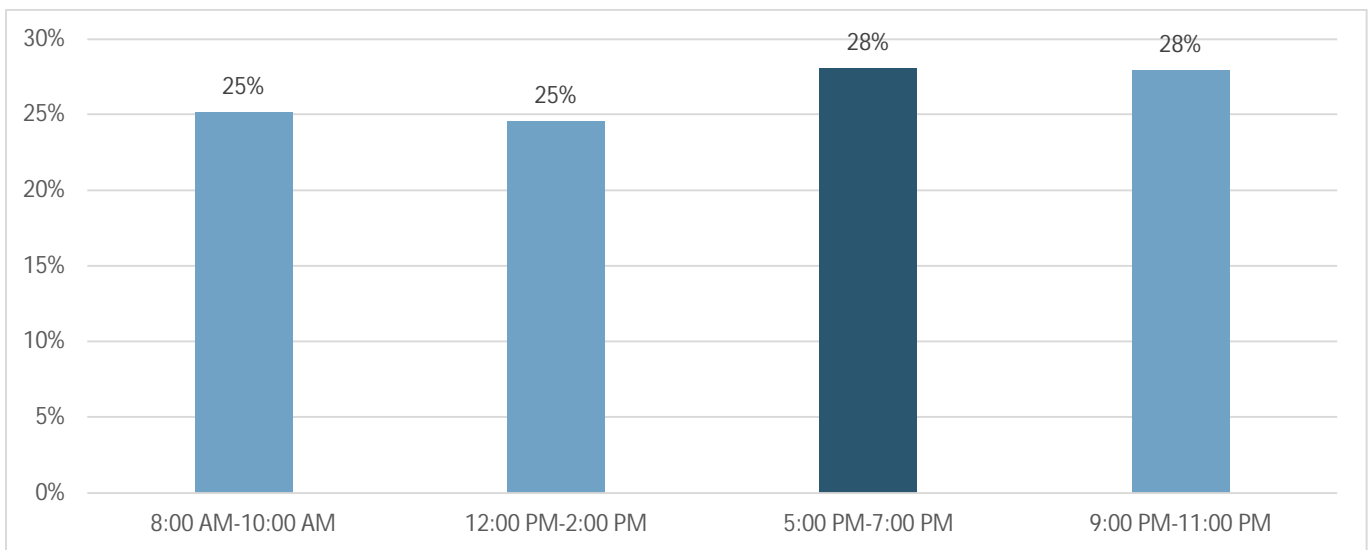


Figure 15 – Weekend Occupancy by Time of Day

The peak occupancy for weekday and weekend observed conditions are shown in Figure 16 and Figure 17. Parking occupancy collection data and additional occupancy figures are located in the Appendix A.

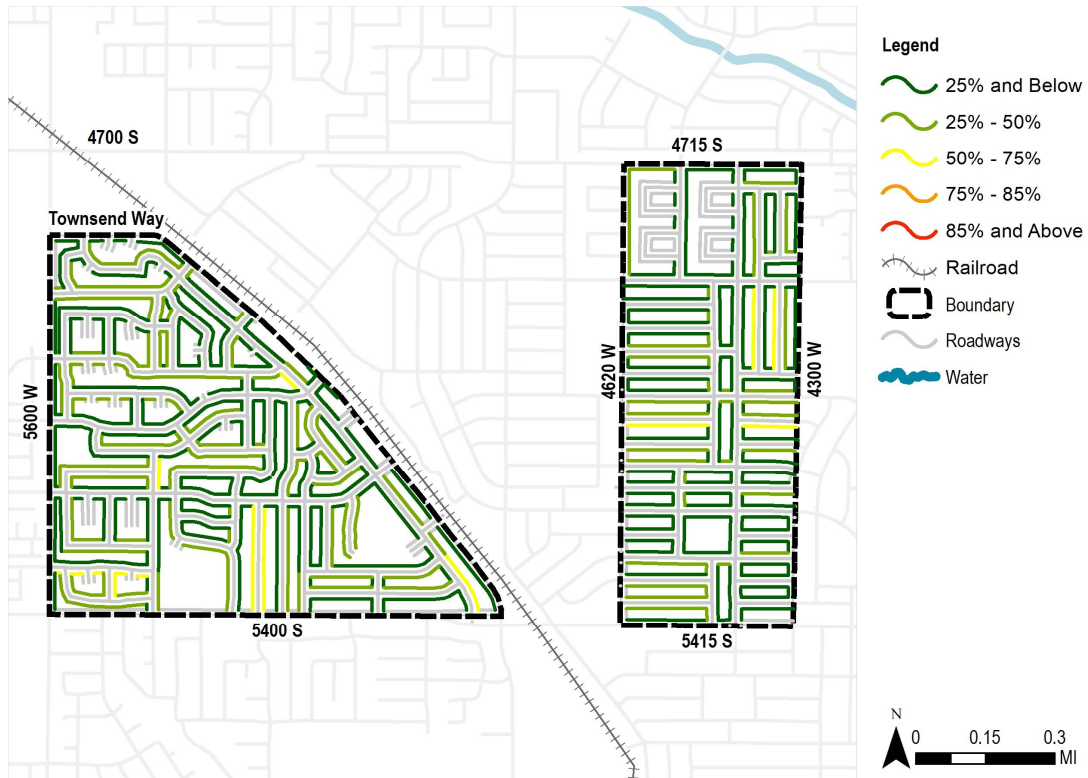


Figure 16 – Weekday Peak Occupancy (5:00 PM - 7:00 PM)

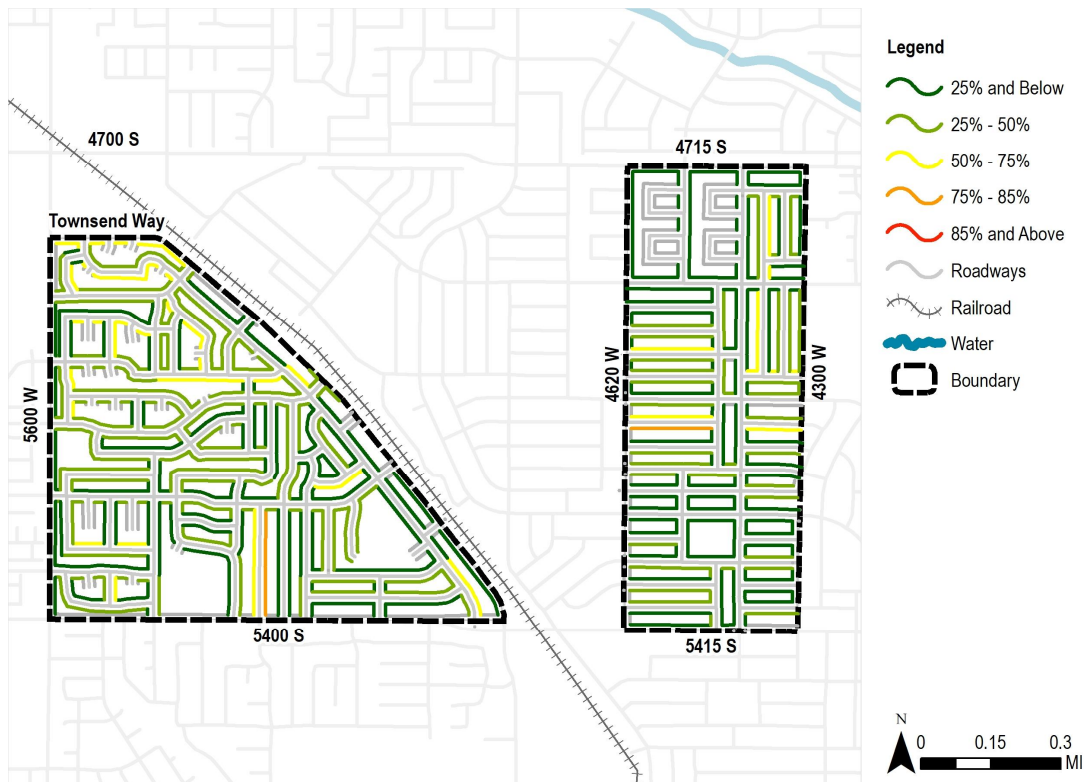


Figure 17 – Weekend Peak Occupancy (5:00 PM - 7:00 PM)



5.3. Vehicles Parked on the Sidewalk

To determine parking trends within the study areas, the number of vehicles parked on the sidewalks was also recorded. This metric provides observations of the use of parking facilities and how on-street parking is limiting active transportation within the study areas. The percent of on-street parked vehicles partially parked on the sidewalk by time of day is shown in Table 4. The lowest percentage of vehicles parked on the sidewalk was observed during the late evening time period at 25% and 27%. The highest percentage of vehicles parked on the sidewalk was observed during the evening time period at 35% and 32%. Although the majority of vehicles parking on the street are not interfering with the sidewalk, 25% - 35% is a significant number of vehicles that may be impeding the pedestrian walkways within the study areas.

Table 4 – Vehicles Parked on Sidewalks by Time

Weekday	% of Parked Vehicles on the Sidewalk	Weekend	% of Parked Vehicles on the Sidewalk
7:00 AM – 9:00 PM	26%	8:00 AM – 10:00 AM	29%
2:00 PM – 4:00 PM	33%	12:00 PM – 2:00 PM	32%
5:00 PM – 7:00 PM	35%	5:00 PM – 7:00 PM	32%
8:00 PM – 10:00 PM	25%	9:00 PM – 11:00 PM	27%

5.4. Vehicles Parked on Intersection Corners

One issue highlighted in the first stakeholder meeting was vehicles parking at or near intersection corners resulting in sight distance and access issues for snowplows and garbage truck making turning movements at those locations. During the data collection, vehicles parked at intersection corners were noted. Figure 18 shows the segments where vehicles parking on corners were present during the various observation times and days. Corner parking is most common in Study Area #1, specifically throughout the northern half of the study areas. These roadways are typically more winding than Study Area #2, making corner parking a larger impact on sight distance.



Figure 18 – Segments with Vehicles Parked on a Corner

5.5. Vehicles Parked in Yards

The data collection also observed the number of vehicles parked in residential yards, outside of a typical or standard driveway. During all collection days and times, over 2,000 vehicles were identified as parked in yards. During data collection vehicles were considered as parked in a yard if a vehicle is found parked in a location other than a clearly identified driveway. Kearns Metro Township Municipal Code (19.80.035) states that private vehicles parked on residential properties shall be parked or stored on a paved surface in the front yard, side yard, or rear yard of a dwelling. This paved surface or driveway cannot occupy more than 50% of the area of a front or rear yard.

If vehicles identified as parked in yards weren't allowed because of a lack of compliance to municipal code, then those vehicles would most likely have to park at on-street parking locations. To understand the impact a change like that would have all the vehicles observed parking in yards were allocated to on-street parking. Table 5 shows the impacted on-street occupancy with the reallocation of vehicles parked in yards to on-street parking. In general, the occupancy of the on-street parking within the study areas would increase but would still not exceed available on-street parking spaces under this scenario. The peak occupancy for weekday and weekend observed conditions for each study area are shown in Figure 19 and Figure 20.



Table 5 – On-Street Parking Occupancy adjusted to include Vehicles Parked in Yards

Weekday			Weekend		
Data Collection Times	On-Street Occupancy	On-Street Occupancy with Vehicles Reallocated	Data Collection Times	On-Street Occupancy	On-Street Occupancy with Vehicles Reallocated
7:00 AM – 9:00 AM	19%	31%	8:00 AM – 10:00 AM	25%	35%
2:00 PM – 4:00 PM	19%	30%	12:00 PM – 2:00 PM	25%	35%
5:00 PM – 7:00 PM	24%	38%	5:00 PM – 7:00 PM	28%	40%
8:00 PM – 10:00 PM	23%	34%	9:00 PM – 11:00 PM	28%	38%

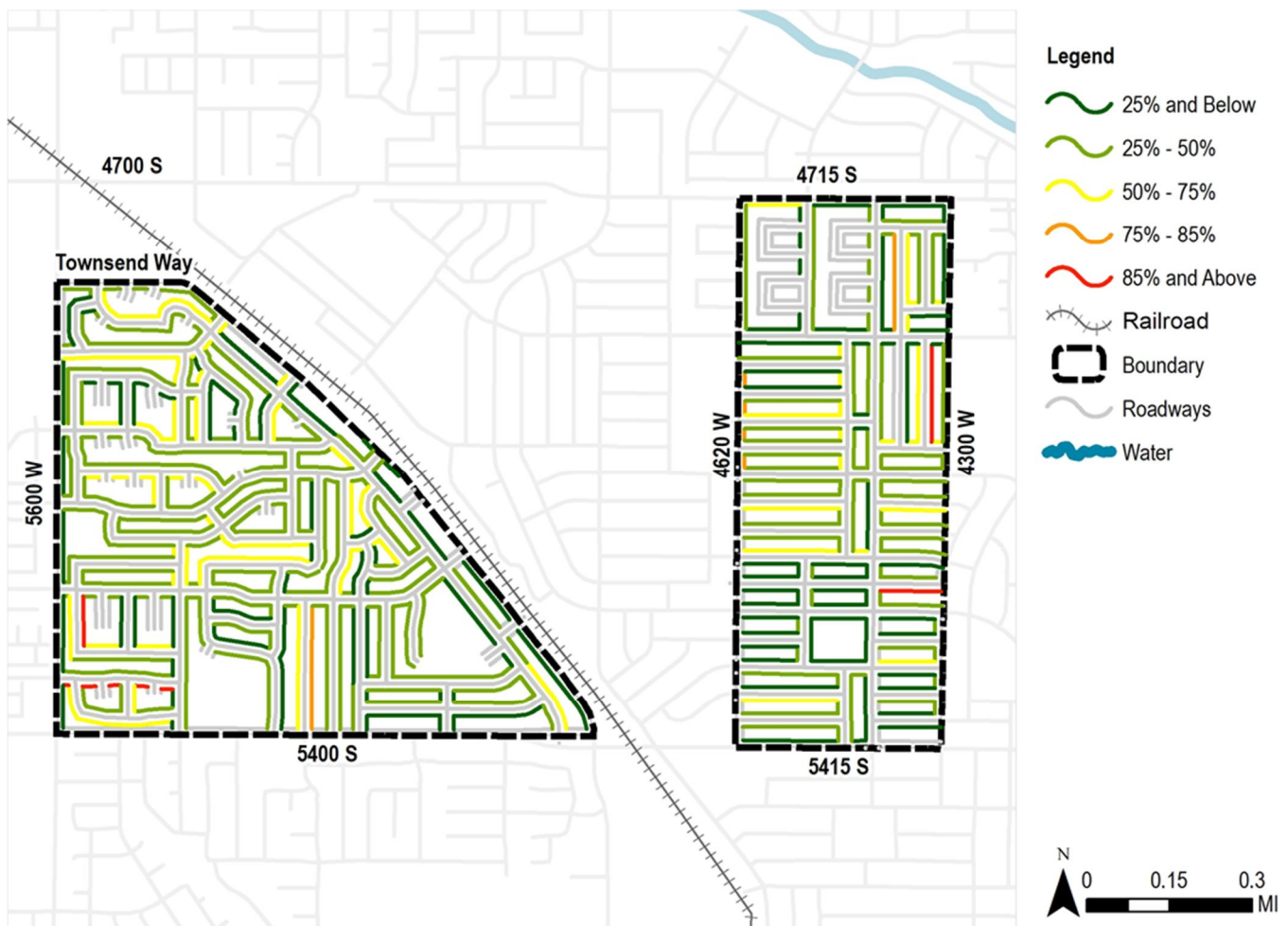


Figure 19 – Weekday Peak Occupancy (5:00 PM - 7:00 PM) including Yard Parking



Figure 20 – Weekend Peak Occupancy (5:00 PM - 7:00 PM) including Yard Parking

5.6. Future Parking Conditions

Parking demand in the observed study areas is not anticipated to grow in the future planning horizons of 2027 and 2032. This is based on the following findings and observations:

- The existing conditions of the study areas are at full build out
- No identified redevelopment in the future for the study areas
- Single family residential has a low propensity for turnover
- Future plans have identified Travel Demand Management (Transit) projects surrounding the study areas

No relevant data was available on to the relationship between household size or vehicle ownership and their effects on on-street parking. However, the larger household sizes and vehicle ownership rates of the study areas than the whole of Salt Lake County were considered in the recommendation process.



5.7. Key Observations and Takeaways

The following are the key observations and takeaways identified from the parking supply and demand analysis conducted for the study areas:

- Parking trends for weekday and weekend peak occupancy during the various time periods are similar with the majority of roadway segments being underutilized, with peak occupancy reaching 28%. Peak occupancy on both weekday and weekend collection was observed at 5:00 PM to 7:00 PM.
- Observed parking conditions during the weekend peak occupancy had more segments above 50% occupancy than observed during peak occupancy on weekday collection.
- Common higher occupancies are observed at the following locations:
 - Study Area #1:
 - 5240 West from 5200 South to 5400 South
 - Trident Drive from 5600 West to 5420 West
 - Study Area #2:
 - 5055 South from 4620 West to 4300 West
 - 4380 West from 4745 South to 4835 South
- Vehicles parked on the street are commonly intersecting with the sidewalk, blocking active transportation and ADA uses. Of the observed periods, 25% to 35% of vehicles parked on the street were also interfering with the sidewalk.
- Corner parking is most common in Study Area #1, specifically throughout the northern half of the study areas. These roadways are typically more winding than Study Area #2, making corner parking a larger impact on sight distance.
- Many vehicles were observed to park in yards rather than utilizing on-street parking. During all of the observed times, over 2,000 vehicles were parked in yards. If vehicles parked in yards were relocated to street parking, on-street parking occupancy would increase but still be underutilized, with a peak occupancy of 38% and 40% for weekday and weekend collection, respectively.

6. Parking Signage

Signage related to on-street parking was present in both of the study areas. Figure 21 provides an example of the two on-street parking signs that can be found within the two study areas. Both of these signs appropriately communicate the parking regulations found in these areas. Like all roadway signage, their effectiveness is directly correlated to the level of effort and time that is available to enforce these parking regulations. It was noted that additional signage related to no on-street parking during snow events should be considered for Study Area #2 as this area has less of the main entrance roadways into the neighborhood with this sign present.



Figure 21 – Examples of On-Street Parking Signs



7. Public Engagement

Public engagement efforts were comprised of two separate phases. The first phase was a stakeholder meeting with key representatives from various entities or agencies. The second public engagement phase involved community outreach to communicate the results of the technical analysis and allow the community the opportunity to provide input and feedback. This section of the report provides an overview of the public engagement phases conducted as a part of this study.

7.1. Stakeholder Meeting

The initial phase of the public engagement plan was comprised of a stakeholder meeting. The stakeholder meeting was hosted virtually on September 29, 2021. Key stakeholders included representatives from the following entities:

- MSD Planning and Development Services Staff
- Kearns Council
- Salt Lake County Public Works Engineering
- Unified Police Department (UPD) – Kearns Precinct

The purpose of the meeting was to identify opportunities and challenges within the study areas. These identified opportunities and challenges were used to guide the technical analysis and future recommendations for the project. Topics discussed included the parking availability, safety, management practices, and policy. The discussion is summarized in the following subsections.

Parking Availability

The following parking opportunities and challenges were identified within the project study areas.

Opportunities

- Establishing time limits for on-street parking may be beneficial
- During event conditions, high parking demands are present, but people are generally accepting of it due to the short-term event conditions
- Implementing signage that indicates vehicles may be ticketed or towed when parked on the street during a snow event or when vehicles are blocking snowplows
- Improving the reporting and communication process for parking issues since UPD relies on residents to report parking issues or violations

Challenges

- Smaller driveways cannot support the number of vehicles required per household
- On-street parking is overwhelmed due to lack of off-street parking
- Mismatch of locations with parking demand and locations where parking supply is available
- Lack of enforcement resources to capture parking violations
- To be effective, enforcers must be able to take pictures of the parked vehicles and document the duration for which a vehicle is parked
- No good access between residential areas and activity areas to allow neighborhoods to absorb parking overflow from parking facilities
- Difficult for garbage trucks and fire trucks to turn corners due to vehicles parking too close to the street corners



- More parking at Lodestone Park is needed

Safety

The following safety opportunities and challenges were identified within the project study areas during the stakeholder meeting.

Opportunities

- Possible benefit to leave residential roadways unplowed to allow vehicles to park on the street and just plow main arterials

Challenges

- Construction and service vehicles park on the street in places that can block the view of other vehicles or block residential driveways
- Snowplows are unable to navigate the roadways
 - Creates safety issues for vehicles and pedestrians
 - Snowplows start plowing between 3:00 and 4:00 AM whereas UPD begins after these times
 - When violations come in to UPD, the snowplow has already gone through this area
- Narrow streets cause safety hazards for all user groups
- Many residents use the roll-up curbs to park on the sidewalk impeding the pedestrian travel way
- Residential neighborhoods have few marked crossings

Management Practices/Coordination

The following opportunities and challenges were identified within the study areas regarding current management practices and coordination processes.

Opportunities

- Communication of management practices occurs through the General Plan
- Internal Municipal Services District (MSD) Manager's meetings are performed weekly
 - Teams use these meetings to coordinate across disciplines
- Monthly meetings with the Mayor, UPD, and Fire Department to discuss code violations and nuisance complaints
 - Meetings may provide an opportunity to discuss plans for parking needs
- Opportunity to coordinate with the engineering department on grant projects
- MSD has traffic safety, complaint, and citation data available

Challenges

- Communication with UPD parking enforcement team can be difficult to coordinate with at times
 - Staff capacity is stretched with other responsibilities and projects

Policies

The following policy opportunities and challenges were identified for the project study areas.

Opportunities

- A code re-write for all of MSD is currently underway
- Changes to zoning code can require driveway design to be a "C" shape to allow for more off-street parking at residences



Challenges

- Policies require residents to remove vehicles from street parking when snow is anticipated.
 - This policy is hard to enforce
 - It may be helpful to put out a mailer at the beginning of each year related to the snow removal policy
 - Current policies allow residents to park their vehicles in their yard for 48 hours before, during, and after a snowstorm
- Difficult to create new policy when Kearns is built out. Density increasing without an increase in parking available will be an issue in the future.

7.2. Community Outreach

The intent of the community outreach and survey was to communicate the results of the technical analysis and allow the public an opportunity to react to the analysis, which assesses the current condition of on-street parking in the Metro Township based on data collected directly from the designated study areas. The Kearns On-Street Parking Study survey took place from June 20, 2022 to July 18, 2022. The effort consisted of one overview/introduction graphic along with five individual infographics to inform the community about the results of the Kearns On-Street Parking Study technical analysis and encourage them to participate in the survey by providing a direct link. Included in this subsection is an overview of the developed infographics, a summary of survey responses, and key takeaways utilized in the development of on-street parking recommendations for the Metro Township. The complete Community Outreach Memorandum is located in Appendix B.

Infographic Overview

Infographics were developed as part of the public engagement plan to disseminate information to the community and encourage individuals to participate in the survey. The Greater Salt Lake Municipal Services District posted the series of infographics on their Facebook page on June 20, June 23, June 27, June 30, July 6, and July 11. These posts were shared on the K-Town (Kearns Township) Facebook page on June 20, June 27, June 30, July 6, and July 11. Five individual infographics were developed to convey information related to vehicles parking in yards, crashes involving parked vehicles, vehicles parking on sidewalks, intersection-related crashes, and on-street parking availability. These infographics can be seen in Figure 22. The introduction graphic, as seen in Figure 23, was also shared in the MSD e-Newsletter on July 6. Full details of the Facebook posts and comments can be found in Appendix B.



Figure 22 – Summary Infographics



Kearns On-Street Parking Study

Kearns Metro Township and the Salt Lake Municipal Services District have conducted an On-Street Parking Study to evaluate parking availability, demand for parking, and parking habits.

Our study chose two areas in Kearns that represented the diverse homes and neighborhoods in Metro Township. We studied parking habits on one weekday and one weekend day in the morning, afternoon, evening, and nighttime. Our team used these findings to draft some potential recommended changes that we are aiming to implement by 2025.

Over the next two weeks, we will be sharing some key findings from the study with you. Follow along and let us know what you think of our results and recommendations by taking our survey. Our complete report will be available later this summer.

Tell us more about your experience parking in Kearns Metro Township here:

<https://tinyurl.com/KearnsParking>

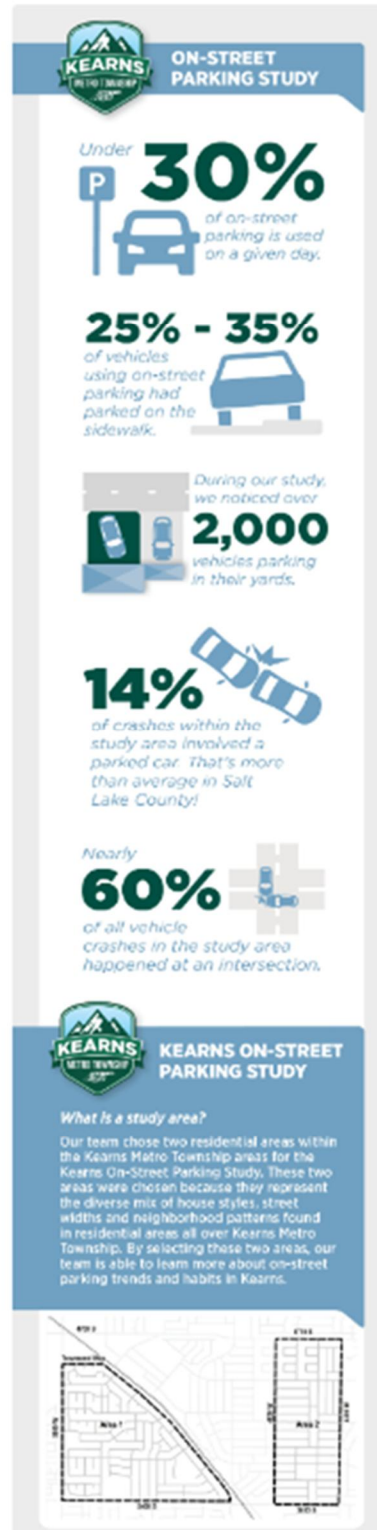


Figure 23 – MSD Newsletter Post (July 2022)



Survey

The Kearns On-Street Parking Study survey took place from June 20, 2022 to July 18, 2022. A total of six survey questions were developed based on the technical analysis performed. The following subsections introduce each of the survey questions, outline the results of the survey questions, and provide an overview of the respondents' comments.

Question #1

The survey asked residents their opinion why they park their vehicles in their yard with the following question. A summary of responses is provided in Figure 24 and Table 6.

QUESTION #1: Our study found that many vehicles were parked in yards, despite there being street parking available. If you park your vehicle in your yard, what is the reason for your decision?

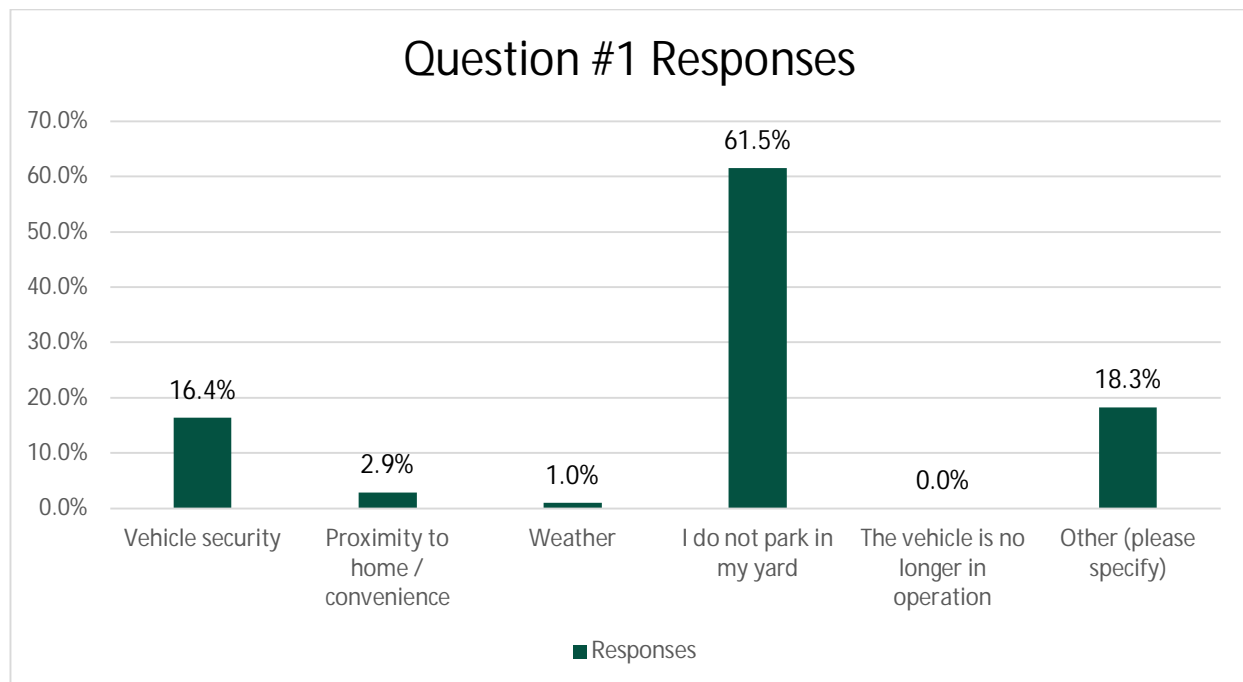


Figure 24 – Survey Question #1 Responses

Table 6 – Survey Question #1 Response Breakdown

Answer Choices	Percent	# of Responses
Vehicle security	16.4%	17
Proximity to home/convenience	2.9%	3
Weather	1.0%	1
I do not park in my yard	61.5%	64
The vehicle is no longer in operation	0.0%	0
Other (please specify)	18.3%	19
Total		104
Skipped		4



104 respondents participated in Question 1, and 19 left a comment related to Question #1. The following is a summary of the respondents that reported “Other” reasons for parking in their yards:

- One respondent mentioned poor street lighting.
- Two reported that the street they live on is too narrow.
- Two respondents reported they were trying to get concrete installed.
- Five respondents cited their neighbors as a reason for parking in their yard.
- Two respondents stated they got into the habit due to winter parking restrictions.
- Two respondents indicated they park in the yard to avoid getting hit by a passing car.
- One respondent explained they do not have enough room in the driveway for all the vehicles in the household.

Question #2

The survey asked residents their opinion on potential on-street parking changes with the following question. A summary of responses is provided in Figure 25 and Table 7.

QUESTION #2: Our project team has drafted some potential recommended changes. Which recommendations would you like to see?

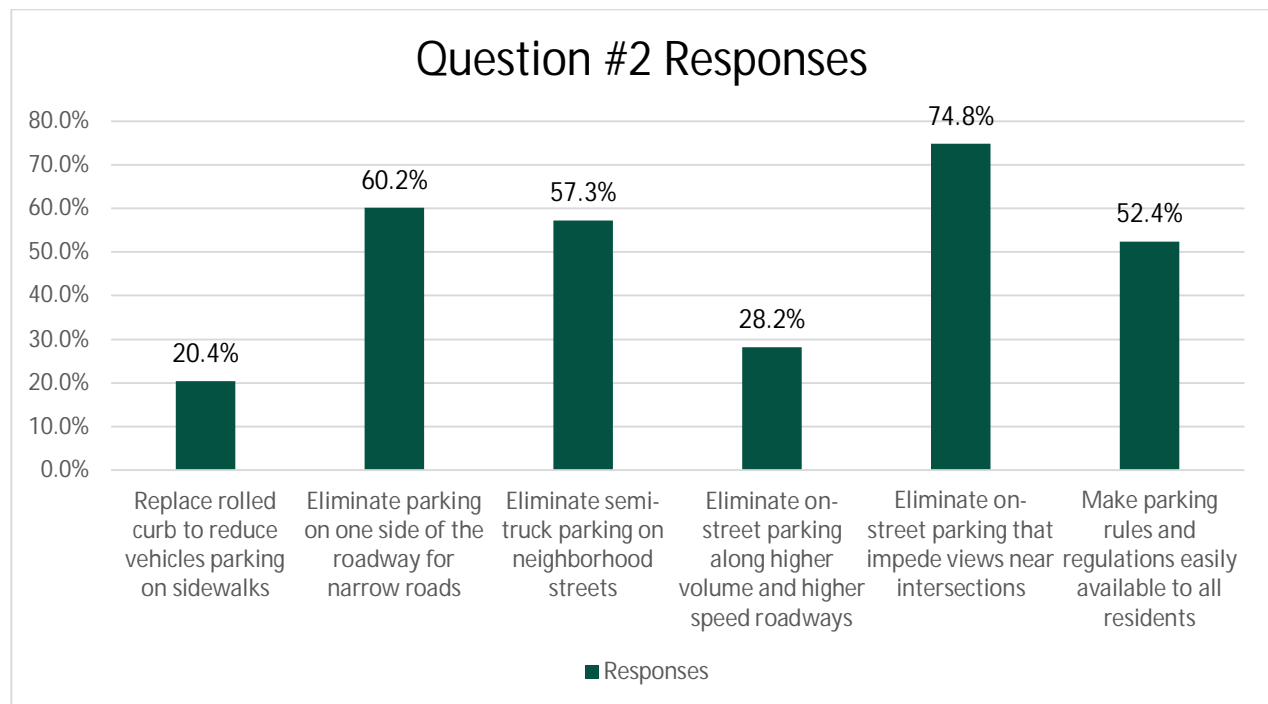


Figure 25 – Survey Question #2 Responses



Table 7 – Survey Question #2 Response Breakdown

Answer Choices	Percent	# of Responses
Replace rolled curb to reduce vehicles parking on sidewalks	20.4%	21
Eliminate parking on one side of the roadway for narrow roads	60.2%	62
Eliminate semi-truck parking on neighborhood streets	57.3%	59
Eliminate on-street parking along higher volume and higher speed roadways	28.2%	29
Eliminate on-street parking that impedes views near intersections	74.8%	77
Make parking rules and regulations easily available to all residents	52.4%	54
Total		103
Skipped		5

103 respondents participated in Question #2. The most popular proposed improvements were:

- Eliminate on-street parking that impedes views near intersections, which received 77 votes (74.8%).
- Eliminate parking on one side of the roadway for narrow roads, which received 62 votes (60.2%).
- Eliminate semi-truck parking on neighborhood streets, which received 59 votes (57.3%).
- Make parking rules and regulations easily available to all residents, which received 54 votes (52.4%).

Question #3

The survey asked residents for their input of what recommendations they have to improve on-street parking with the following question. Figure 26 displays key comments associated with the survey responses for Question #3.

QUESTION #3: What recommendations do you have to improve on-street parking in Kearns Metro Township?

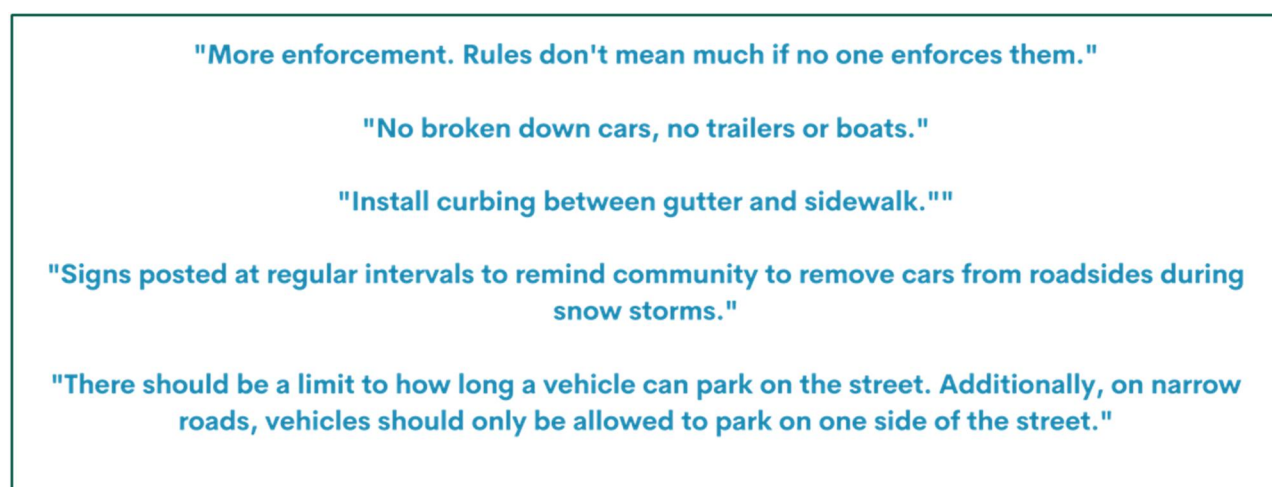


Figure 26 – Survey Question #3 Key Comments



60 respondents left a response to Question #3. The following is a summary of those responses.

- Eleven respondents expressed that they wanted to see better enforcement of existing parking rules.
 - Two respondents expressed concerns about theft/burglary.
- Eight respondents said something about limiting the types of vehicles on the street—specifically concerned with RVs, large trucks, trailers, and cars that are inoperable.
- Four respondents wanted to limit the number of vehicles allowed per household.
- Four respondents want to see a limitation on the number of vehicles a household can park on the street.
- Seven respondents wanted to see parking restricted to one side of the street, and two respondents expressed safety concerns when parking is permitted on both sides of the road.
- Six respondents want stronger enforcement of winter parking restrictions, with one suggesting better signage to advertise the rules.
- Four respondents expressed interest in better signage to better promote existing parking rules.
- Four respondents expressed concern with parking near intersections and corners, with one suggesting road/curb striping to prevent cars from parking too close.
- Three respondents supported a change in regulation to allow cars to park in yards.
- One respondent expressed interest in one-on-one meetings with certain properties and neighborhood meetings to discuss ideas.

Question #4

The survey asked residents their opinion on additional items the project team should think about when considering on-street parking with the following question. Figure 27 displays the key comments associated with the survey responses for Question #4.

QUESTION #4: What additional items do you think the project team needs to know about on-street parking in Kearns Metro Township?

"I think rules and regulations should take into consideration that some households have a lot of cars for a reason. It's expensive to park your car elsewhere, to move around your work car or work tools, and that some households have a lot of families."

It happens because our neighborhoods are old and only allow for one car in the driveway, two if you put them one in front of the other. We don't all have garages. The street is the only other option; people will not NOT park in the street."

"I think it would be good if people could be more aware that they should avoid parking in a way that obstructs the sidewalk. This is especially important for pedestrians with disabilities."

"Trailers parked to close to intersections and make it difficult to see oncoming traffic. Or they are parked to close to crosswalks making it a hazard to see pedestrians."

Figure 27 – Survey Question #4 Key Comments



42 respondents left a comment on Question 4. The following is a summary of those comments.

- Eleven respondents commented that there are many homes with multiple generations or multiple families living there.
 - Two respondents were against restricting on-street parking for this reason.
 - One comment expressed feeling it would be unfair to restrict parking on both sides of the street.
 - One comment suggested requiring rental properties to supply adequate off-street parking.
 - Three comments suggested the lack of on-street parking was due to older properties not being built to accommodate multiple vehicles.
- Eleven respondents commented that on-street parking comes with safety concerns—such as narrow streets, visibility, lighting, and parking in unsafe locations (near intersections, speedbumps, and sidewalks).
 - Two comments suggested that people park on sidewalks to avoid getting hit by passing cars.
- Six comments suggested that vehicles parking on sidewalks is a big issue, and one respondent suggested painting the curbs to prevent a parked car from touching the yard.
- Four comments explained that enforcement is lacking in neighborhoods and three comments suggested winter parking rules should be enforced or better advertised.
- Three comments pointed to larger vehicles, such as RVs and trailers reducing visibility and taking up much of the available on-street parking.

Question #5

The survey asked residents their opinion on safety concerns at intersections with the following question. A summary of responses is provided in Figure 28 along with the key comments associated with the survey responses in Figure 29.

QUESTION #5: Do you feel there are safety concerns at intersections in your neighborhood

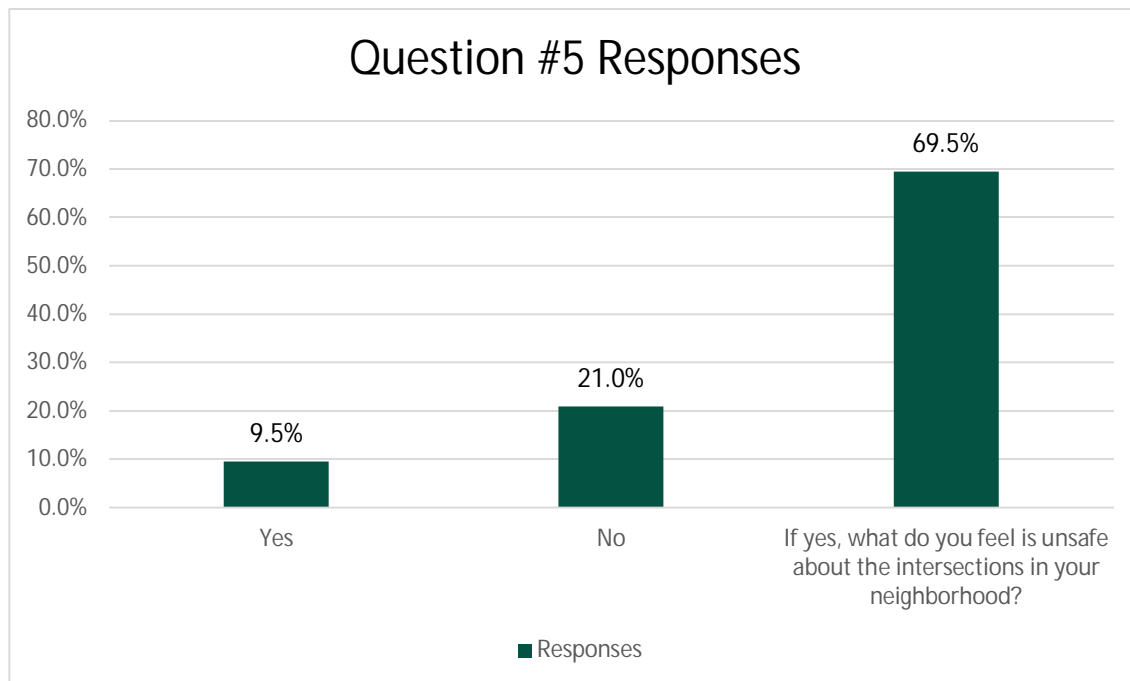


Figure 28 – Survey Question #5 Responses



"People are park on both sides of narrow streets and way too close to the intersection."

"Intersections are dark."

"There are so many cars parked on the roads I cannot see the oncoming traffic from most stop signs."

"I think a lot of people don't stop at intersections or run red lights. It's really hard for pedestrians especially. Maybe a way to make intersections slower, or pop out to get people's attention to stop or go slower."

"People parking on both sides of the street. Parking right next to intersections, obstructing views, and parking on curves."

"You can't see to turn around cars safely. Also at some it would be easy to have a car backed up into you as you turn because so many cars are next to their driveway so they don't see you driving down the road trying to turn by their house."

Figure 29 – Survey Question #5 Key Comments

79% of the respondents had some type of safety concern at neighborhood intersections. 73 respondents left a comment on Question #5 related to what they felt was unsafe about intersections in their neighborhoods. The following is a summary of those safety concern comments.

- 35 comments suggested that cars parked on-street pose a safety concern.
- 26 comments explicitly state that cars parked too close to the intersection reduce visibility.
- 22 comments mentioned visibility as a concern.
 - Four comments mentioned overgrown trees and bushes as the cause of visibility issues.
- 11 comments expressed concern with signage in the area.
 - Ten of those comments identified corners and intersections with no stop signs as being a major concern.
 - One comment stated that the intersection of 6200 South and Impressions Drive needs a traffic signal.
- Eight comments expressed concerns with vehicles speeding on neighborhood streets.
- Seven comments identified cars running stop signs and lighting as a safety concern.
 - One comment pointed to 4420 West as an area of concern.
- Six comments pointed to the built environment as a source of concern. Specific comments include:
 - Curvature of roads make 2-way stops dangerous.
 - The cement blocks at 4620 South and 5215 West are scuffed up from people hitting them and could pose a hazard in the winter.
 - Narrow streets can't accommodate larger vehicles like trucks.
 - Concerns with visibility issues at the intersections of:
 - 4015 West and 5780 South
 - 4300 West 4700 South
 - Trowbridge Way and 6200 South



Question #6

The survey asked residents if they had any additional comments about on-street parking with the following question. Figure 30 displays the key comments associated with the survey responses for Question #6.

QUESTION #6: Do you have any other comments about on-street parking in Kearns Metro Township?

"I believe we have too many cars parked on the roads. I feel it's dangerous with blocked view of kids and animals."

"Concert traffic and parking is a problem in some neighborhoods."

"There's a lot of areas in Kearns where the neighborhood is older and the streets are really narrow. I think it would be super beneficial if you were only allowed to park on one side."

"More parking is better... unless it makes the road dangerous."

"Most of the cars parked in yards don't run and aren't licensed. We should encourage people to clean up their junk so they can pull off the roads to increase visibility, thus protecting our kids, not turning every road into a single lane, and making our neighborhoods look cleaner and more open."

Figure 30 – Survey Question #6 Key Comments

35 respondents left a comment on Question #6. The following is a summary of those comments

- Seven respondents suggested that there were too many cars parking on the street in their neighborhood.
 - Of these respondents, two expressed their distaste for seeing neighbors parking vehicles in their yards.
- Four comments expressed interest in stronger enforcement of winter parking rules, and four expressed interest in stronger enforcement of other existing parking rules.
- One commenter stated, "people are going to park where they park."
- Five comments stated they have noticed inoperable or unlicensed vehicles using on-street parking in their neighborhood.
- Three commenters stated that vehicles using on-street parking blocks their driveway or mailbox.
- Five comments expressed interest in new parking regulations or other changes, such as:
 - No overnight parking
 - Limiting the number of cars allowed per house
 - Limiting parking to one side of the road
 - No parking within 5' of a driveway
 - Cars parking in the same direction as traffic

Community Outreach Summary

The online survey received 229 comments across the six survey questions from 108 respondents. In addition to the survey responses, an additional 114 comments were left on the Facebook posts advertising the survey. Those comments can also be found in Appendix B.



Based on the survey responses and comments the following key observations and takeaways were found and should be considered when developing recommendations related to on-street parking changes in the Kearns Metro Township.

- 62% of respondents do not park in their yards.
- 75% of respondents were in favor of eliminating on-street parking that impedes views near intersections.
- Respondents left 32 comments expressing interest in stronger enforcement of existing parking rules, 13 of which were requests for stronger enforcement of winter parking rules. This included better signage to advertise the parking rules.
- Respondents left 86 comments across the survey expressing safety concerns related to on-street parking and neighborhood intersections in the Kearns Metro Township.
- 60% of respondents are in favor of eliminating on-street parking on one side of the roadway for narrow roads.
- Multiple comments expressed the need to limit the types of vehicles that can park on the street including RVs, large trucks with trailers, semi-trucks, and inoperable vehicles.



8. Recommendations

Based on the technical analysis and public engagement performed as part of the Kearns On-Street Parking Study, a list of recommendations was developed to be considered for implementation in the Metro Township. These recommendations are summarized in this section of the report. Included with each recommendation is a description of the recommendations, the purpose behind the recommendation, items considered in the development of the recommendation, the specifics of the recommendation, performance measures to track the effectiveness of the recommendation, cost of implementation, community support, and potential barriers to implementation. The proposed recommendations are as follows:

- No parking clear zones at residential intersection
- On-street parking allowances based on street widths
- Increase signage related to winter parking regulations
- Eliminate rolled curbs
- More accessible parking regulations
- Increase parking enforcement efforts
- Parking code changes
- Monitoring of parking performance measures

8.1. No Parking Clear Zones at Residential Intersections

This recommendation involves creating No Parking Clear Zones at all residential intersections. The following subsections detail the purpose, considerations, recommendations, performance measures, cost, community support, and barriers for this recommendation.

Purpose

The purpose of No Parking Clear Zones at residential intersections is to improve driver visibility, improve safety, and reduce crashes at residential intersections by eliminating vehicles parked within a specific distance from the intersection.

Considerations

There are multiple methods or approaches that can be taken when determining no parking clear zones at residential intersections. The following approaches were evaluated when determining the appropriate No Parking Clear Zone distances:

- Clear Zones calculated based on the American Association of State Highway and Transportation Officials (AASHTO) intersection sight distance (ISD) triangle calculations as found in the “Policy on Geometric Design of Highways and Streets” manual (Green Book)
- Clear Zones calculated based on a modified version of the ISD triangle calculations as found in the Green Book with the assumption that vehicles are stopped at the intersection
- Review similar clear zones as found in other local jurisdictions

The resulting No Parking Clear Zone curb length calculations from using the Green Book ISD triangle approach and the modified ISD triangle approach are shown in Table 8.

Table 8 – No Parking Clear Zone Alternatives

Description	No Parking Clear Zone (Curb Length)							
	Green Book ISD Triangle				Modified ISD Triangle**			
	Major Road		Minor Road		Major Road		Minor Road	
	Left	Right	Left	Right	Left	Right	Left	Right
Stop Controlled Neighborhood Roads*	155	120	15	15	105	55	5	5
Yield Control Neighborhood Roads*	260	240	80	80	135	60	5	5
Not Controlled Neighborhood Roads*	95	95	105	105	35	20	5	5

*All neighborhood roads calculated to be 25 mph

**Modified the AASHTO standards to assume the vehicle is stopped at the intersection (5 ft Setback)

Multiple local jurisdictions have parking codes that prohibit parking, in some form, within 30 feet of an intersection including Utah State Code (41-6a-1401), Salt Lake County Code (11.20.050), Salt Lake City Code (12.56.440), and Kearns Metro Township Code (11.20.050). Figure 31 shows the parking that would be removed or prohibited to create 30-foot Clear Zones.

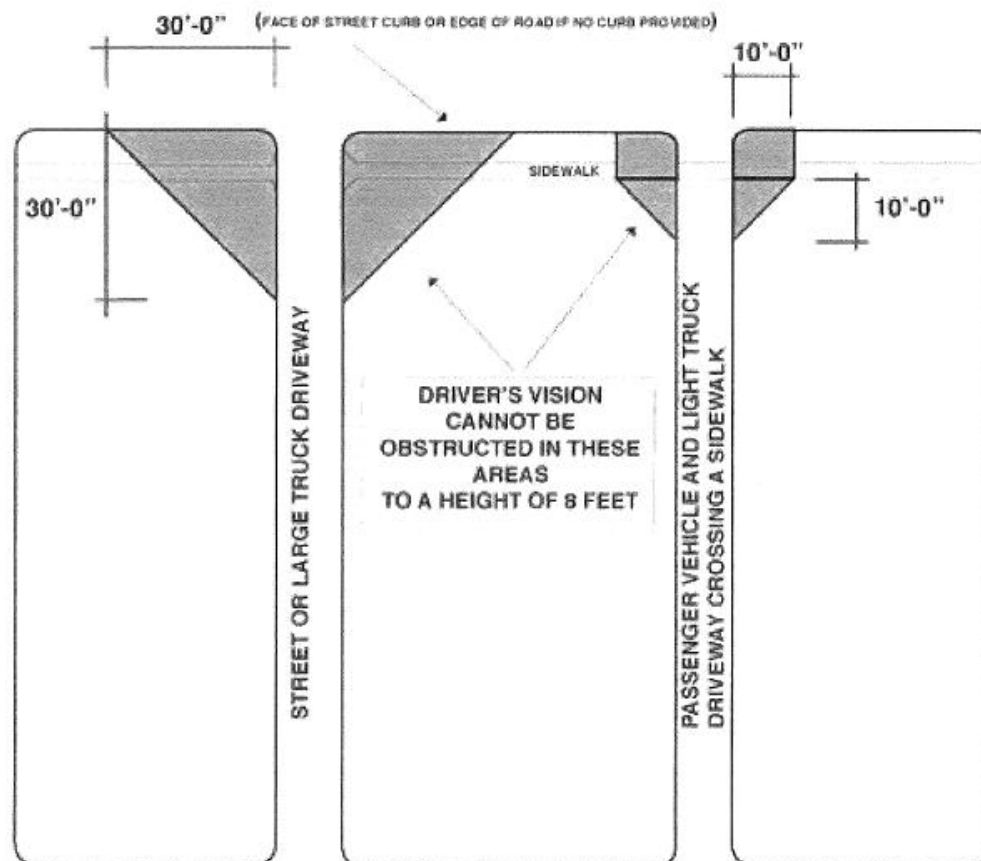


Figure 31 – 30' Intersection Clear Zone Example



Based upon a review of the various approaches used to determine no parking clear zones at residential intersections and how that would affect on-street parking within the study areas, it was determined that the most feasible approach would be to implement a recommendation similar to those found in other local jurisdictions.

Recommendations

The following recommendations should be considered in relation to creating no-parking clear zones at residential intersections:

- Eliminate parking within 30 feet of residential intersection corners
- Implement painted red curb marking where parking is prohibited
- Update the existing Municipal Code (11.20.050) to ensure all residential intersection corners are included

Figure 32 illustrates the locations that no parking clear zones would be found within the study areas.



Figure 32 – No Parking Areas using 30' Clear Zones



Performance Measures

The following performance measures should be tracked to evaluate the effectiveness of this recommendation:

- Historical crash data at intersections
- Parking compliance and citations in no parking clear zones
- Community complaints about on-street parking within the study areas

Cost

The following time and materials costs are associated with this recommendation:

- Painting of curb - \$0.40/ft
 - Approximate total length of painted curb – 37,800 ft
- Approximate total cost - \$15,200

Community Support

As part of the public engagement process the community was asked to identify potential recommended changes they would like to see implemented. One of these recommended changes was to eliminate on-street parking that impede views near intersections. Of those that responded to the survey, 74.8% were in favor of this recommendation.

Barriers

With the current municipal code (11.20.050) already in place, the main implementation barrier would be ensuring funding was available. The current code restricting on-street parking within 30 feet of traffic control devices at the side of a roadway, this recommendation would just expand the code to include all intersection corners even those without a traffic control device. Proper enforcement of the parking restrictions should also be considered a barrier.

8.2. On-Street Parking Allowances Based on Street Widths

This recommendation involves determining on-street parking allowances based on the width of the street. The following subsections detail the purpose, considerations, recommendations, performance measures, cost, community support, and barriers for this recommendation.

Purpose

The purpose of determining on-street parking allowances based on street widths is to improve the safety and functionality of the residential streets by ensuring that on-street parking is aligned with actual street widths. This will ensure that sufficient width is available to accommodate through traffic, parked vehicles, bicyclists, and pedestrians.

Considerations

As seen in the study area when on-street parking is allowed on narrow street widths this can lead to vehicles parking on the sidewalk and insufficient width for vehicles traveling in opposite directions to pass each other. This results in safety issues for pedestrians, bicyclists, and vehicles. A review of other local jurisdictions' parking code was performed to evaluate their approach to on-street parking allowances based on street widths. Sandy City (Sec.14-7-6) and Salt Lake City (12.56.480) have identical municipal code related to prohibiting parking on narrow streets. Both city codes read as follows:



No person shall park a vehicle:

1. On any public street or alley where the width of the roadway is less than 20 feet.
2. On the south or east side of any public street or alley where the width of the roadway is over 20 feet, but less than 30 feet unless otherwise directed by traffic control devices.

Based upon a review of these local jurisdiction municipal codes with WFRC and MSD staff (planners and engineers) it was determined that a similar on-street parking allowance based on street width would be appropriate for the Kearns Metro Township. During this review it was mentioned that a width of 32 feet would be needed to allow on-street parking on both sides of the street to meet the United Fire Authority (UFA) preferences.

Recommendations

The following recommendations should be considered in determining on-street parking allowances based on street widths:

- No on-street parking allowed for streets less than 20 feet wide
- No on-street parking allowed on the south or east side of the street for street widths between 20 to 32 feet unless otherwise directed by traffic control devices
- On-street parking can be allowed on both sides of the street for street widths greater than 32 feet unless otherwise directed by traffic control devices
- Implement painted red curb marking and "No Parking" signs where on-street parking is prohibited
- Update the existing Municipal Code (11.20.050) to include parking restrictions based on street widths

Figure 33 illustrates the locations where on-street parking would be prohibited within the study areas based on current street widths. Available street widths were measured from edge of asphalt to edge of asphalt. The intersection clear zones are also shown in this figure.

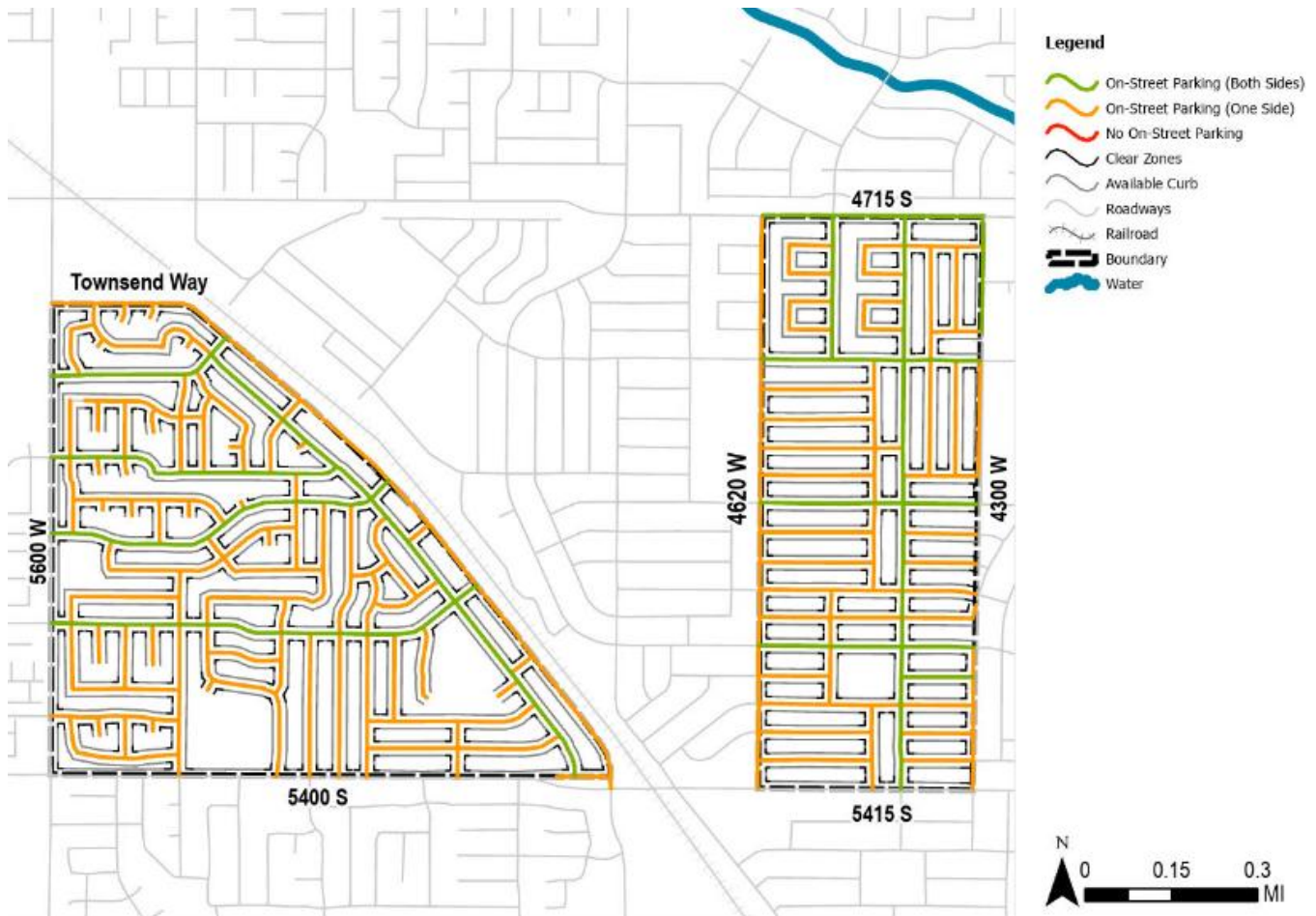


Figure 33 – No Parking Areas using Roadway Widths

Performance Measures

The following performance measures should be tracked to evaluate the effectiveness of this recommendation:

- Historical crash data for the study areas
- Parking compliance and citations on segments with no parking locations
- Community complaints about on-street parking within the study areas
- Updated parking supply, demand, and occupancy analysis after implementation

Cost

The following time and materials costs are associated with this recommendation:

- Installing “No Parking” signs - \$2.16/ft
 - Assumes signs cost \$540 and are placed every 250 feet
- Painting of curb - \$0.40/ft
 - Approximate total length of no parking areas - 114,400 ft
- Approximate total cost (signs and paint) - \$292,864



Community Support

As part of the public engagement process, the community was asked to identify potential recommended changes they would like to see implemented. One of these recommended changes was to eliminate parking on one side of the roadway for narrow roads. Of those that responded to the survey, 60.2% were in favor of this recommendation.

Barriers

The major barriers to implementing on-street parking allowances based on street widths are the cost of implementation, the potential that the on-street parking supply along certain roadway segments cannot accommodate the on-street parking demand, and the proper enforcement of the parking restrictions.

To identify the potential impact that eliminating on-street parking for narrow roadways would have on overall parking supply and demand, the peak parking occupancy was recalculated with the recommended on-street parking restriction implemented in the study areas. This involved relocating the observed vehicles from roadway segments where on-street parking would be eliminated to the nearest segment where on-street parking would be maintained. Figure 34 and Figure 35 shows the recalculated peak time of day occupancy for both the weekday and weekend with on-street parking restrictions included. Additional occupancy figures and data are provided in Appendix C. It should be noted that various segments would exceed 100% occupancy resulting in residents needing to adjust their on-street parking behaviors should this recommendation be implemented.

Additional occupancy figures showing the impact of eliminating parking based on street widths and relocating all vehicles parked in yards to the nearest on-street parking location are provided in Appendix D. This would be considered the worst-case scenario when it comes to parking occupancy rates found in the study areas.





Figure 35 – Weekend Peak Occupancy (5:00 PM - 7:00 PM) with Parking Restrictions

8.3. Increase Signage Related to Winter Parking Regulations

This recommendation involves increasing the signage related to winter parking regulations throughout the study areas. The following subsections detail the purpose, considerations, recommendations, performance measures, cost, community support, and barriers for this recommendation.

Purpose

The purpose of increased signage related to winter parking regulations is to increase the compliance of vehicle owners properly parking during snow events or when snow is present on the roadways. Proper compliance with winter parking regulations will allow snowplows to properly maintain the roadways during the winter months resulting in improved safety for snowplow drivers and all road users.

Considerations

The Winter Parking Enforcement and Snow Removal Guide (2020/2021) states that when vehicles are parked on roadways when it is currently snowing or there is snow on the roadway, snowplows cannot properly clear the roads. Per Ordinance 11.20.130, it is illegal to park on the street during the months of November through April when there is an active snowstorm or there is snow on the street.



Recommendations

The following recommendation should be considered related to increased signage related to winter parking regulations:

- Install at least one sign per block face stating the winter parking restrictions.

Performance Measures

The following performance measures should be tracked to evaluate the effectiveness of this recommendations:

- Historical crash data that is snow related
- Parking compliance and citations during winter months
- Community complaints about winter parking within the study areas

Cost

The following time and materials costs are associated with this recommendation:

- Installing "No Parking" signs - \$0.70/ft
 - Assumes signs cost \$540 and are placed every 800 feet
 - Approximate total length of curb – 114,400 ft
- Approximate total cost - \$80,080

Community Support

As part of the public engagement process the community was asked to identify potential recommended changes they would like to see implemented. One of these recommended changes was to make parking rules and regulations easily available to all residents. Of those that responded to the survey, 52.4% were in favor of this recommendation.

During the survey several residents provided comments related to winter parking. The following is a summary of a few of those comments:

- Signs should be posted at regular intervals to remind residents to remove cars from roadsides during snowstorms.
- Winter parking rules should be enforced and better advertised

Barriers

The primary implementation barriers to increasing signage of winter parking regulations are funding the additional signs and proper enforcement of those regulations. This recommendation is reliant on an increase of enforcement efforts and should only be implemented if the issue is still present after implementing increased enforcement efforts.

8.4. Eliminate Rolled Curbs

This recommendation involves eliminating all rolled curbs along residential roadways when possible. The following subsections detail the purpose, considerations, recommendations, performance measures, cost, community support, and barriers for this recommendation.



Purpose

The purpose of eliminating rolled curbs in residential neighborhoods is to reduce the number of vehicles using the rolled curb to park on the sidewalk and creating obstacles for pedestrians. Eliminating rolled curbs will improve the environment and overall safety of pedestrians using the sidewalk. Figure 36 and Figure 37 are examples of vehicles using the rolled curb to park on the park strip and the sidewalk.



Figure 36 – Vehicles using the Rolled Curb to Park on the Park Strip



Figure 37 – Vehicles Using the Rolled Curb to Park on the Sidewalk



Considerations

During the various times that on-street parking occupancy data was collected, the percent of vehicles parked on the sidewalk ranged from 25% to 35%. The high percentage of vehicles parked on the sidewalk is due to the rolled curb found within the study areas allowing a vehicle's wheels to easily mount the curb. Vehicles parked on the sidewalk, even partially, creates barriers for active transportation because the sidewalk is inaccessible.

Rolled curbs are found extensively throughout the study areas. It wouldn't be feasible to replace all of the existing rolled curbs within the study areas or the Metro Township with a single project. This improvement would likely occur over several years and would be part of other projects in the area as funding becomes available.

Recommendations

The following recommendations should be considered related to eliminating rolled curbs in residential neighborhoods:

- Include curb replacement with all future sidewalk and roadway projects as funding becomes available
- Update municipal code to eliminate the use of rolled curb including new development or the replacement of existing rolled curb

Performance Measures

The following performance measures should be tracked to evaluate the effectiveness of this recommendation:

- Amount of rolled curb replaced, in feet, by year
- Parking compliance and citations related to vehicles parking on sidewalks

Cost

The following time and materials costs are associated with this recommendation:

- Removal of existing curb and gutter - \$8.00/ft
- Installation of new curb and gutter - \$35.00/ft

Community Support

As part of the public engagement process the community was asked to identify potential recommended changes they would like to see implemented. One of these recommended changes was to replace rolled curb to reduce vehicles parking on sidewalks. Of those that responded to the survey, 20.4% were in favor of this recommendation.

Barriers

The major implementation barrier related to eliminating rolled curb with standard vertical curb is acquiring the funding necessary to include this improvement with other roadway and sidewalk improvements projects in the study areas.

8.5. More Accessible Parking Regulations

This recommendation involves making parking regulations more accessible to Metro Township residents. The following subsections detail the purpose, considerations, recommendations, performance measures, cost, community support, and barriers for this recommendation.



Purpose

The purpose of making parking regulations more accessible to Metro Township residents is to ensure the residents of the community are informed about what the existing parking regulations are and can easily find those regulations should any changes be made in the future. With parking regulation more accessible, compliance to those parking regulations will likely increase over time as residents become more familiar with them.

Considerations

As parking regulations change, it is important for the community to have easy access to those regulations. To expect compliance to the parking regulations, the community must be aware of any new or changing regulations. Based on the diversity of the residents found within the study areas, a multiple pronged approach would be necessary to make parking regulations more accessible to all residents.

Salt Lake City has a Parking Enforcement FAQ webpage that summarizes all the information vital for residents to understand related to parking with the city. This webpage includes graphics, educational videos, and links to other resources.

Recommendations

The following recommendations should be considered related to making parking regulations more accessible to all Metro Township residents:

- Create a one-page informational parking guide that can be distributed to residents.
- Develop a parking regulations informational webpage

Performance Measures

The following performance measures should be tracked to evaluate the effectiveness of this recommendation:

- Overall parking citations by year
- Number of clicks or visits to parking regulation webpage

Cost

The only costs associated with this recommendation are related to the allocation of staff time, resources, and availability.

Community Support

As part of the public engagement process the community was asked to identify potential recommended changes they would like to see implemented. One of these recommended changes was to make parking rules and regulations easily available to all residents. Of those that responded to the survey, 52.4% were in favor of this recommendation.

During the survey several residents provided comments related to the accessibility of parking regulations. The following is a summary of a few of those comments:

- I think it would be good if people could be more aware that they should avoid parking in a way that obstructs the sidewalk
- Winter parking rules should be enforced and better advertised



Barriers

The main implementation barrier to making parking regulations more accessible is related to the Metro Township and MSD staff availability and resource allocation to implement the recommendations.

8.6. Increased Parking Enforcement Efforts

This recommendation involves the need for increased enforcement efforts related to parking. The following subsections detail the purpose, considerations, recommendations, performance measures, cost, community support, and barriers for this recommendation.

Purpose

The purpose of increased enforcement efforts related to parking ensures there is greater compliance to parking codes and regulations. The other recommendations and changes found within this report are only effective when enforcement efforts exist to make sure compliance is achieved. Increased enforcement will result in increased driver visibility, improved safety, reduce crashes, and an improved pedestrian environment.

Considerations

Enforcement is the most effective approach to ensuring compliance to parking codes and regulations. Without enforcement there is typically poor compliance to parking codes and regulations. Increased enforcement efforts would need to be coordinated with the Unified Police Department (UPD) to enforce the on-street parking regulations. Local code enforcement staff is responsible for the enforcement of municipal code. This could alternatively be done through public outreach efforts in conjunction with focused enforcement times throughout the year. This could be similar to law enforcement's "Click it or Ticket" campaign.

Recommendations

The following recommendations should be considered related to increased parking enforcement efforts:

- Conduct yearly residential parking enforcement compliance campaigns including public outreach efforts
- Increase winter parking enforcement efforts
- Increase sidewalk parking enforcement efforts

Performance Measures

The following performance measures should be tracked to evaluate the effectiveness of this recommendation:

- Overall parking citations by year

Cost

The only costs associated with this recommendation are related to the allocation of staff time, resources, and availability.

Community Support

Question #3 of the survey asked resident for their input on how to improve on-street parking. Eleven of the respondents expressed that they wanted to see better enforcement of the existing parking rules. This theme of increased or better enforcement was common throughout all survey responses. In total there were 32 responses expressing interest in increased parking enforcement efforts in some form or another.



Barriers

The main implementation barrier to increasing enforcement efforts for parking regulations is related staff availability and resource allocation to implement increased enforcement efforts.

8.7. Parking Code Changes

This recommendation involves changing the existing municipal parking code to ensure parking regulations are clearly defined and enforceable. The following subsections detail the purpose, considerations, recommendations, performance measures, and cost for this recommendation.

Purpose

The purpose of updating the municipal parking code for the Metro Township is to ensure that the latest best practices related to on-street parking are being utilized and are enforceable in an effort to eliminate any ambiguity with existing parking codes and reinforce on-street parking rules.

Considerations

Based upon the recommendations highlighted in this section of the report and a review of existing local municipal parking codes, example text is proposed to be considered for new or modified parking codes.

No Parking Clear Zones at Residential Intersections

Update the existing Municipal Code (11.20.050) to eliminate parking within 30 feet of residential intersection corners. The follow text is an example of how the existing code could be modified:

7. Within thirty feet of any flashing beacon, traffic control device located at the side of a roadway or on the corners of residential intersections.

On-Street Parking Based on Street Widths

Update the existing Municipal Code (11.20.050) or create new municipal code to clarify on-street parking allowances based of street widths. The follow text is an example of what the new parking code could be:

No person shall park a vehicle:

1. On any public street or alley where the width of the roadway is less than 20 feet.
2. On the south or east side of any public street or alley where the width of the roadway is over 20 feet, but less than 32 feet unless otherwise directed by traffic control devices.

Overnight Parking on Non-Motorized Vehicles

New municipal code should be added to eliminate overnight parking of non-motorized vehicles. The follow text is an example of what the new parking code could be:

It shall be unlawful to park, place, store, or otherwise leave any non-motorized vehicle or any unmounted or unattached camper, boat, boat trailer, utility trailer, camp trailer, or any other type of trailer, or any automobile hulk, on any public street, alley, sidewalk, or right-of-way overnight.

Parking of Inoperable Vehicles

New municipal code should be added to eliminate the parking of inoperable vehicles on any public street. The follow text is an example of what the new parking code could be:



It shall be unlawful to park any inoperable motorized vehicle on any public street, alley, sidewalk, or right-of-way at any time.

Recommendations

The following recommendations should be considered related to parking code changes:

- Update existing parking code to include the recommendations changes as found in this section of the report
- Add new code related to no overnight parking of non-motorized vehicles
- Add new code related to the elimination of on-street parking of inoperable vehicles

Performance Measures

No performance measures are recommended to be tracked as part of this recommendation.

Cost

The only costs associated with this recommendation are related to the allocation of staff time, resources, and availability.

8.8. Monitoring Parking Performance Measures

This recommendation involves monitoring parking performance measures to determine a particular recommendation's effectiveness. The following subsections detail the purpose, considerations, recommendations, cost, and barriers for this recommendation.

Purpose

The purpose of monitoring parking performance measures is to evaluate the effectiveness of each recommendation and determine if future changes to on-street parking are needed within the Metro Township.

Considerations

The following performance measures should be considered to evaluate the effectiveness of the recommendations provided within this report and to evaluate the overall on-street parking environment within the study areas:

- Historical crash data for the study areas
- Historical crash data at intersections
- Historical crash data that is snow related
- Parking compliance and citations in No Parking Clear Zones
- Parking compliance and citations on segments with "no parking" locations
- Parking compliance and citations during winter months
- Parking compliance and citations related to vehicles parking on sidewalks
- Community complaints about on-street parking within the study areas
- Community complaints about winter parking within the study areas
- Updated parking supply, demand, and occupancy analysis after implementation
- Amount of rolled curb replaced, in feet, by year
- Overall parking citations by year
- Number of clicks or visits to parking regulation webpage

Recommendations

The following recommendations should be considered when monitoring parking performance measures:



- Perform yearly evaluations of parking performance measures related to historical crash data, number of citations, complaints, curb replacement, and website visits.
- Perform updated parking supply, demand, and occupancy analysis approximately 2-3 years after the implementation of the parking recommendations found in this report.

Cost

The only costs associated with this recommendation are related to the allocation of staff time, resources, and availability.

Barriers

The major barrier to monitoring parking performance measures is the availability of staff to perform the additional work that would be required. If municipal staff is unavailable to perform the monitoring of parking performance measures, then additional funding resources would need to be allocated to this effort.

8.9. Other Considerations

This subsection looks at other items to consider when evaluating changing on-street parking within the study areas. These other considerations include general recommendations, equity impacts, and the need for additional public engagement.

General Recommendations

General active transportation, urban design, and land use-related recommendations were developed as a part of the Kearns On-Street Parking Study. These recommendations are intended to build on the parking, safety, and land use analysis work performed for the study and complement the code, signage, educational, and ongoing monitoring recommendations presented in this section of the report. A technical memorandum was prepared to provide more detail related to these general recommendations that could be considered throughout the township and not just the study areas identified as part of this study. Appendix E includes the technical memorandum summarizing the following general recommendation topics:

- Corner Treatments
- Parking Restrictions at Bends
- Parking Restrictions on Bikeways
- Town Center and Parking
- Parking on major Streets and at Destinations
- Pedestrian Realm Reconfigurations
- Heath Avenue Focus

Equity Impacts

The MSD and Kearns Metro Township should consider equity-related impacts of parking policies and enforcement going forward. Equity implies that policies and other actions affect people differently depending on their demographic characteristics, and that an individual or group may need to experience or receive something different (not equal) in order to maintain fairness and access. Equity in this context can be complex: On one hand, the Kearns Parking Study areas' larger households have more transportation demand, which leads to more vehicle ownership, so policies restricting parking will affect these households more and potentially more negatively than smaller households. On the other hand, people without cars in the community, using other modes such as bicycling, walking, and transit, can be negatively affected by a lack of parking enforcement such as cars on the



sidewalk, and by a dominance of cars in the public realm of the street, such as the blocking of sight lines by parked cars close to corners. There are not necessarily simple solutions to these equity challenges, but they should be considered in policy and enforcement.

Public Engagement

Following the review of this report, additional public engagement is recommended to determine which of the recommendations from this report should be implemented and when those recommendations are implemented. The equity impacts and public support of each parking recommendation should be discussed with the residents of the Metro Township.

8.10. Recommendations Summary

Table 9 provides a summary of the recommendations presented in this section of the report.



Table 9 – Kearns On-Street Parking Study Recommendations

Recommendation Title	Recommendations	Purpose	Implementation Timeframe	Cost	Performance Measures	Barriers
No Parking Clear Zones at Residential Intersection	30 ft “No Parking” Clear Zones at Intersections Painted Red Curb in Clear Zones Updated Parking Code	Improve Visibility Improve Safety Reduce Crashes	Short Term	Red Curb - \$0.40/ft	Historical Crash Data – Intersection Related Compliance & Citations Community Complaints	Implementation Cost Proper Enforcement
On-Street Parking Allowances based on Street Widths	No On-Street Parking on Streets Less than 20 ft Wide No On-Street Parking on the South or East Side of Streets Between 20 to 32 ft Wide On-Street Parking on Both Sides of Streets Wider than 32 ft Implement Red Curbs and “No Parking” Signs Update Parking Code	Improve Safety Improve Street Function Accommodate All Users	Medium Term	Signs - \$2.16/ft Red Curb - \$0.40/ft	Historical Crash Data Compliance & Citations Community Complaints Updated Supply, Demand, and Occupancy Analysis	Implementation Cost Demand Exceeds New Supply Proper Enforcement
Increased Signage Related to Winter Parking Regulations	Install at Least One Winter Parking Regulation Sign per Block	Increase Compliance Improve Safety Road Maintenance	Short Term	Signs - \$0.70/ft	Historical Crash Data – Snow Related Compliance & Citations Community Complaints	Implementation Cost Proper Enforcement



Kearns On-Street Parking Study

Final Report

Recommendation Title	Recommendations	Purpose	Implementation Timeframe	Cost	Performance Measures	Barriers
Eliminate Rolled Curbs	Include Curb Replacement with Future Projects Update Municipal Code to Eliminate Future Use of Rolled Curb	Reduce Sidewalk Parking Improve Pedestrian Environment Improve Safety	Long Term	Remove Curb - \$8.00/ft Install Curb - \$35.00/ft	Rolled Curb Replacement (ft/year) Sidewalk Parking Compliance & Citations Community Complaints	Implementation Cost
More Accessible Parking Regulations	Create a One-Page Informational Parking Guide Develop a Parking Regulations Webpage	Inform Residents Greater Parking Compliance	Short Term (Information Guide) Medium Term (Webpage)	Staff Time	Parking Citations by Year Number of Website Clicks or Visits	Staff Resources & Availability
Increased Enforcement Efforts	Conduct Yearly Parking Enforcement Campaigns with Public Outreach Increase Winter Parking Enforcement Increase Sidewalk Parking Enforcement	Greater Parking Compliance Improve Visibility Improve Safety Improve Pedestrian Environment	Medium Term	Staff Time	Parking Citations by Year	Staff Resources & Availability
Parking Code Changes	Update Parking Code Based on Recommendations Add Code Related to Non-Motorized Vehicle Parking Add Code Related to Inoperable Vehicle Parking	Update Code Ensure Parking Best Practices are Utilized and Enforceable Reinforce Rules and Regulations	Short Term	Staff Time	NA	Staff Resources & Availability
Monitoring Parking Performance Measures	Perform Yearly Evaluations of Parking Performance Measures Update Parking Supply, Demand, and Occupancy Analysis 2-3 Years After Implementation	Evaluate Effectiveness of Parking Recommendations	Medium/Long Term	Staff Time	NA	Staff Resources & Availability Implementation Cost



APPENDIX A

Parking Occupancy Data and Figures



Date 12/7/2021
Counters Mitchell Hadfield, Alison Brown

Notes
Data collection bins show 2 hours allotted for each collection period, however, it took about 2.5 hours for each collection period
If a boat, trailer, or RV was parked on the street or in a yard it was counted as a vehicle
No vehicle was double counted in data collection. Each column shows separate vehicles. If a car was on the sidewalk and the street it was counted as a sidewalk blocking vehicle.

Collection Time: 7:00 AM - 9:00 AM

Section 1						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Townsend Way 5600 W - Planada Way - South	5	0	6	Y	
2	5600 W Townsend Way - Planada Way – East	0	0	0	N	
3	Heath Ave Townsend Way - Planada Way -- South	10	1	9	N	
4	Heath Ave Townsend Way - Planada Way – North	8	0	7	N	
5	Planada Way 5600 W - 5245 W – North	14	0	3	N	
6	Planada Way 5600 W - 5245 W – South	10	0	3	N	
7	5575 W Joaquin St – Planada Way - West	5	0	0	N	
8	5575 W Joaquin St – Planada Way - East	4	0	0	N	
9	Joaquin St 5575 W – Heath Ave - North	1	0	0	N	
10	Joaquin St 5575 W – Heath Ave - South	0	0	0	N	
Section 2						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Planada Way – Westslope Dr – East	0	0	0	N	
2	Jeremiah Dr Jeremiah Dr – Westslope Dr – West	2	0	1	N	
3	Jeremiah Dr Jeremiah Dr – Westslope Dr – East	1	0	2	N	
4	5480 W Jeremiah Dr – Westslope Dr - West	2	0	0	N	
5	5480 W Jeremiah Dr – Westslope Dr – East	1	0	0	N	
6	5415 W Planada Way – Westslope Dr - West	7	0	2	N	
7	5415 W Planada Way – Westslope Dr – East	7	0	4	N	
8	Jeremiah Dr Jeremiah Dr – 5415 S – North	3	0	1	N	
9	Jeremiah Dr Jeremiah Dr – 5415 S – South	5	1	0	N	
10	Westslope Dr 5600 W – 5415 W - North	14	0	2	Y	
11	Westslope Dr 5600 W – 5415 W - South	9	0	5	N	
Section 3						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Jeremiah Dr 5415 W – La Brea St - North	1	0	0	N	
2	Jeremiah Dr 5415 W – La Brea St - South	1	0	0	N	
3	La Brea St Farah Dr – Westslope Dr - West	6	0	5	N	
4	La Brea St Farah Dr – Westslope Dr - East	2	0	2	N	
5	Farah Dr Planada Way – Westslope Dr - West	4	0	3	N	
6	Farah Dr Planada Way – Westslope Dr – East	4	0	7	N	
7	Stockton St Heath Ave – Westslope Dr - West	5	0	4	Y	
8	Stockton St Heath Ave – Westslope Dr – East	4	0	0	N	
9	Westslope Dr La Brea St – Heath Ave - North	9	0	4	N	
10	Westslope Dr La Brea St – Heath Ave – South	13	0	3	N	
11	Heath Ave Planada Way – Westslope Ave - West	7	0	6	Y	
12	Heath Ave Planada Way – Westslope Ave – East	10	0	3	N	

Section 4						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Westslope Dr – Mountain Men Dr – East	0	0	0	N	
2	Jeremiah Dr Westslope Dr – Mountain Men Dr - West	6	0	4	N	
3	Jeremiah Dr Westslope Dr – Mountain Men Dr – East	2	1	0	N	
4	5450 W Colter Dr – Mountain Men Dr - West	1	0	2	N	
5	5450 W Colter Dr – Mountain Men Dr – East	1	0	4	N	
6	Colter Dr Jeremiah Dr – Mountain Men Dr - North	8	0	4	N	
7	Colter Dr Jeremiah Dr – Mountain Men Dr - South	8	0	5	N	
8	Mountain Men Dr 5600 W – Tuscan St - North	9	0	5	N	
9	Mountain Men Dr 5600 W – Tuscan St - South	12	0	5	N	
10	Campbell Dr Westslope Dr – 5115 S - West	1	0	0	N	
11	Campbell Dr Westslope Dr – 5115 S - West	0	0	2	N	
12	Heath Ave Westslope Dr – Mountain Men Dr - West	1	1	0	N	
13	Heath Ave Westslope Dr – Mountain Men Dr - East	1	0	0	N	
Section 5						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Mountain Men Dr – Henley Dr – East	0	0	0	N	
2	Lewis Clark Dr Mountain Men Dr – Campbell Dr - North	7	1	7	N	
3	Lewis Clark Dr Mountain Men Dr – Campbell Dr – South	15	0	9	N	
4	Bailiff Dr Henley Dr – 5420 W - North	7	0	6	N	
5	Bailiff Dr Henley Dr – 5420 W – South	10	0	3	N	
6	5420 W Lewis Clark Dr – Henley Dr - West	5	1	1	N	
7	5420 W Lewis Clark Dr – Henley Dr – East	1	0	0	N	
8	Henley Dr 5600 W – 5200 W - North	7	0	9	N	
9	Henley Dr 5600 W – 5200 W – South	9	0	10	N	
10	Leprechaun Dr Henley Dr – Jededian Dr - North	4	0	8	Y	
11	Leprechaun Dr Henley Dr – Jededian Dr - South	3	0	4	N	
12	5115 W Mountain Men Dr – Jededian Dr – North	3	1	5	N	
13	5115 W Mountain Men Dr – Jededian Dr – South	8	2	6	N	
14	Shalee St Leprechaun Dr – 5200 S - West	1	0	0	N	
15	Shalee St Leprechaun Dr – 5200 S – East	1	0	0	N	
16	Jededian Dr Mountain Men Dr – Leprechaun Dr - West	5	0	0	N	
17	Jededian Dr Mountain Men Dr – Leprechaun Dr – East	2	0	7	N	
18	Campbell Dr Mountain Men Dr – 5115 S - West	1	0	1	N	
19	Campbell Dr Mountain Men Dr – 5115 S - East	1	0	2	N	

Section 6						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5185 W Mountain Men Dr – Hoopes St - West	4	2	2	N	
2	5185 W Mountain Men Dr – Hoopes St – East	2	0	4	N	
3	Verde St 5185 W – Westwind Way - North	3	0	1	Y	
4	Verde St 5185 W – Westwind Way - South	1	0	0	Y	
5	5160 W Verde St – Hoopes St - West	1	0	0	N	
6	5160 W Verde St – Hoopes St – East	0	0	0	N	
7	5125 S 5160 W – Heath Ave - North	3	0	1	N	
8	5125 S 5160 W – Heath Ave - South	2	0	1	N	
9	Westwind Way Heath Ave – 5125 S – West	7	0	3	N	
10	Westwind Way Heath Ave – 5125 S – East	4	0	1	N	
11	Heath Ave Mountain Men Dr – Hoopes St - West	4	0	4	N	
12	Heath Ave Mountain Men Dr – Hoopes St – East	2	0	0	N	
13	Tuscan St – West	2	0	2	N	
14	Tuscan St – East	3	1	3	N	
Section 7						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Henley Dr – Trident Dr East	0	0	0	N	
2	Parish Dr Henley Dr – Falstaff Dr West	1	0	2	N	
3	Parish Dr Henley Dr – Falstaff Dr East	4	1	1	Y	
4	Kemp Dr Henley Dr – Falstaff Dr West	2	0	1	N	
5	Kemp Dr Henley Dr – Falstaff Dr East	1	0	1	N	
6	5420 W Henley Dr – Trident Ln	3	0	1	N	
7	5420 W Henley Dr – Trident Ln	3	0	4	N	
8	Falstaff Dr Parish Dr – 5420 W North	6	0	4	N	
9	Falstaff Dr Parish Dr – 5420 W South	4	0	3	N	
10	Trident Dr 5600 W – 5420 W North	3	2	6	N	
11	Trident Dr 5600 W – 5420 W South	12	0	7	N	
Section 8						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Trident Dr – 5400 S East	0	0	0	N	
2	Nautilus Dr Trident Dr – 5420 W North	9	0	3	Y	
3	Nautilus Dr Trident Dr – 5420 W South	10	0	5	Y	
4	5495 W Trident Ln – Nautilus Dr West	2	0	0	Y	
5	5495 W Trident Ln – Nautilus Dr East	3	0	2	N	
6	5420 W Trident Dr – 5400 S - West	3	0	0	N	
7	5420 W Trident Dr – 5400 S – East	0	0	1	N	
8	5400 S 5600 W – 5420 W - North	0	0	0	N	

Section 9						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Leprechaun Ln 5200 S – 5400 S South	7	0	7	N	
2	Leprechaun Ln 5200 S – 5400 S North	2	0	5	N	
3	5240 S 5385 W – Shalee St North	1	0	1	N	
4	5240 S 5385 W – Shalee St South	5	0	5	N	
5	Shalee St 5200 S – Leperchaun Ln West	0	0	1	N	
6	Shalee St 5200 S – Leperchaun Ln East	2	0	2	N	
7	5240 W 5200 S – 5400 S West	5	7	2	N	All on street cars were on curb.
8	5240 W 5200 S – 5400 S East	8	2	2	N	All on street cars were on curb.
9	5200 W 5200 S – 5400 S West	0	3	7	N	
10	5200 W 5200 S – 5400 S East	5	0	3	N	
11	5400 S 5120 W – 5200 W North	0	0	0	N	
Section 10						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5160 W Hoopes St – 5320 S – West	0	4	1	N	
2	5160 W Hoopes St – 5320 S - East	1	0	0	N	
3	5120 W Hoopes St – 5320 S - West	0	4	4	N	
4	5120 W Hoopes St – 5320 S - East	2	0	2	N	
5	Hoopes Cir South of Hoopes St - West	1	0	5	N	
6	Hoopes Cir South of Hoopes St - East	3	1	0	N	
7	Heath Ave Hoopes St – 5320 S - West	0	0	0	N	
8	Heath Ave Hoopes St – 5320 S – East	8	0	5	Y	
9	5320 S 5160 W – Heath Ave - North	2	2	14	N	
10	5320 S 5160 W – Heath Ave - North	4	2	2	N	
11	Charlotte Ave Hoopes St – 5320 S - West	2	1	1	N	
12	Charlotte Ave Hoopes St – 5320 S - East	4	0	0	N	
Section 11						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5160 W 5320 S – 5415 S West	1	0	0	N	
2	5160 W 5320 S – 5415 S East	0	0	0	N	
3	5030 W 5320 S – 5415 S West	1	0	0	N	
4	5030 W 5320 S – 5415 S East	0	0	0	N	
5	Heath Ave 5320 S – 5100 S West	5	0	4	N	
6	Heath Ave 5320 S – 5100 S East	13	0	1	Y	
7	5360 S 5160 W – Heath Ave North	4	0	3	N	
8	5360 S 5160 W – Heath Ave South	2	0	2	N	
9	5415 S 5160 W – Heath Ave North	0	0	0	N	
10	Charlotte Ave Cross St – 5415 S - West	0	2	2	N	
11	Charlotte Ave Cross St – 5415 S - East	8	0	2	N	

Section 12						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5245 W Planada Way – Dead End - West	8	0	2	Y	
2	5245 W Planada Way – Dead End - East	4	0	2	N	
3	Stockton St Heath Ave – 5245 E North	2	0	2	Y	
4	Stockton St Heath Ave – 5245 E South	1	0	0	N	

Collection Time: 2:00 PM - 4:00 PM

Section 1						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Townsend Way 5600 W - Planada Way - South	8	0	12	N	
2	5600 W Townsend Way - Planada Way – East	0	0	0	N	
3	Heath Ave Townsend Way - Planada Way -- South	5	1	6	N	
4	Heath Ave Townsend Way - Planada Way – North	6	0	8	N	
5	Planada Way 5600 W - 5245 W – North	12	0	2	N	
6	Planada Way 5600 W - 5245 W – South	8	0	2	N	
7	5575 W Joaquin St – Planada Way - West	1	0	1	N	
8	5575 W Joaquin St – Planada Way - East	3	0	0	N	
9	Joaquin St 5575 W – Heath Ave - North	1	0	0	N	
10	Joaquin St 5575 W – Heath Ave - South	0	0	0	N	
Section 2						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Planada Way – Westslope Dr – East	0	0	0	N	
2	Jeremiah Dr Jeremiah Dr – Westslope Dr – West	1	0	2	N	
3	Jeremiah Dr Jeremiah Dr – Westslope Dr – East	1	0	3	N	
4	5480 W Jeremiah Dr – Westslope Dr - West	2	0	0	N	
5	5480 W Jeremiah Dr – Westslope Dr – East	1	0	0	N	
6	5415 W Planada Way – Westslope Dr - West	7	0	4	Y	
7	5415 W Planada Way – Westslope Dr – East	6	1	4	N	
8	Jeremiah Dr Jeremiah Dr – 5415 S – North	5	0	3	Y	
9	Jeremiah Dr Jeremiah Dr – 5415 S – South	2	2	2	Y	
10	Westslope Dr 5600 W – 5415 W - North	12	0	1	N	
11	Westslope Dr 5600 W – 5415 W - South	7	0	2	N	
Section 3						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Jeremiah Dr 5415 W – La Brea St - North	1	0	1	N	
2	Jeremiah Dr 5415 W – La Brea St - South	1	0	0	N	
3	La Brea St Farah Dr – Westslope Dr - West	6	0	2	N	Man walking down middle of street
4	La Brea St Farah Dr – Westslope Dr - East	2	0	1	N	
5	Farah Dr Planada Way – Westslope Dr - West	1	0	1	Y	Car parked in middle of road
6	Farah Dr Planada Way – Westslope Dr – East	3	0	4	N	
7	Stockton St Heath Ave – Westslope Dr - West	1	0	1	N	
8	Stockton St Heath Ave – Westslope Dr – East	5	0	1	N	
9	Westslope Dr La Brea St – Heath Ave - North	7	0	3	N	Boats parked in yard
10	Westslope Dr La Brea St – Heath Ave – South	5	0	8	Y	
11	Heath Ave Planada Way – Westslope Ave - West	6	0	5	N	
12	Heath Ave Planada Way – Westslope Ave – East	8	0	0	N	

Section 4						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Westslope Dr – Mountain Men Dr – East	0	0	0	N	
2	Jeremiah Dr Westslope Dr – Mountain Men Dr - West	4	0	2	N	
3	Jeremiah Dr Westslope Dr – Mountain Men Dr – East	4	1	0	N	
4	5450 W Colter Dr – Mountain Men Dr - West	2	1	0	N	
5	5450 W Colter Dr – Mountain Men Dr – East	0	1	0	N	
6	Colter Dr Jeremiah Dr – Mountain Men Dr - North	8	0	4	N	
7	Colter Dr Jeremiah Dr – Mountain Men Dr - South	9	0	6	N	
8	Mountain Men Dr 5600 W – Tuscan St - North	7	0	6	N	
9	Mountain Men Dr 5600 W – Tuscan St - South	6	0	6	N	
10	Campbell Dr Westslope Dr – 5115 S - West	2	0	0	N	
11	Campbell Dr Westslope Dr – 5115 S - West	0	0	0	N	
12	Heath Ave Westslope Dr – Mountain Men Dr - West	1	1	0	N	
13	Heath Ave Westslope Dr – Mountain Men Dr - East	1	0	1	N	
Section 5						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Mountain Men Dr – Henley Dr – East	0	0	0	N	
2	Lewis Clark Dr Mountain Men Dr – Campbell Dr - North	6	1	4	N	
3	Lewis Clark Dr Mountain Men Dr – Campbell Dr – South	7	0	10	N	
4	Bailiff Dr Henley Dr – 5420 W - North	8	0	3	N	
5	Bailiff Dr Henley Dr – 5420 W – South	8	0	2	N	
6	5420 W Lewis Clark Dr – Henley Dr - West	3	0	1	N	
7	5420 W Lewis Clark Dr – Henley Dr – East	1	0	0	N	
8	Henley Dr 5600 W – 5200 W - North	4	0	4	N	
9	Henley Dr 5600 W – 5200 W – South	7	1	2	N	
10	Leprechaun Dr Henley Dr – Jededian Dr - North	10	0	5	Y	
11	Leprechaun Dr Henley Dr – Jededian Dr - South	3	1	3	N	
12	5115 W Mountain Men Dr – Jededian Dr – North	6	2	2	N	
13	5115 W Mountain Men Dr – Jededian Dr – South	9	2	4	N	
14	Shalee St Leprechaun Dr – 5200 S - West	1	0	0	N	
15	Shalee St Leprechaun Dr – 5200 S – East	2	0	0	N	
16	Jededian Dr Mountain Men Dr – Leprechaun Dr - West	4	0	0	N	
17	Jededian Dr Mountain Men Dr – Leprechaun Dr – East	3	0	8	Y	
18	Campbell Dr Mountain Men Dr – 5115 S - West	1	0	2	N	
19	Campbell Dr Mountain Men Dr – 5115 S - East	1	0	0	N	

Section 6						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5185 W Mountain Men Dr – Hoopes St - West	5	0	2	N	
2	5185 W Mountain Men Dr – Hoopes St – East	5	0	6	Y	
3	Verde St 5185 W – Westwind Way - North	2	0	0	N	
4	Verde St 5185 W – Westwind Way - South	1	0	0	Y	
5	5160 W Verde St – Hoopes St - West	1	0	1	N	
6	5160 W Verde St – Hoopes St – East	2	0	0	N	
7	5125 S 5160 W – Heath Ave - North	2	0	2	N	
8	5125 S 5160 W – Heath Ave - South	1	1	0	N	
9	Westwind Way Heath Ave – 5125 S – West	8	0	3	N	
10	Westwind Way Heath Ave – 5125 S – East	4	0	0	N	
11	Heath Ave Mountain Men Dr – Hoopes St - West	2	0	2	N	
12	Heath Ave Mountain Men Dr – Hoopes St – East	1	0	0	N	
13	Tuscan St – West	2	1	0	N	
14	Tuscan St – East	4	0	2	N	
Section 7						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Henley Dr – Trident Dr East	0	0	0	N	
2	Parish Dr Henley Dr – Falstaff Dr West	2	1	1	N	
3	Parish Dr Henley Dr – Falstaff Dr East	3	0	1	Y	
4	Kemp Dr Henley Dr – Falstaff Dr West	2	0	1	N	
5	Kemp Dr Henley Dr – Falstaff Dr East	1	0	0	N	
6	5420 W Henley Dr – Trident Ln	1	1	1	N	
7	5420 W Henley Dr – Trident Ln	2	1	2	N	
8	Falstaff Dr Parish Dr – 5420 W North	5	0	4	N	
9	Falstaff Dr Parish Dr – 5420 W South	4	0	2	N	
10	Trident Dr 5600 W – 5420 W North	1	2	2	N	
11	Trident Dr 5600 W – 5420 W South	13	0	4	Y	
Section 8						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Trident Dr – 5400 S East	0	0	0	N	
2	Nautilus Dr Trident Dr – 5420 W North	9	0	6	Y	
3	Nautilus Dr Trident Dr – 5420 W South	11	0	4	Y	
4	5495 W Trident Ln – Nautilus Dr West	1	0	1	N	
5	5495 W Trident Ln – Nautilus Dr East	2	0	1	N	
6	5420 W Trident Dr – 5400 S - West	2	0	1	N	
7	5420 W Trident Dr – 5400 S – East	0	1	2	N	
8	5400 S 5600 W – 5420 W - North	0	0	0	N	

Section 9						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Leprechaun Ln 5200 S – 5400 S South	2	0	7	N	
2	Leprechaun Ln 5200 S – 5400 S North	9	0	7	N	
3	5240 S 5385 W – Shalee St North	1	0	3	N	
4	5240 S 5385 W – Shalee St South	2	0	6	N	
5	Shalee St 5200 S – Leperchaun Ln West	2	0	3	N	
6	Shalee St 5200 S – Leperchaun Ln East	1	0	1	N	
7	5240 W 5200 S – 5400 S West	8	6	2	N	
8	5240 W 5200 S – 5400 S East	4	6	1	N	
9	5200 W 5200 S – 5400 S West	0	4	2	Y	
10	5200 W 5200 S – 5400 S East	5	0	1	N	
11	5400 S 5120 W – 5200 W North	0	0	0	N	
Section 10						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5160 W Hoopes St – 5320 S – West	1	2	0	N	
2	5160 W Hoopes St – 5320 S - East	2	0	1	N	
3	5120 W Hoopes St – 5320 S - West	3	3	5	N	
4	5120 W Hoopes St – 5320 S - East	2	0	1	N	
5	Hoopes Cir South of Hoopes St - West	1	1	3	N	
6	Hoopes Cir South of Hoopes St - East	4	2	2	N	
7	Heath Ave Hoopes St – 5320 S - West	1	0	0	N	
8	Heath Ave Hoopes St – 5320 S – East	12	0	1	N	
9	5320 S 5160 W – Heath Ave - North	1	2	5	N	
10	5320 S 5160 W – Heath Ave - North	4	3	2	N	
11	Charlotte Ave Hoopes St – 5320 S - West	3	0	0	Y	
12	Charlotte Ave Hoopes St – 5320 S - East	3	0	1	N	
Section 11						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5160 W 5320 S – 5415 S West	2	2	0	N	
2	5160 W 5320 S – 5415 S East	0	0	0	N	
3	5030 W 5320 S – 5415 S West	0	0	0	N	
4	5030 W 5320 S – 5415 S East	0	0	0	N	
5	Heath Ave 5320 S – 5100 S West	3	1	3	N	
6	Heath Ave 5320 S – 5100 S East	10	0	2	N	
7	5360 S 5160 W – Heath Ave North	7	2	1	Y	
8	5360 S 5160 W – Heath Ave South	3	2	2	N	
9	5415 S 5160 W – Heath Ave North	0	0	0	N	
10	Charlotte Ave Cross St – 5415 S - West	1	2	6	N	
11	Charlotte Ave Cross St – 5415 S - East	5	0	1	N	

Section 12						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5245 W Planada Way – Dead End - West	7	0	0	Y	
2	5245 W Planada Way – Dead End - East	1	0	4	N	
3	Stockton St Heath Ave – 5245 E North	2	0	3	N	
4	Stockton St Heath Ave – 5245 E South	1	0	0	N	

Collection Time: 5:00 PM - 7:00 PM

Section 1						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Townsend Way 5600 W - Planada Way - South	6	0	6	N	
2	5600 W Townsend Way - Planada Way – East	0	0	0	N	
3	Heath Ave Townsend Way - Planada Way -- South	7	0	7	N	
4	Heath Ave Townsend Way - Planada Way – North	7	0	6	N	
5	Planada Way 5600 W - 5245 W – North	9	1	5	N	
6	Planada Way 5600 W - 5245 W – South	10	0	3	N	
7	5575 W Joaquin St – Planada Way - West	2	0	1	N	
8	5575 W Joaquin St – Planada Way - East	4	0	0	N	
9	Joaquin St 5575 W – Heath Ave - North	1	0	0	N	
10	Joaquin St 5575 W – Heath Ave - South	0	0	0	N	
Section 2						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Planada Way – Westslope Dr – East	0	0	0	N	
2	Jeremiah Dr Jeremiah Dr – Westslope Dr – West	3	0	3	N	
3	Jeremiah Dr Jeremiah Dr – Westslope Dr – East	2	0	3	N	
4	5480 W Jeremiah Dr – Westslope Dr - West	2	0	0	N	
5	5480 W Jeremiah Dr – Westslope Dr – East	1	0	0	N	
6	5415 W Planada Way – Westslope Dr - West	9	0	3	Y	
7	5415 W Planada Way – Westslope Dr – East	6	0	5	Y	
8	Jeremiah Dr Jeremiah Dr – 5415 S – North	5	0	2	N	
9	Jeremiah Dr Jeremiah Dr – 5415 S – South	2	2	0	Y	
10	Westslope Dr 5600 W – 5415 W - North	12	0	1	N	
11	Westslope Dr 5600 W – 5415 W - South	6	1	2	N	
Section 3						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Jeremiah Dr 5415 W – La Brea St - North	1	0	1	N	
2	Jeremiah Dr 5415 W – La Brea St - South	2	0	0	N	
3	La Brea St Farah Dr – Westslope Dr - West	8	0	3	N	
4	La Brea St Farah Dr – Westslope Dr - East	3	0	1	N	
5	Farah Dr Planada Way – Westslope Dr - West	2	0	2	N	
6	Farah Dr Planada Way – Westslope Dr – East	4	1	5	N	
7	Stockton St Heath Ave – Westslope Dr - West	2	0	1	N	
8	Stockton St Heath Ave – Westslope Dr – East	4	2	2	N	
9	Westslope Dr La Brea St – Heath Ave - North	7	0	2	N	
10	Westslope Dr La Brea St – Heath Ave – South	9	0	2	N	
11	Heath Ave Planada Way – Westslope Ave - West	5	0	5	N	
12	Heath Ave Planada Way – Westslope Ave – East	9	0	0	N	

Section 4						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Westslope Dr – Mountain Men Dr – East	0	0	0	N	
2	Jeremiah Dr Westslope Dr – Mountain Men Dr - West	6	0	0	N	
3	Jeremiah Dr Westslope Dr – Mountain Men Dr – East	5	0	0	N	
4	5450 W Colter Dr – Mountain Men Dr - West	1	0	4	N	
5	5450 W Colter Dr – Mountain Men Dr – East	0	0	4	N	
6	Colter Dr Jeremiah Dr – Mountain Men Dr - North	4	0	3	N	
7	Colter Dr Jeremiah Dr – Mountain Men Dr - South	12	0	6	N	
8	Mountain Men Dr 5600 W – Tuscan St - North	14	1	9	N	
9	Mountain Men Dr 5600 W – Tuscan St - South	14	0	6	N	
10	Campbell Dr Westslope Dr – 5115 S - West	4	0	2	N	
11	Campbell Dr Westslope Dr – 5115 S - West	4	0	1	N	
12	Heath Ave Westslope Dr – Mountain Men Dr - West	4	0	1	N	
13	Heath Ave Westslope Dr – Mountain Men Dr - East	3	0	1	N	Scoter on sidewalk
Section 5						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Mountain Men Dr – Henley Dr – East	0	0	0	N	
2	Lewis Clark Dr Mountain Men Dr – Campbell Dr - North	14	2	8	N	
3	Lewis Clark Dr Mountain Men Dr – Campbell Dr – South	10	1	5	N	
4	Bailiff Dr Henley Dr – 5420 W - North	8	2	4	N	
5	Bailiff Dr Henley Dr – 5420 W – South	7	1	6	N	
6	5420 W Lewis Clark Dr – Henley Dr - West	4	0	1	N	
7	5420 W Lewis Clark Dr – Henley Dr – East	5	0	0	N	
8	Henley Dr 5600 W – 5200 W - North	9	0	5	N	
9	Henley Dr 5600 W – 5200 W – South	8	0	8	N	
10	Leprechaun Dr Henley Dr – Jededian Dr - North	10	1	10	N	
11	Leprechaun Dr Henley Dr – Jededian Dr - South	4	0	4	N	
12	5115 W Mountain Men Dr – Jededian Dr – North	3	3	3	Y	
13	5115 W Mountain Men Dr – Jededian Dr – South	10	1	4	Y	
14	Shalee St Leprechaun Dr – 5200 S - West	1	0	1	N	
15	Shalee St Leprechaun Dr – 5200 S – East	1	0	0	N	
16	Jededian Dr Mountain Men Dr – Leprechaun Dr - West	3	0	0	N	
17	Jededian Dr Mountain Men Dr – Leprechaun Dr – East	5	0	5	N	
18	Campbell Dr Mountain Men Dr – 5115 S - West	1	0	1	N	
19	Campbell Dr Mountain Men Dr – 5115 S - East	2	0	0	N	

Section 6						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5185 W Mountain Men Dr – Hoopes St - West	2	2	7	N	
2	5185 W Mountain Men Dr – Hoopes St – East	10	1	3	Y	
3	Verde St 5185 W – Westwind Way - North	1	0	0	N	
4	Verde St 5185 W – Westwind Way - South	3	0	2	Y	
5	5160 W Verde St – Hoopes St - West	2	0	1	N	
6	5160 W Verde St – Hoopes St – East	1	0	0	N	
7	5125 S 5160 W – Heath Ave - North	3	0	1	N	
8	5125 S 5160 W – Heath Ave - South	6	0	2	N	
9	Westwind Way Heath Ave – 5125 S – West	8	1	6	N	
10	Westwind Way Heath Ave – 5125 S – East	6	1	2	N	
11	Heath Ave Mountain Men Dr – Hoopes St - West	4	0	4	N	
12	Heath Ave Mountain Men Dr – Hoopes St – East	6	0	1	N	
13	Tuscan St – West	2	0	1	N	
14	Tuscan St – East	5	1	5	N	
Section 7						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Henley Dr – Trident Dr East	0	0	0	N	
2	Parish Dr Henley Dr – Falstaff Dr West	4	0	6	N	
3	Parish Dr Henley Dr – Falstaff Dr East	4	0	5	N	
4	Kemp Dr Henley Dr – Falstaff Dr West	1	0	2	N	
5	Kemp Dr Henley Dr – Falstaff Dr East	1	0	0	N	
6	5420 W Henley Dr – Trident Ln	2	0	0	N	
7	5420 W Henley Dr – Trident Ln	2	0	4	N	
8	Falstaff Dr Parish Dr – 5420 W North	12	0	2	N	
9	Falstaff Dr Parish Dr – 5420 W South	7	1	4	N	
10	Trident Dr 5600 W – 5420 W North	5	2	5	N	
11	Trident Dr 5600 W – 5420 W South	13	2	6	Y	
Section 8						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Trident Dr – 5400 S East	0	0	0	N	
2	Nautilus Dr Trident Dr – 5420 W North	10	0	3	N	
3	Nautilus Dr Trident Dr – 5420 W South	17	0	4	N	
4	5495 W Trident Ln – Nautilus Dr West	4	0	0	Y	
5	5495 W Trident Ln – Nautilus Dr East	5	0	1	N	
6	5420 W Trident Dr – 5400 S - West	2	0	0	N	
7	5420 W Trident Dr – 5400 S – East	4	0	1	N	
8	5400 S 5600 W – 5420 W - North	0	0	0	N	

Section 9						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Leprechaun Ln 5200 S – 5400 S South	13	0	9	N	
2	Leprechaun Ln 5200 S – 5400 S North	4	0	5	N	
3	5240 S 5385 W – Shalee St North	3	0	1	N	
4	5240 S 5385 W – Shalee St South	3	0	3	N	
5	Shalee St 5200 S – Leperchaun Ln West	3	0	1	N	
6	Shalee St 5200 S – Leperchaun Ln East	1	0	1	N	
7	5240 W 5200 S – 5400 S West	0	17	3	N	
8	5240 W 5200 S – 5400 S East	1	12	1	N	
9	5200 W 5200 S – 5400 S West	1	5	3	N	
10	5200 W 5200 S – 5400 S East	10	0	3	Y	
11	5400 S 5120 W – 5200 W North	0	0	0	N	
Section 10						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5160 W Hoopes St – 5320 S – West	0	4	1	N	
2	5160 W Hoopes St – 5320 S - East	3	0	1	N	
3	5120 W Hoopes St – 5320 S - West	1	4	5	N	
4	5120 W Hoopes St – 5320 S - East	5	1	2	N	
5	Hoopes Cir South of Hoopes St - West	4	0	3	N	
6	Hoopes Cir South of Hoopes St - East	6	0	0	N	
7	Heath Ave Hoopes St – 5320 S - West	2	0	0	N	
8	Heath Ave Hoopes St – 5320 S – East	12	1	3	N	
9	5320 S 5160 W – Heath Ave - North	2	2	6	N	
10	5320 S 5160 W – Heath Ave - North	4	8	4	N	
11	Charlotte Ave Hoopes St – 5320 S - West	2	1	1	N	
12	Charlotte Ave Hoopes St – 5320 S - East	4	0	1	N	
Section 11						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5160 W 5320 S – 5415 S West	5	1	0	N	
2	5160 W 5320 S – 5415 S East	1	0	0	N	
3	5030 W 5320 S – 5415 S West	1	1	1	N	
4	5030 W 5320 S – 5415 S East	0	0	0	N	
5	Heath Ave 5320 S – 5100 S West	2	3	2	N	
6	Heath Ave 5320 S – 5100 S East	9	2	4	N	
7	5360 S 5160 W – Heath Ave North	12	3	5	N	
8	5360 S 5160 W – Heath Ave South	5	2	2	N	
9	5415 S 5160 W – Heath Ave North	0	0	0	N	
10	Charlotte Ave Cross St – 5415 S - West	4	0	3	N	
11	Charlotte Ave Cross St – 5415 S - East	4	1	3	N	

Section 12						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5245 W Planada Way – Dead End - West	8	0	1	Y	
2	5245 W Planada Way – Dead End - East	3	0	5	N	
3	Stockton St Heath Ave – 5245 E North	2	0	1	N	
4	Stockton St Heath Ave – 5245 E South	0	0	0	N	

Collection Time: 8:00 PM - 10:00 PM

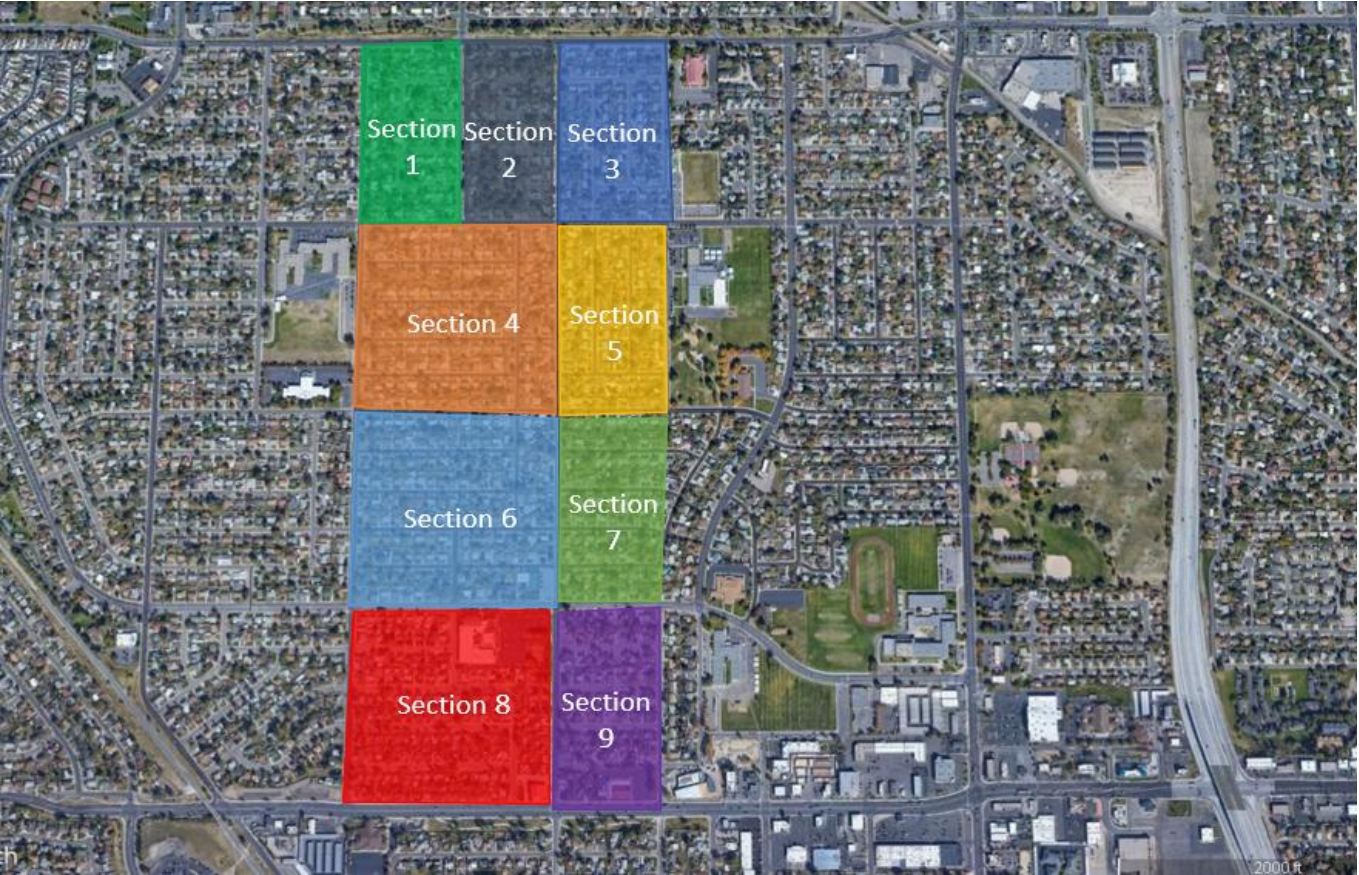
Section 1						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Townsend Way 5600 W - Planada Way - South	8	1	8	Y	
2	5600 W Townsend Way - Planada Way – East	0	0	0	N	
3	Heath Ave Townsend Way - Planada Way -- South	11	1	8	N	
4	Heath Ave Townsend Way - Planada Way – North	6	1	8	Y	
5	Planada Way 5600 W - 5245 W – North	17	1	2	N	
6	Planada Way 5600 W - 5245 W – South	15	0	2	N	
7	5575 W Joaquin St – Planada Way - West	3	0	2	N	
8	5575 W Joaquin St – Planada Way - East	6	0	0	N	
9	Joaquin St 5575 W – Heath Ave - North	1	0	0	N	
10	Joaquin St 5575 W – Heath Ave - South	0	0	0	N	
Section 2						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Planada Way – Westslope Dr – East	0	0	0	N	
2	Jeremiah Dr Jeremiah Dr – Westslope Dr – West	6	1	4	N	
3	Jeremiah Dr Jeremiah Dr – Westslope Dr – East	1	0	2	N	
4	5480 W Jeremiah Dr – Westslope Dr - West	2	0	0	N	
5	5480 W Jeremiah Dr – Westslope Dr – East	1	0	0	N	
6	5415 W Planada Way – Westslope Dr - West	8	0	4	N	
7	5415 W Planada Way – Westslope Dr – East	10	0	4	N	
8	Jeremiah Dr Jeremiah Dr – 5415 S – North	9	1	0	N	
9	Jeremiah Dr Jeremiah Dr – 5415 S – South	2	2	2	Y	
10	Westslope Dr 5600 W – 5415 W - North	10	0	4	Y	
11	Westslope Dr 5600 W – 5415 W - South	11	2	3	N	
Section 3						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Jeremiah Dr 5415 W – La Brea St - North	3	1	0	N	
2	Jeremiah Dr 5415 W – La Brea St - South	1	0	0	N	
3	La Brea St Farah Dr – Westslope Dr - West	4	0	4	N	
4	La Brea St Farah Dr – Westslope Dr - East	3	0	3	N	
5	Farah Dr Planada Way – Westslope Dr - West	8	0	3	N	
6	Farah Dr Planada Way – Westslope Dr – East	12	1	6	N	
7	Stockton St Heath Ave – Westslope Dr - West	10	0	5	Y	
8	Stockton St Heath Ave – Westslope Dr – East	7	0	0	N	
9	Westslope Dr La Brea St – Heath Ave - North	10	0	4	N	
10	Westslope Dr La Brea St – Heath Ave – South	14	0	1	N	
11	Heath Ave Planada Way – Westslope Ave - West	11	0	3	N	
12	Heath Ave Planada Way – Westslope Ave – East	13	0	2	N	

Section 4						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Westslope Dr – Mountain Men Dr – East	0	0	0	N	
2	Jeremiah Dr Westslope Dr – Mountain Men Dr - West	6	0	2	N	
3	Jeremiah Dr Westslope Dr – Mountain Men Dr – East	4	1	1	Y	
4	5450 W Colter Dr – Mountain Men Dr - West	2	0	2	N	
5	5450 W Colter Dr – Mountain Men Dr – East	1	0	6	N	
6	Colter Dr Jeremiah Dr – Mountain Men Dr - North	9	0	7	N	
7	Colter Dr Jeremiah Dr – Mountain Men Dr - South	7	2	12	Y	
8	Mountain Men Dr 5600 W – Tuscan St - North	20	0	8	N	
9	Mountain Men Dr 5600 W – Tuscan St - South	15	0	9	Y	
10	Campbell Dr Westslope Dr – 5115 S - West	2	0	1	N	
11	Campbell Dr Westslope Dr – 5115 S - West	3	0	2	Y	
12	Heath Ave Westslope Dr – Mountain Men Dr - West	4	1	0	N	
13	Heath Ave Westslope Dr – Mountain Men Dr - East	4	0	1	N	
Section 5						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Mountain Men Dr – Henley Dr – East	0	0	0	N	
2	Lewis Clark Dr Mountain Men Dr – Campbell Dr - North	15	2	6	N	
3	Lewis Clark Dr Mountain Men Dr – Campbell Dr – South	14	1	14	N	
4	Bailiff Dr Henley Dr – 5420 W - North	9	0	3	N	
5	Bailiff Dr Henley Dr – 5420 W – South	13	0	6	N	
6	5420 W Lewis Clark Dr – Henley Dr - West	2	0	1	Y	
7	5420 W Lewis Clark Dr – Henley Dr – East	2	0	1	Y	
8	Henley Dr 5600 W – 5200 W - North	15	0	9	N	
9	Henley Dr 5600 W – 5200 W – South	12	0	13	N	
10	Leprechaun Dr Henley Dr – Jededian Dr - North	13	1	9	N	
11	Leprechaun Dr Henley Dr – Jededian Dr - South	4	0	7	N	
12	5115 W Mountain Men Dr – Jededian Dr – North	6	1	3	Y	
13	5115 W Mountain Men Dr – Jededian Dr – South	11	1	2	Y	
14	Shalee St Leprechaun Dr – 5200 S - West	3	0	0	N	
15	Shalee St Leprechaun Dr – 5200 S – East	3	0	0	N	
16	Jededian Dr Mountain Men Dr – Leprechaun Dr - West	7	0	1	N	
17	Jededian Dr Mountain Men Dr – Leprechaun Dr – East	7	1	7	N	
18	Campbell Dr Mountain Men Dr – 5115 S - West	1	0	3	N	
19	Campbell Dr Mountain Men Dr – 5115 S - East	3	0	0	N	

Section 6						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5185 W Mountain Men Dr – Hoopes St - West	7	1	7	N	
2	5185 W Mountain Men Dr – Hoopes St – East	6	2	3	N	
3	Verde St 5185 W – Westwind Way - North	4	0	2	N	
4	Verde St 5185 W – Westwind Way - South	1	0	2	N	
5	5160 W Verde St – Hoopes St - West	2	0	3	N	
6	5160 W Verde St – Hoopes St – East	0	0	2	N	
7	5125 S 5160 W – Heath Ave - North	3	0	3	N	
8	5125 S 5160 W – Heath Ave - South	6	0	1	N	
9	Westwind Way Heath Ave – 5125 S – West	9	0	6	N	
10	Westwind Way Heath Ave – 5125 S – East	6	0	3	N	
11	Heath Ave Mountain Men Dr – Hoopes St - West	4	0	3	Y	
12	Heath Ave Mountain Men Dr – Hoopes St – East	5	0	1	N	
13	Tuscan St – West	6	1	1	N	
14	Tuscan St – East	5	1	2	N	
Section 7						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Henley Dr – Trident Dr East	0	0	0	N	
2	Parish Dr Henley Dr – Falstaff Dr West	4	2	3	N	
3	Parish Dr Henley Dr – Falstaff Dr East	5	0	2	N	
4	Kemp Dr Henley Dr – Falstaff Dr West	3	0	1	N	
5	Kemp Dr Henley Dr – Falstaff Dr East	2	0	0	N	
6	5420 W Henley Dr – Trident Ln	1	1	2	N	
7	5420 W Henley Dr – Trident Ln	2	0	3	N	
8	Falstaff Dr Parish Dr – 5420 W North	15	0	10	N	
9	Falstaff Dr Parish Dr – 5420 W South	5	1	3	N	
10	Trident Dr 5600 W – 5420 W North	4	2	6	N	
11	Trident Dr 5600 W – 5420 W South	12	1	8	N	
Section 8						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Trident Dr – 5400 S East	0	0	0	N	
2	Nautilus Dr Trident Dr – 5420 W North	12	0	4	Y	
3	Nautilus Dr Trident Dr – 5420 W South	18	2	10	N	
4	5495 W Trident Ln – Nautilus Dr West	4	0	0	Y	
5	5495 W Trident Ln – Nautilus Dr East	5	0	3	N	
6	5420 W Trident Dr – 5400 S - West	3	0	0	N	
7	5420 W Trident Dr – 5400 S – East	4	0	2	N	
8	5400 S 5600 W – 5420 W - North	0	0	0	N	

Section 9						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Leprechaun Ln 5200 S – 5400 S South	10	3	12	N	
2	Leprechaun Ln 5200 S – 5400 S North	2	0	9	N	
3	5240 S 5385 W – Shalee St North	4	0	1	N	
4	5240 S 5385 W – Shalee St South	3	0	5	N	
5	Shalee St 5200 S – Leperchaun Ln West	3	0	2	N	
6	Shalee St 5200 S – Leperchaun Ln East	2	0	0	N	
7	5240 W 5200 S – 5400 S West	5	14	6	N	
8	5240 W 5200 S – 5400 S East	2	11	4	N	
9	5200 W 5200 S – 5400 S West	0	5	6	N	
10	5200 W 5200 S – 5400 S East	12	0	3	N	
11	5400 S 5120 W – 5200 W North	0	0	0	N	
Section 10						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5160 W Hoopes St – 5320 S – West	1	3	0	N	
2	5160 W Hoopes St – 5320 S - East	1	0	0	N	
3	5120 W Hoopes St – 5320 S - West	1	8	3	N	
4	5120 W Hoopes St – 5320 S - East	5	1	3	N	
5	Hoopes Cir South of Hoopes St - West	3	1	4	N	
6	Hoopes Cir South of Hoopes St - East	6	2	1	N	
7	Heath Ave Hoopes St – 5320 S - West	2	0	0	N	
8	Heath Ave Hoopes St – 5320 S – East	12	0	6	N	
9	5320 S 5160 W – Heath Ave - North	4	2	7	Y	
10	5320 S 5160 W – Heath Ave - North	7	7	3	N	
11	Charlotte Ave Hoopes St – 5320 S - West	1	2	2	Y	
12	Charlotte Ave Hoopes St – 5320 S - East	4	1	1	N	
Section 11						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5160 W 5320 S – 5415 S West	3	0	0	N	
2	5160 W 5320 S – 5415 S East	1	0	0	N	
3	5030 W 5320 S – 5415 S West	1	0	0	N	
4	5030 W 5320 S – 5415 S East	0	0	1	N	
5	Heath Ave 5320 S – 5100 S West	5	1	3	N	
6	Heath Ave 5320 S – 5100 S East	15	2	4	N	
7	5360 S 5160 W – Heath Ave North	9	3	3	N	
8	5360 S 5160 W – Heath Ave South	3	0	2	N	
9	5415 S 5160 W – Heath Ave North	0	0	0	N	
10	Charlotte Ave Cross St – 5415 S - West	3	2	6	N	
11	Charlotte Ave Cross St – 5415 S - East	7	0	6	N	

Section 12						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5245 W Planada Way – Dead End - West	9	0	2	N	
2	5245 W Planada Way – Dead End - East	7	3	3	N	
3	Stockton St Heath Ave – 5245 E North	2	1	0	N	
4	Stockton St Heath Ave – 5245 E South	1	0	0	N	



Date 12/14/2021
Counters Mitchell Hadfield, Alison Brown

Notes
Winter storm hit around 8:30 PM, and rapidly got worse we were unable to continue data collection for safety reasons.
If a boat, trailer, or RV was parked on the street or in a yard it was counted as a vehicle
No vehicle was double counted in data collection. Each column shows separate vehicles. If a car was on the sidewalk and the street it was counted as a sidewalk blocking vehicle.
Sight distance issues were noticed on north south roads in the study area. From minor approaches onto 4420 W and 4620

Collection Time: 7:00 AM - 9:00 AM

Section 1						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4720 W 4715 S – 4865 S - West	0	4	6	N	
2	4720 W 4715 S – 4865 S – East	0	4	6	N	
3	4715 S 4720 W – 4620 W - South	6	0	2	N	
4	4620 W 4715 S – 4865 S - West	1	2	3	N	
5	4620 W 4715 S – 4865 S - West	0	1	7	N	
6	4865 S 4620 W – 4520 W - North	1	2	2	N	
7	4865 S 4620 W – 4520 W - North	0	0	2	N	
Section 2						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4715 S 4520 W – 4420 W – South	0	0	1	N	
2	4420 W 4715 S – 4866 S - West	0	0	2	N	
3	4420 W 4715 S – 4866 S – East	3	1	7	N	
4	4865 S 4520 W – 4420 S - North	2	1	1	N	
5	4865 S 4520 W – 4420 S - South	3	1	0	N	
Section 3						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4715 S 4420 W – 4300 W – South	0	0	1	N	
2	4745 S 4420 W – 4300 W – North	0	1	4	N	
3	4745 S 4420 W – 4300 W – South	0	3	2	Y	
4	4835 S 4380 W – 4300 W – North	0	2	0	N	
5	4835 S 4380 W – 4300 W – South	0	3	0	N	
6	4380 W 4745 S – 4835 S - West	1	6	5	N	
7	4380 W 4745 S – 4835 S – East	0	6	5	N	
8	4340 W 4745 S – 4835 S - West	2	0	2	N	
9	4340 W 4745 S – 4835 S – East	0	6	3	N	
10	4300 W 4715 S – 4835 S – West	0	2	1	N	
11	4300 W 4715 S – 4835 S - East	0	2	0	N	

Section 4						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4620 W 5865 S – 5015 S – West	0	0	0	N	
2	4620 W 5865 S – 5015 S – East	0	1	7	N	
3	4895 S 4620 W – 4460 W – North	1	1	4	N	
4	4895 S 4620 W – 4460 W – South	1	2	3	N	
5	4925 S 4620 W – 4460 W – North	0	6	1	Y	
6	4925 S 4620 W – 4460 W – South	2	0	5	N	
7	4955 S 4620 W – 4460 W - North	2	8	3	N	
8	4955 S 4620 W – 4460 W – South	0	6	0	Y	
9	4985 S 4620 W – 4460 W - North	2	3	2	N	
10	4985 S 4620 W – 4460 W – South	0	4	4	N	
11	5015 S 4620 W – 4460 W - North	0	0	6	N	
12	5015 S 4620 W – 4460 W – South	6	3	2	N	
13	4460 W 4865 S – 5015 S - West	0	0	5	N	
14	4460 W 4865 S – 5015 S - East	3	6	0	Y	
Section 5						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4955 S 4460 W – 4420 W - North	0	0	0	N	
2	4955 S 4460 W – 4420 W – South	1	0	0	N	
3	4420 W 4865 S – 5015 S - West	0	0	3	N	
4	4420 W 4865 S – 5015 S – East	2	0	8	N	
5	4380 W 4865 S – 4985 S - West	2	6	5	N	
6	4380 W 4865 S – 4985 S – East	1	1	3	N	
7	4340 W 4865 S – 4985 S - West	1	6	3	N	
8	4340 W 4865 S – 4985 S – East	3	5	5	N	
9	4300 W 4865 S – 5015 S - West	1	2	7	N	
10	4300 W 4865 S – 5015 S – East	2	0	0	N	
11	4985 S 4420 W – 4300 W - North	3	2	1	Y	
12	4985 S 4420 W – 4300 W – South	0	2	0	N	
13	5015 S 4460 W – 4300 W - North	3	1	1	N	
14	5015 S 4460 W – 4300 W - South	3	2	2	N	

Section 6						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5055 S 4620 W – 4460 W - North	9	1	0	N	
2	5055 S 4620 W – 4460 W – South	7	6	1	N	
3	5100 S 4620 W – 4460 W - North	5	0	2	N	
4	5100 S 4620 W – 4460 W – South	6	0	2	N	
5	5135 S 4620 W – 4420 W - North	4	8	7	N	
6	5135 S 4620 W – 4420 W – South	1	2	2	N	
7	5175 S 4620 W – 4420 W - North	1	0	0	Y	
8	5175 S 4620 W – 4420 W – South	3	2	1	Y	
9	5215 S 4620 W – 4420 W - North	0	1	5	N	
10	5215 S 4620 W – 4420 W – South	2	2	0	N	
11	4520 W 5135 S – 5215 S - West	0	1	1	N	
12	4520 W 5135 S – 5215 S – East	0	3	2	N	
13	4460 W 5015 S – 5135 S - West	2	0	2	N	
14	4460 W 5015 S – 5135 S – East	0	1	2	N	
15	4420 W 5015 S – 5215 S - West	0	0	3	N	
16	4420 W 5015 S – 5215 S - East	1	0	3	N	
Section 7						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5055 S 4420 W – Alex St - North	3	5	5	N	
2	5055 S 4420 W – Alex St – South	3	5	2	N	
3	5100 S 4420 W – Alex St - North	3	0	1	N	
4	5100 S 4420 W – Alex St – South	1	0	0	N	
5	5135 S 4420 – Alex St - North	0	1	1	N	
6	5135 S 4420 – Alex St – South	0	2	1	N	
7	5175 S 4420 W – Alex St - North	0	1	3	Y	
8	5175 S 4420 W – Alex St - South	0	6	0	Y	
9	5215 S 4420 W – 4245 W - North	3	1	2	N	
10	5215 S 4420 W – 4245 W – South	0	0	3	N	

Section 8						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5255 S 4620 W – 4520 W - North	0	2	0	N	
2	5255 S 4620 W – 4520 W – South	1	0	0	N	
3	5295 S 4620 W – 4420 W - North	1	4	3	N	
4	5295 S 4620 W – 4420 W – South	9	1	2	N	
5	5335 S 4620 W – 4460 W - North	2	0	3	N	
6	5335 S 4620 W – 4460 W – South	4	2	4	N	
7	5375 S 4620 W – 4460 W - North	0	8	1	N	
8	5375 S 4620 W – 4460 W – South	5	1	0	N	
9	4520 W 5215 S – 4295 S - West	2	0	1	N	
10	4520 W 5215 S – 4295 S – East	1	0	0	N	
11	4460 W 5295 S – 5415 S - West	1	0	0	N	
12	4460 W 5295 S – 5415 S – East	1	3	2	N	
13	4420 W 5215 S – 5415 S - West	1	0	2	N	
14	4420 W 5215 S – 5415 S – East	0	7	7	N	
15	5415 S 4620 W – 4420 W – North	0	0	7	N	
16	5415 S 4620 W – 4420 W – South	0	0	0	N	
Section 9						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5255 S 4420 W – 4320 W - North	2	0	1	N	
2	5255 S 4420 W – 4320 W – South	2	0	1	N	
3	5295 S 4420 W – 4320 W - North	3	0	2	N	
4	5295 S 4420 W – 4320 W – South	1	0	0	N	
5	5335 S 4420 W – 4320 W - North	0	1	0	N	
6	5335 S 4420 W – 4320 W – South	0	2	2	N	
7	5375 S 4420 W – 4280 W - North	3	0	1	N	
8	5375 S 4420 W – 4280 W – South	0	0	0	N	
9	4320 W 5215 S – 5415 S - West	0	0	4	N	
10	4320 W 5215 S – 5415 S – East	0	3	2	N	
11	4280 W 5215 S – 5375 S - West	0	5	8	N	
12	4280 W 5215 S – 5375 S – East	0	0	0	N	
13	5415 S 4420 W – 4220 W - North	0	0	0	N	
14	5415 S 4420 W – 4220 W - South	0	0	0	N	

Collection Time: 2:00 PM - 4:00 PM

Section 1						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4720 W 4715 S – 4865 S - West	0	1	5	N	
2	4720 W 4715 S – 4865 S – East	0	6	3	N	
3	4715 S 4720 W – 4620 W - South	3	2	1	N	
4	4620 W 4715 S – 4865 S - West	1	0	2	N	
5	4620 W 4715 S – 4865 S - West	0	3	4	N	
6	4865 S 4620 W – 4520 W - North	1	0	1	N	Child running in street
7	4865 S 4620 W – 4520 W - North	0	0	2	N	
Section 2						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4715 S 4520 W – 4420 W – South	0	0	2	N	
2	4420 W 4715 S – 4866 S - West	0	0	6	N	
3	4420 W 4715 S – 4866 S – East	3	0	2	N	
4	4865 S 4520 W – 4420 S - North	0	1	2	N	
5	4865 S 4520 W – 4420 S - South	2	0	2	N	
Section 3						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4715 S 4420 W – 4300 W – South	0	0	1	N	
2	4745 S 4420 W – 4300 W – North	0	1	3	N	
3	4745 S 4420 W – 4300 W – South	0	2	1	N	
4	4835 S 4380 W – 4300 W – North	0	4	1	N	
5	4835 S 4380 W – 4300 W – South	0	2	0	N	
6	4380 W 4745 S – 4835 S - West	2	7	5	N	
7	4380 W 4745 S – 4835 S – East	1	4	3	N	
8	4340 W 4745 S – 4835 S - West	0	1	2	N	
9	4340 W 4745 S – 4835 S – East	0	8	2	N	
10	4300 W 4715 S – 4835 S – West	0	2	0	N	
11	4300 W 4715 S – 4835 S - East	0	2	0	N	

Section 4						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4620 W 5865 S – 5015 S – West	0	0	0	N	
2	4620 W 5865 S – 5015 S – East	0	0	9	N	
3	4895 S 4620 W – 4460 W – North	0	3	1	N	
4	4895 S 4620 W – 4460 W – South	0	1	3	N	
5	4925 S 4620 W – 4460 W – North	2	3	1	Y	
6	4925 S 4620 W – 4460 W – South	3	0	4	N	
7	4955 S 4620 W – 4460 W - North	1	9	2	N	
8	4955 S 4620 W – 4460 W – South	0	5	0	Y	
9	4985 S 4620 W – 4460 W - North	1	6	2	N	
10	4985 S 4620 W – 4460 W – South	2	3	4	N	
11	5015 S 4620 W – 4460 W - North	0	1	6	N	
12	5015 S 4620 W – 4460 W – South	3	3	0	N	
13	4460 W 4865 S – 5015 S - West	1	0	4	N	
14	4460 W 4865 S – 5015 S - East	0	5	0	N	
Section 5						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4955 S 4460 W – 4420 W - North	0	0	0	N	
2	4955 S 4460 W – 4420 W – South	1	0	0	N	
3	4420 W 4865 S – 5015 S - West	0	0	4	N	Sight distance issues were noticed on north south roads in the study area. From minor approaches onto 4420 W there was not adequate sight distance to turn on to 4420 W due to parked vehicles
4	4420 W 4865 S – 5015 S – East	1	3	5	N	Sight distance issues were noticed on north south roads in the study area. From minor approaches onto 4420 W there was not adequate sight distance to turn on to 4420 W due to parked vehicles
5	4380 W 4865 S – 4985 S - West	2	10	7	N	
6	4380 W 4865 S – 4985 S – East	0	2	3	N	
7	4340 W 4865 S – 4985 S - West	2	3	2	N	
8	4340 W 4865 S – 4985 S – East	2	4	5	Y	
9	4300 W 4865 S – 5015 S - West	16	20	3	N	School Pickup
10	4300 W 4865 S – 5015 S – East	25	0	0	N	School Pickup
11	4985 S 4420 W – 4300 W - North	2	2	2	Y	
12	4985 S 4420 W – 4300 W – South	0	1	0	N	
13	5015 S 4460 W – 4300 W - North	5	2	1	N	
14	5015 S 4460 W – 4300 W - South	3	1	2	N	

Section 6						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5055 S 4620 W – 4460 W - North	0	5	1	N	
2	5055 S 4620 W – 4460 W – South	2	7	4	N	
3	5100 S 4620 W – 4460 W - North	2	5	4	N	
4	5100 S 4620 W – 4460 W – South	10	5	2	N	
5	5135 S 4620 W – 4420 W - North	2	8	4	N	
6	5135 S 4620 W – 4420 W – South	1	0	3	N	
7	5175 S 4620 W – 4420 W - North	0	1	0	N	
8	5175 S 4620 W – 4420 W – South	1	1	1	N	
9	5215 S 4620 W – 4420 W - North	1	1	8	N	
10	5215 S 4620 W – 4420 W – South	1	0	0	N	
11	4520 W 5135 S – 5215 S - West	0	0	1	N	
12	4520 W 5135 S – 5215 S – East	0	3	2	N	
13	4460 W 5015 S – 5135 S - West	1	0	1	N	
14	4460 W 5015 S – 5135 S – East	0	0	4	N	
15	4420 W 5015 S – 5215 S - West	0	0	4	N	
16	4420 W 5015 S – 5215 S - East	1	1	5	N	
Section 7						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5055 S 4420 W – Alex St - North	3	5	4	N	
2	5055 S 4420 W – Alex St – South	4	1	1	N	
3	5100 S 4420 W – Alex St - North	3	2	2	N	
4	5100 S 4420 W – Alex St – South	2	0	0	N	
5	5135 S 4420 – Alex St - North	0	0	2	N	
6	5135 S 4420 – Alex St – South	0	2	3	N	
7	5175 S 4420 W – Alex St - North	0	2	3	N	
8	5175 S 4420 W – Alex St - South	0	5	0	N	
9	5215 S 4420 W – 4245 W - North	1	3	4	N	
10	5215 S 4420 W – 4245 W – South	0	1	3	N	

Section 8						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5255 S 4620 W – 4520 W - North	0	0	0	N	
2	5255 S 4620 W – 4520 W – South	0	2	0	N	
3	5295 S 4620 W – 4420 W - North	0	3	0	N	
4	5295 S 4620 W – 4420 W – South	2	6	2	N	
5	5335 S 4620 W – 4460 W - North	2	1	2	N	
6	5335 S 4620 W – 4460 W – South	5	5	4	N	
7	5375 S 4620 W – 4460 W - North	1	8	0	N	
8	5375 S 4620 W – 4460 W – South	5	2	0	Y	
9	4520 W 5215 S – 4295 S - West	1	0	0	N	
10	4520 W 5215 S – 4295 S – East	0	0	0	N	
11	4460 W 5295 S – 5415 S - West	0	2	1	N	
12	4460 W 5295 S – 5415 S – East	1	2	3	N	
13	4420 W 5215 S – 5415 S - West	0	0	0	N	
14	4420 W 5215 S – 5415 S – East	0	0	8	N	
15	5415 S 4620 W – 4420 W – North	0	0	7	N	
16	5415 S 4620 W – 4420 W – South	0	0	0	N	
Section 9						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5255 S 4420 W – 4320 W - North	0	2	1	N	
2	5255 S 4420 W – 4320 W – South	0	3	3	N	
3	5295 S 4420 W – 4320 W - North	0	5	2	N	
4	5295 S 4420 W – 4320 W – South	0	1	2	N	
5	5335 S 4420 W – 4320 W - North	0	0	1	N	
6	5335 S 4420 W – 4320 W – South	0	3	3	N	
7	5375 S 4420 W – 4280 W - North	2	1	2	N	
8	5375 S 4420 W – 4280 W – South	0	0	0	N	
9	4320 W 5215 S – 5415 S - West	0	0	7	N	
10	4320 W 5215 S – 5415 S – East	1	2	5	N	
11	4280 W 5215 S – 5375 S - West	0	7	7	N	
12	4280 W 5215 S – 5375 S – East	0	0	0	N	
13	5415 S 4420 W – 4220 W - North	0	0	0	N	
14	5415 S 4420 W – 4220 W - South	0	0	0	N	

Collection Time: 5:00 PM - 7:00 PM

Section 1						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4720 W 4715 S – 4865 S - West	0	3	6	N	
2	4720 W 4715 S – 4865 S – East	0	9	1	N	
3	4715 S 4720 W – 4620 W - South	7	0	2	N	
4	4620 W 4715 S – 4865 S - West	0	1	4	N	
5	4620 W 4715 S – 4865 S - West	2	1	5	N	
6	4865 S 4620 W – 4520 W - North	1	1	2	N	
7	4865 S 4620 W – 4520 W - North	1	0	2	N	
Section 2						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4715 S 4520 W – 4420 W – South	0	0	1	N	
2	4420 W 4715 S – 4866 S - West	0	0	6	N	
3	4420 W 4715 S – 4866 S – East	4	1	3	N	
4	4865 S 4520 W – 4420 S - North	0	3	3	N	
5	4865 S 4520 W – 4420 S - South	3	0	1	N	
Section 3						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4715 S 4420 W – 4300 W – South	0	0	2	N	
2	4745 S 4420 W – 4300 W – North	0	6	3	N	
3	4745 S 4420 W – 4300 W – South	0	1	3	N	
4	4835 S 4380 W – 4300 W – North	0	4	1	N	
5	4835 S 4380 W – 4300 W – South	0	2	0	Y	
6	4380 W 4745 S – 4835 S - West	0	14	9	N	
7	4380 W 4745 S – 4835 S – East	0	4	8	N	
8	4340 W 4745 S – 4835 S - West	0	4	3	N	
9	4340 W 4745 S – 4835 S – East	0	7	2	N	
10	4300 W 4715 S – 4835 S – West	0	4	0	N	
11	4300 W 4715 S – 4835 S - East	0	2	0	N	

Section 4						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4620 W 5865 S – 5015 S – West	1	0	0	N	
2	4620 W 5865 S – 5015 S – East	1	2	13	N	
3	4895 S 4620 W – 4460 W – North	0	5	3	N	
4	4895 S 4620 W – 4460 W – South	2	0	2	N	
5	4925 S 4620 W – 4460 W – North	2	4	0	N	
6	4925 S 4620 W – 4460 W – South	3	1	5	N	
7	4955 S 4620 W – 4460 W - North	2	10	3	N	
8	4955 S 4620 W – 4460 W – South	1	7	0	N	
9	4985 S 4620 W – 4460 W - North	3	4	4	N	
10	4985 S 4620 W – 4460 W – South	1	6	5	N	
11	5015 S 4620 W – 4460 W - North	2	3	5	N	
12	5015 S 4620 W – 4460 W – South	3	5	2	N	
13	4460 W 4865 S – 5015 S - West	0	6	5	N	
14	4460 W 4865 S – 5015 S - East	2	2	4	N	
Section 5						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4955 S 4460 W – 4420 W - North	1	0	0	N	
2	4955 S 4460 W – 4420 W – South	0	0	0	N	
3	4420 W 4865 S – 5015 S - West	0	0	8	N	
4	4420 W 4865 S – 5015 S – East	1	2	10	N	
5	4380 W 4865 S – 4985 S - West	8	7	10	Y	
6	4380 W 4865 S – 4985 S – East	0	2	4	Y	
7	4340 W 4865 S – 4985 S - West	3	7	2	N	
8	4340 W 4865 S – 4985 S – East	0	8	8	N	
9	4300 W 4865 S – 5015 S - West	2	3	4	N	
10	4300 W 4865 S – 5015 S – East	0	0	0	N	
11	4985 S 4420 W – 4300 W - North	0	3	4	Y	
12	4985 S 4420 W – 4300 W – South	0	4	2	Y	
13	5015 S 4460 W – 4300 W - North	0	9	3	N	
14	5015 S 4460 W – 4300 W - South	1	7	2	N	

Section 6						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5055 S 4620 W – 4460 W - North	8	3	2	N	
2	5055 S 4620 W – 4460 W – South	8	9	3	N	
3	5100 S 4620 W – 4460 W - North	1	6	3	N	
4	5100 S 4620 W – 4460 W – South	0	10	4	N	
5	5135 S 4620 W – 4420 W - North	0	11	4	N	
6	5135 S 4620 W – 4420 W – South	0	1	4	N	
7	5175 S 4620 W – 4420 W - North	3	0	2	N	
8	5175 S 4620 W – 4420 W – South	1	0	5	N	
9	5215 S 4620 W – 4420 W - North	2	2	2	N	
10	5215 S 4620 W – 4420 W – South	3	0	2	N	
11	4520 W 5135 S – 5215 S - West	0	0	1	N	
12	4520 W 5135 S – 5215 S – East	3	2	1	N	
13	4460 W 5015 S – 5135 S - West	0	3	3	N	
14	4460 W 5015 S – 5135 S – East	0	0	4	N	
15	4420 W 5015 S – 5215 S - West	1	0	6	N	
16	4420 W 5015 S – 5215 S - East	2	0	5	N	
Section 7						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5055 S 4420 W – Alex St - North	2	5	3	N	
2	5055 S 4420 W – Alex St – South	0	7	2	N	
3	5100 S 4420 W – Alex St - North	2	3	1	N	
4	5100 S 4420 W – Alex St – South	2	1	0	N	
5	5135 S 4420 – Alex St - North	2	0	2	N	
6	5135 S 4420 – Alex St – South	1	0	2	N	
7	5175 S 4420 W – Alex St - North	0	4	3	N	
8	5175 S 4420 W – Alex St - South	2	5	7	N	
9	5215 S 4420 W – 4245 W - North	2	2	4	N	
10	5215 S 4420 W – 4245 W – South	0	0	1	N	

Section 8						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5255 S 4620 W – 4520 W - North	0	2	0	N	
2	5255 S 4620 W – 4520 W – South	0	4	0	N	
3	5295 S 4620 W – 4420 W - North	0	6	0	N	
4	5295 S 4620 W – 4420 W – South	7	3	1	N	
5	5335 S 4620 W – 4460 W - North	3	2	2	N	
6	5335 S 4620 W – 4460 W – South	12	3	4	N	
7	5375 S 4620 W – 4460 W - North	0	10	2	N	
8	5375 S 4620 W – 4460 W – South	2	7	0	N	
9	4520 W 5215 S – 4295 S - West	0	3	1	N	
10	4520 W 5215 S – 4295 S – East	0	0	0	N	
11	4460 W 5295 S – 5415 S - West	4	2	1	N	
12	4460 W 5295 S – 5415 S – East	0	5	2	N	
13	4420 W 5215 S – 5415 S - West	2	0	1	N	
14	4420 W 5215 S – 5415 S – East	0	0	13	N	
15	5415 S 4620 W – 4420 W – North	0	0	6	N	
16	5415 S 4620 W – 4420 W – South	0	0	0	N	
Section 9						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5255 S 4420 W – 4320 W - North	3	0	1	N	
2	5255 S 4420 W – 4320 W – South	3	1	2	N	
3	5295 S 4420 W – 4320 W - North	0	7	3	N	
4	5295 S 4420 W – 4320 W – South	2	0	0	N	
5	5335 S 4420 W – 4320 W - North	0	0	2	N	
6	5335 S 4420 W – 4320 W – South	1	1	3	N	
7	5375 S 4420 W – 4280 W - North	0	3	1	N	
8	5375 S 4420 W – 4280 W – South	2	0	0	N	
9	4320 W 5215 S – 5415 S - West	0	1	4	N	
10	4320 W 5215 S – 5415 S – East	3	1	33	N	
11	4280 W 5215 S – 5375 S - West	0	7	9	N	
12	4280 W 5215 S – 5375 S – East	0	0	0	N	
13	5415 S 4420 W – 4220 W - North	0	0	0	N	
14	5415 S 4420 W – 4220 W - South	0	0	0	N	

Collection Time: 8:00 PM - 10:00 PM

Section 1						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4720 W 4715 S – 4865 S - West	1	5	4	N	
2	4720 W 4715 S – 4865 S – East	7	1	3	N	
3	4715 S 4720 W – 4620 W - South	6	0	2	N	
4	4620 W 4715 S – 4865 S - West	1	1	4	N	
5	4620 W 4715 S – 4865 S - West	0	3	5	N	
6	4865 S 4620 W – 4520 W - North	6	0	0	N	
7	4865 S 4620 W – 4520 W - North	1	0	1	N	
Section 2						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4715 S 4520 W – 4420 W – South	1	0	1	N	
2	4420 W 4715 S – 4866 S - West	2	3	1	N	
3	4420 W 4715 S – 4866 S – East	1	3	1	N	
4	4865 S 4520 W – 4420 S - North	3	1	2	N	
5	4865 S 4520 W – 4420 S - South	2	1	2	N	
Section 3						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4715 S 4420 W – 4300 W – South	0	0	4	N	
2	4745 S 4420 W – 4300 W – North	0	2	2	N	
3	4745 S 4420 W – 4300 W – South	0	1	1	N	
4	4835 S 4380 W – 4300 W – North	0	5	0	N	
5	4835 S 4380 W – 4300 W – South	0	2	1	N	
6	4380 W 4745 S – 4835 S - West	2	9	6	N	
7	4380 W 4745 S – 4835 S – East	1	3	3	N	
8	4340 W 4745 S – 4835 S - West	0	2	3	N	
9	4340 W 4745 S – 4835 S – East	0	9	3	N	
10	4300 W 4715 S – 4835 S – West	2	3	0	N	
11	4300 W 4715 S – 4835 S - East	0	0	1	N	

Section 4						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4620 W 5865 S – 5015 S – West	0	2	2	N	
2	4620 W 5865 S – 5015 S – East	0	0	9	N	
3	4895 S 4620 W – 4460 W – North	2	2	3	N	
4	4895 S 4620 W – 4460 W – South	2	0	3	N	
5	4925 S 4620 W – 4460 W – North	2	1	4	N	
6	4925 S 4620 W – 4460 W – South	2	4	1	N	
7	4955 S 4620 W – 4460 W - North	0	12	3	N	
8	4955 S 4620 W – 4460 W – South	1	5	0	N	
9	4985 S 4620 W – 4460 W - North	2	6	2	N	
10	4985 S 4620 W – 4460 W – South	2	5	6	N	
11	5015 S 4620 W – 4460 W - North	4	4	7	N	
12	5015 S 4620 W – 4460 W – South	11	2	2	N	
13	4460 W 4865 S – 5015 S - West	0	1	2	N	
14	4460 W 4865 S – 5015 S - East	1	7	5	N	
Section 5						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4955 S 4460 W – 4420 W - North	0	0	0	N	
2	4955 S 4460 W – 4420 W – South	1	0	0	N	
3	4420 W 4865 S – 5015 S - West	3	2	4	N	
4	4420 W 4865 S – 5015 S – East	7	0	4	N	
5	4380 W 4865 S – 4985 S - West	3	11	6	N	
6	4380 W 4865 S – 4985 S – East	0	4	4	N	
7	4340 W 4865 S – 4985 S - West	1	10	1	Y	
8	4340 W 4865 S – 4985 S – East	3	8	5	Y	
9	4300 W 4865 S – 5015 S - West	2	4	3	N	
10	4300 W 4865 S – 5015 S – East	0	0	0	N	
11	4985 S 4420 W – 4300 W - North	0	5	1	Y	
12	4985 S 4420 W – 4300 W – South	0	9	1	Y	
13	5015 S 4460 W – 4300 W - North	5	4	2	N	
14	5015 S 4460 W – 4300 W - South	0	5	5	N	

Section 6* Not Counted Due to Snowstorm						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5055 S 4620 W – 4460 W - North					
2	5055 S 4620 W – 4460 W – South					
3	5100 S 4620 W – 4460 W - North					
4	5100 S 4620 W – 4460 W – South					
5	5135 S 4620 W – 4420 W - North					
6	5135 S 4620 W – 4420 W – South					
7	5175 S 4620 W – 4420 W - North					
8	5175 S 4620 W – 4420 W – South					
9	5215 S 4620 W – 4420 W - North					
10	5215 S 4620 W – 4420 W – South					
11	4520 W 5135 S – 5215 S - West					
12	4520 W 5135 S – 5215 S – East					
13	4460 W 5015 S – 5135 S - West					
14	4460 W 5015 S – 5135 S – East					
15	4420 W 5015 S – 5215 S - West					
16	4420 W 5015 S – 5215 S - East					
Section 7* Not Counted Due to Snowstorm						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5055 S 4420 W – Alex St - North					
2	5055 S 4420 W – Alex St – South					
3	5100 S 4420 W – Alex St - North					
4	5100 S 4420 W – Alex St – South					
5	5135 S 4420 – Alex St - North					
6	5135 S 4420 – Alex St – South					
7	5175 S 4420 W – Alex St - North					
8	5175 S 4420 W – Alex St - South					
9	5215 S 4420 W – 4245 W - North					
10	5215 S 4420 W – 4245 W – South					

Section 8* Not Counted Due to Snowstorm						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5255 S 4620 W – 4520 W - North					
2	5255 S 4620 W – 4520 W – South					
3	5295 S 4620 W – 4420 W - North					
4	5295 S 4620 W – 4420 W – South					
5	5335 S 4620 W – 4460 W - North					
6	5335 S 4620 W – 4460 W – South					
7	5375 S 4620 W – 4460 W - North					
8	5375 S 4620 W – 4460 W – South					
9	4520 W 5215 S – 4295 S - West					
10	4520 W 5215 S – 4295 S – East					
11	4460 W 5295 S – 5415 S - West					
12	4460 W 5295 S – 5415 S – East					
13	4420 W 5215 S – 5415 S - West					
14	4420 W 5215 S – 5415 S – East					
15	5415 S 4620 W – 4420 W – North					
16	5415 S 4620 W – 4420 W – South					
Section 9* Not Counted Due to Snowstorm						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5255 S 4420 W – 4320 W - North					
2	5255 S 4420 W – 4320 W – South					
3	5295 S 4420 W – 4320 W - North					
4	5295 S 4420 W – 4320 W – South					
5	5335 S 4420 W – 4320 W - North					
6	5335 S 4420 W – 4320 W – South					
7	5375 S 4420 W – 4280 W - North					
8	5375 S 4420 W – 4280 W – South					
9	4320 W 5215 S – 5415 S - West					
10	4320 W 5215 S – 5415 S – East					
11	4280 W 5215 S – 5375 S - West					
12	4280 W 5215 S – 5375 S – East					
13	5415 S 4420 W – 4220 W - North					
14	5415 S 4420 W – 4220 W - South					



Date 12/11/2021
Counters Riley Weaver, Alison Brown

Notes
Data collection bins show 2 hours allotted for each collection period, however, it took about 2.5 hours for each collection period
If a boat, trailer, or RV was parked on the street or in a yard it was counted as a vehicle
No vehicle was double counted in data collection. Each column shows separate vehicles. If a car was on the sidewalk and the street it was counted as a sidewalk blocking vehicle.
Last two data collection periods were completed by Alison by herself. When the other data collectors finished in Area 2 they came over to area 1 to help her finish.

Collection Time: 8:00 AM - 10:00 AM

Section 1						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Townsend Way 5600 W - Planada Way - South	16	3	4	N	
2	5600 W Townsend Way - Planada Way – East	0	0	0	N	
3	Heath Ave Townsend Way - Planada Way -- South	19	0	6	N	
4	Heath Ave Townsend Way - Planada Way – North	6	0	6	N	
5	Planada Way 5600 W - 5245 W – North	16	0	2	N	
6	Planada Way 5600 W - 5245 W – South	9	1	0	N	
7	5575 W Joaquin St – Planada Way - West	5	0	1	N	
8	5575 W Joaquin St – Planada Way - East	4	0	0	N	
9	Joaquin St 5575 W – Heath Ave - North	1	1	0	N	
10	Joaquin St 5575 W – Heath Ave - South	0	0	0	N	
Section 2						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Planada Way – Westslope Dr – East	3	0	1	N	
2	Jeremiah Dr Jeremiah Dr – Westslope Dr – West	2	0	1	N	
3	Jeremiah Dr Jeremiah Dr – Westslope Dr – East	0	0	0	N	
4	5480 W Jeremiah Dr – Westslope Dr - West	8	0	0	N	
5	5480 W Jeremiah Dr – Westslope Dr – East	0	0	0	N	
6	5415 W Planada Way – Westslope Dr - West	5	0	2	N	
7	5415 W Planada Way – Westslope Dr – East	8	0	8	N	
8	Jeremiah Dr Jeremiah Dr – 5415 S – North	5	1	1	N	
9	Jeremiah Dr Jeremiah Dr – 5415 S – South	11	0	3	N	
10	Westslope Dr 5600 W – 5415 W - North	9	0	0	N	
11	Westslope Dr 5600 W – 5415 W - South	9	2	4	N	
Section 3						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Jeremiah Dr 5415 W – La Brea St - North	3	0	1	N	
2	Jeremiah Dr 5415 W – La Brea St - South	2	0	0	N	
3	La Brea St Farah Dr – Westslope Dr - West	7	0	3	N	
4	La Brea St Farah Dr – Westslope Dr - East	3	0	2	N	
5	Farah Dr Planada Way – Westslope Dr - West	9	2	2	N	
6	Farah Dr Planada Way – Westslope Dr – East	10	1	2	N	
7	Stockton St Heath Ave – Westslope Dr - West	5	0	0	N	
8	Stockton St Heath Ave – Westslope Dr – East	4	0	3	N	
9	Westslope Dr La Brea St – Heath Ave - North	12	1	2	N	
10	Westslope Dr La Brea St – Heath Ave – South	17	0	2	N	
11	Heath Ave Planada Way – Westslope Ave - West	14	0	4	N	
12	Heath Ave Planada Way – Westslope Ave – East	8	0	1	N	

Section 4						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Westslope Dr – Mountain Men Dr – East	0	0	0	N	
2	Jeremiah Dr Westslope Dr – Mountain Men Dr - West	3	0	3	N	
3	Jeremiah Dr Westslope Dr – Mountain Men Dr – East	3	0	0	N	
4	5450 W Colter Dr – Mountain Men Dr - West	0	0	0	N	
5	5450 W Colter Dr – Mountain Men Dr – East	3	0	0	N	
6	Colter Dr Jeremiah Dr – Mountain Men Dr - North	7	0	2	N	
7	Colter Dr Jeremiah Dr – Mountain Men Dr - South	6	4	0	N	
8	Mountain Men Dr 5600 W – Tuscan St - North	16	0	11	N	
9	Mountain Men Dr 5600 W – Tuscan St - South	13	1	6	N	
10	Campbell Dr Westslope Dr – 5115 S - West	2	0	0	N	
11	Campbell Dr Westslope Dr – 5115 S - West	2	0	1	N	
12	Heath Ave Westslope Dr – Mountain Men Dr - West	2	0	0	N	
13	Heath Ave Westslope Dr – Mountain Men Dr - East	1	0	1	N	
Section 5						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Mountain Men Dr – Henley Dr – East	0	0	0	N	
2	Lewis Clark Dr Mountain Men Dr – Campbell Dr - North	11	1	4	N	
3	Lewis Clark Dr Mountain Men Dr – Campbell Dr – South	8	1	8	Y	
4	Bailiff Dr Henley Dr – 5420 W - North	10	1	4	N	
5	Bailiff Dr Henley Dr – 5420 W – South	9	0	2	N	
6	5420 W Lewis Clark Dr – Henley Dr - West	3	0	1	N	
7	5420 W Lewis Clark Dr – Henley Dr – East	1	0	1	N	
8	Henley Dr 5600 W – 5200 W - North	5	0	8	N	
9	Henley Dr 5600 W – 5200 W – South	14	0	13	N	
10	Leprechaun Dr Henley Dr – Jededian Dr - North	10	2	7	N	
11	Leprechaun Dr Henley Dr – Jededian Dr - South	3	0	3	N	
12	5115 W Mountain Men Dr – Jededian Dr – North	7	3	5	N	
13	5115 W Mountain Men Dr – Jededian Dr – South	11	4	0	N	
14	Shalee St Leprechaun Dr – 5200 S - West	1	0	0	N	
15	Shalee St Leprechaun Dr – 5200 S – East	3	0	0	N	
16	Jededian Dr Mountain Men Dr – Leprechaun Dr - West	2	0	0	N	
17	Jededian Dr Mountain Men Dr – Leprechaun Dr – East	2	1	3	N	
18	Campbell Dr Mountain Men Dr – 5115 S - West	1	0	2	N	
19	Campbell Dr Mountain Men Dr – 5115 S - East	1	0	1	N	

Section 6						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5185 W Mountain Men Dr – Hoopes St - West	4	2	2	N	
2	5185 W Mountain Men Dr – Hoopes St – East	5	5	4	N	
3	Verde St 5185 W – Westwind Way - North	1	0	2	Y	
4	Verde St 5185 W – Westwind Way - South	1	0	0	N	
5	5160 W Verde St – Hoopes St - West	3	0	2	N	
6	5160 W Verde St – Hoopes St – East	3	0	0	N	
7	5125 S 5160 W – Heath Ave - North	4	0	1	N	
8	5125 S 5160 W – Heath Ave - South	5	0	3	N	
9	Westwind Way Heath Ave – 5125 S – West	8	0	4	N	
10	Westwind Way Heath Ave – 5125 S – East	5	0	0	N	
11	Heath Ave Mountain Men Dr – Hoopes St - West	4	0	3	N	
12	Heath Ave Mountain Men Dr – Hoopes St – East	5	0	2	N	
13	Tuscan St – West	6	0	2	N	
14	Tuscan St – East	6	2	1	N	
Section 7						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Henley Dr – Trident Dr East	0	0	0	N	
2	Parish Dr Henley Dr – Falstaff Dr West	3	2	3	N	
3	Parish Dr Henley Dr – Falstaff Dr East	5	0	1	N	
4	Kemp Dr Henley Dr – Falstaff Dr West	4	0	2	N	
5	Kemp Dr Henley Dr – Falstaff Dr East	2	0	0	N	
6	5420 W Henley Dr – Trident Ln	0	0	1	N	
7	5420 W Henley Dr – Trident Ln	5	0	3	N	
8	Falstaff Dr Parish Dr – 5420 W North	9	0	3	N	
9	Falstaff Dr Parish Dr – 5420 W South	4	3	1	N	
10	Trident Dr 5600 W – 5420 W North	9	3	1	N	
11	Trident Dr 5600 W – 5420 W South	12	3	2	N	
Section 8						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Trident Dr – 5400 S East	2	0	0	N	
2	Nautilus Dr Trident Dr – 5420 W North	8	0	3	N	
3	Nautilus Dr Trident Dr – 5420 W South	14	1	4	N	
4	5495 W Trident Ln – Nautilus Dr West	4	0	0	N	
5	5495 W Trident Ln – Nautilus Dr East	5	0	1	N	
6	5420 W Trident Dr – 5400 S - West	1	0	0	N	
7	5420 W Trident Dr – 5400 S – East	3	0	5	N	
8	5400 S 5600 W – 5420 W - North	0	0	0	N	

Section 9						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Leprechaun Ln 5200 S – 5400 S South	9	1	7	N	
2	Leprechaun Ln 5200 S – 5400 S North	4	0	3	N	
3	5240 S 5385 W – Shalee St North	2	0	1	N	
4	5240 S 5385 W – Shalee St South	3	0	3	N	
5	Shalee St 5200 S – Leperchaun Ln West	1	0	1	N	
6	Shalee St 5200 S – Leperchaun Ln East	2	0	0	N	
7	5240 W 5200 S – 5400 S West	0	14	1	N	All on street cars were on curb.
8	5240 W 5200 S – 5400 S East	3	12	2	N	All on street cars were on curb.
9	5200 W 5200 S – 5400 S West	0	6	2	N	
10	5200 W 5200 S – 5400 S East	9	0	0	N	
11	5400 S 5120 W – 5200 W North	0	0	0	N	
Section 10						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5160 W Hoopes St – 5320 S – West	0	5	1	N	
2	5160 W Hoopes St – 5320 S - East	0	1	1	N	
3	5120 W Hoopes St – 5320 S - West	7	0	4	N	
4	5120 W Hoopes St – 5320 S - East	5	0	2	N	
5	Hoopes Cir South of Hoopes St - West	2	0	5	N	
6	Hoopes Cir South of Hoopes St - East	3	2	0	N	
7	Heath Ave Hoopes St – 5320 S - West	4	0	0	N	
8	Heath Ave Hoopes St – 5320 S – East	0	1	0	Y	
9	5320 S 5160 W – Heath Ave - North	2	3	4	N	
10	5320 S 5160 W – Heath Ave - North	4	1	3	N	
11	Charlotte Ave Hoopes St – 5320 S - West	1	3	1	N	
12	Charlotte Ave Hoopes St – 5320 S - East	2	0	0	N	
Section 11						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5160 W 5320 S – 5415 S West	0	0	0	N	
2	5160 W 5320 S – 5415 S East	4	0	0	N	
3	5030 W 5320 S – 5415 S West	3	1	0	N	
4	5030 W 5320 S – 5415 S East	0	0	0	N	
5	Heath Ave 5320 S – 5100 S West	5	2	2	N	
6	Heath Ave 5320 S – 5100 S East	17	0	6	Y	
7	5360 S 5160 W – Heath Ave North	8	1	1	N	
8	5360 S 5160 W – Heath Ave South	7	1	3	N	
9	5415 S 5160 W – Heath Ave North	0	0	0	N	
10	Charlotte Ave Cross St – 5415 S - West	1	2	6	N	
11	Charlotte Ave Cross St – 5415 S - East	5	0	4	N	

Section 12						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5245 W Planada Way – Dead End - West	9	0	2	N	
2	5245 W Planada Way – Dead End - East	6	3	0	N	
3	Stockton St Heath Ave – 5245 E North	1	0	3	N	
4	Stockton St Heath Ave – 5245 E South	2	0	0	N	

Collection Time: 12:00 PM - 2:00 PM

Section 1						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Townsend Way 5600 W - Planada Way - South	15	1	8	N	
2	5600 W Townsend Way - Planada Way – East	0	0	0	N	
3	Heath Ave Townsend Way - Planada Way -- South	17	0	6	N	
4	Heath Ave Townsend Way - Planada Way – North	5	1	0	N	
5	Planada Way 5600 W - 5245 W – North	10	0	3	N	
6	Planada Way 5600 W - 5245 W – South	9	1	2	N	
7	5575 W Joaquin St – Planada Way - West	4	1	1	N	
8	5575 W Joaquin St – Planada Way - East	3	0	0	N	
9	Joaquin St 5575 W – Heath Ave - North	1	0	0	N	
10	Joaquin St 5575 W – Heath Ave - South	1	0	0	N	
Section 2						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Planada Way – Westslope Dr – East	0	0	0	N	
2	Jeremiah Dr Jeremiah Dr – Westslope Dr – West	2	1	1	N	
3	Jeremiah Dr Jeremiah Dr – Westslope Dr – East	3	0	2	N	
4	5480 W Jeremiah Dr – Westslope Dr - West	4	0	0	N	
5	5480 W Jeremiah Dr – Westslope Dr – East	0	0	0	N	
6	5415 W Planada Way – Westslope Dr - West	5	0	4	N	
7	5415 W Planada Way – Westslope Dr – East	7	0	3	N	
8	Jeremiah Dr Jeremiah Dr – 5415 S – North	6	0	1	N	
9	Jeremiah Dr Jeremiah Dr – 5415 S – South	13	1	3	N	
10	Westslope Dr 5600 W – 5415 W - North	9	1	2	N	
11	Westslope Dr 5600 W – 5415 W - South	10	2	1	N	
Section 3						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Jeremiah Dr 5415 W – La Brea St - North	2	1	0	N	
2	Jeremiah Dr 5415 W – La Brea St - South	2	0	0	N	
3	La Brea St Farah Dr – Westslope Dr - West	4	0	0	N	
4	La Brea St Farah Dr – Westslope Dr - East	2	1	2	N	
5	Farah Dr Planada Way – Westslope Dr - West	9	0	2	N	
6	Farah Dr Planada Way – Westslope Dr – East	11	0	2	N	
7	Stockton St Heath Ave – Westslope Dr - West	3	0	6	N	
8	Stockton St Heath Ave – Westslope Dr – East	6	1	2	N	
9	Westslope Dr La Brea St – Heath Ave - North	11	0	5	N	Boats parked in yard
10	Westslope Dr La Brea St – Heath Ave – South	15	1	3	N	
11	Heath Ave Planada Way – Westslope Ave - West	11	0	0	N	
12	Heath Ave Planada Way – Westslope Ave – East	12	0	0	N	

Section 4						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Westslope Dr – Mountain Men Dr – East	0	0	0	N	
2	Jeremiah Dr Westslope Dr – Mountain Men Dr - West	4	0	5	N	
3	Jeremiah Dr Westslope Dr – Mountain Men Dr – East	2	0	0	N	
4	5450 W Colter Dr – Mountain Men Dr - West	1	1	0	N	
5	5450 W Colter Dr – Mountain Men Dr – East	0	1	0	N	
6	Colter Dr Jeremiah Dr – Mountain Men Dr - North	11	3	2	N	
7	Colter Dr Jeremiah Dr – Mountain Men Dr - South	11	1	2	N	
8	Mountain Men Dr 5600 W – Tuscan St - North	13	1	9	N	
9	Mountain Men Dr 5600 W – Tuscan St - South	13	0	3	N	
10	Campbell Dr Westslope Dr – 5115 S - West	2	0	0	N	
11	Campbell Dr Westslope Dr – 5115 S - West	0	0	0	N	
12	Heath Ave Westslope Dr – Mountain Men Dr - West	5	1	0	N	
13	Heath Ave Westslope Dr – Mountain Men Dr - East	3	0	0	N	
Section 5						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Mountain Men Dr – Henley Dr – East	0	0	0	N	
2	Lewis Clark Dr Mountain Men Dr – Campbell Dr - North	9	3	2	N	
3	Lewis Clark Dr Mountain Men Dr – Campbell Dr – South	8	1	7	N	
4	Bailiff Dr Henley Dr – 5420 W - North	10	2	4	N	
5	Bailiff Dr Henley Dr – 5420 W – South	8	0	2	N	
6	5420 W Lewis Clark Dr – Henley Dr - West	5	0	0	N	
7	5420 W Lewis Clark Dr – Henley Dr – East	1	0	0	N	
8	Henley Dr 5600 W – 5200 W - North	7	0	8	N	
9	Henley Dr 5600 W – 5200 W – South	12	0	9	N	
10	Leprechaun Dr Henley Dr – Jededian Dr - North	12	0	6	N	
11	Leprechaun Dr Henley Dr – Jededian Dr - South	4	2	1	N	
12	5115 W Mountain Men Dr – Jededian Dr – North	5	3	1	N	
13	5115 W Mountain Men Dr – Jededian Dr – South	9	2	2	N	
14	Shalee St Leprechaun Dr – 5200 S - West	2	0	0	N	
15	Shalee St Leprechaun Dr – 5200 S – East	1	0	0	N	
16	Jededian Dr Mountain Men Dr – Leprechaun Dr - West	2	0	1	N	
17	Jededian Dr Mountain Men Dr – Leprechaun Dr – East	5	0	3	N	
18	Campbell Dr Mountain Men Dr – 5115 S - West	1	0	0	N	
19	Campbell Dr Mountain Men Dr – 5115 S - East	1	0	1	N	

Section 6						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5185 W Mountain Men Dr – Hoopes St - West	6	3	2	N	
2	5185 W Mountain Men Dr – Hoopes St – East	7	0	3	N	
3	Verde St 5185 W – Westwind Way - North	2	0	4	Y	
4	Verde St 5185 W – Westwind Way - South	3	0	0	Y	
5	5160 W Verde St – Hoopes St - West	3	0	2	N	
6	5160 W Verde St – Hoopes St – East	3	0	0	N	
7	5125 S 5160 W – Heath Ave - North	3	0	1	N	
8	5125 S 5160 W – Heath Ave - South	5	0	3	N	
9	Westwind Way Heath Ave – 5125 S – West	4	0	5	N	
10	Westwind Way Heath Ave – 5125 S – East	5	0	1	N	
11	Heath Ave Mountain Men Dr – Hoopes St - West	4	0	3	N	
12	Heath Ave Mountain Men Dr – Hoopes St – East	4	0	0	N	
13	Tuscan St – West	7	2	2	N	
14	Tuscan St – East	5	0	0	N	
Section 7						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Henley Dr – Trident Dr East	0	0	0	N	
2	Parish Dr Henley Dr – Falstaff Dr West	2	1	3	N	
3	Parish Dr Henley Dr – Falstaff Dr East	2	0	3	N	
4	Kemp Dr Henley Dr – Falstaff Dr West	2	0	1	N	
5	Kemp Dr Henley Dr – Falstaff Dr East	2	0	0	N	
6	5420 W Henley Dr – Trident Ln	0	0	0	N	
7	5420 W Henley Dr – Trident Ln	4	0	2	N	
8	Falstaff Dr Parish Dr – 5420 W North	10	0	2	N	
9	Falstaff Dr Parish Dr – 5420 W South	5	2	1	N	
10	Trident Dr 5600 W – 5420 W North	6	2	2	N	
11	Trident Dr 5600 W – 5420 W South	13	1	0	N	
Section 8						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Trident Dr – 5400 S East	0	0	0	N	
2	Nautilus Dr Trident Dr – 5420 W North	5	0	2	N	
3	Nautilus Dr Trident Dr – 5420 W South	12	1	7	N	
4	5495 W Trident Ln – Nautilus Dr West	3	0	3	N	
5	5495 W Trident Ln – Nautilus Dr East	3	0	0	N	
6	5420 W Trident Dr – 5400 S - West	2	0	0	N	
7	5420 W Trident Dr – 5400 S – East	0	0	1	N	
8	5400 S 5600 W – 5420 W - North	0	0	0	N	

Section 9						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Leprechaun Ln 5200 S – 5400 S South	6	0	4	N	
2	Leprechaun Ln 5200 S – 5400 S North	9	2	5	N	
3	5240 S 5385 W – Shalee St North	1	0	1	N	
4	5240 S 5385 W – Shalee St South	3	0	5	N	
5	Shalee St 5200 S – Leperchaun Ln West	1	0	1	N	
6	Shalee St 5200 S – Leperchaun Ln East	1	0	0	N	
7	5240 W 5200 S – 5400 S West	0	11	6	N	
8	5240 W 5200 S – 5400 S East	0	10	2	N	
9	5200 W 5200 S – 5400 S West	0	8	4	N	
10	5200 W 5200 S – 5400 S East	6	0	0	N	
11	5400 S 5120 W – 5200 W North	0	0	0	N	
Section 10						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5160 W Hoopes St – 5320 S – West	2	1	2	N	
2	5160 W Hoopes St – 5320 S - East	0	1	0	N	
3	5120 W Hoopes St – 5320 S - West	2	5	3	N	
4	5120 W Hoopes St – 5320 S - East	4	2	1	N	
5	Hoopes Cir South of Hoopes St - West	2	0	6	N	
6	Hoopes Cir South of Hoopes St - East	3	2	0	N	
7	Heath Ave Hoopes St – 5320 S - West	0	0	0	N	
8	Heath Ave Hoopes St – 5320 S – East	3	1	1	N	
9	5320 S 5160 W – Heath Ave - North	2	2	6	N	
10	5320 S 5160 W – Heath Ave - North	6	1	3	N	
11	Charlotte Ave Hoopes St – 5320 S - West	2	2	1	N	
12	Charlotte Ave Hoopes St – 5320 S - East	2	1	0	N	
Section 11						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5160 W 5320 S – 5415 S West	0	0	0	N	
2	5160 W 5320 S – 5415 S East	2	0	0	N	
3	5030 W 5320 S – 5415 S West	2	0	1	N	
4	5030 W 5320 S – 5415 S East	1	1	0	N	
5	Heath Ave 5320 S – 5100 S West	7	2	1	N	
6	Heath Ave 5320 S – 5100 S East	16	0	3	N	
7	5360 S 5160 W – Heath Ave North	11	2	1	N	
8	5360 S 5160 W – Heath Ave South	4	2	1	N	
9	5415 S 5160 W – Heath Ave North	0	0	0	N	
10	Charlotte Ave Cross St – 5415 S - West	2	1	2	N	
11	Charlotte Ave Cross St – 5415 S - East	6	0	6	N	

Section 12						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5245 W Planada Way – Dead End - West	10	0	2	Y	Truck with a trailer on corner
2	5245 W Planada Way – Dead End - East	4	2	0	N	
3	Stockton St Heath Ave – 5245 E North	1	3	0	N	
4	Stockton St Heath Ave – 5245 E South	1	0	0	N	

Collection Time: 5:00 PM - 7:00 PM

Section 1						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Townsend Way 5600 W - Planada Way - South	14	3	2	N	
2	5600 W Townsend Way - Planada Way – East	0	0	0	N	
3	Heath Ave Townsend Way - Planada Way -- South	17	1	8	N	
4	Heath Ave Townsend Way - Planada Way – North	7	0	9	N	
5	Planada Way 5600 W - 5245 W – North	9	0	2	N	
6	Planada Way 5600 W - 5245 W – South	6	1	3	N	
7	5575 W Joaquin St – Planada Way - West	5	1	2	N	
8	5575 W Joaquin St – Planada Way - East	5	0	0	N	
9	Joaquin St 5575 W – Heath Ave - North	1	0	0	N	
10	Joaquin St 5575 W – Heath Ave - South	0	0	0	N	
Section 2						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Planada Way – Westslope Dr – East	0	0	0	N	
2	Jeremiah Dr Jeremiah Dr – Westslope Dr – West	0	1	2	N	
3	Jeremiah Dr Jeremiah Dr – Westslope Dr – East	2	0	1	N	
4	5480 W Jeremiah Dr – Westslope Dr - West	1	0	0	N	
5	5480 W Jeremiah Dr – Westslope Dr – East	6	0	0	N	
6	5415 W Planada Way – Westslope Dr - West	5	0	3	N	
7	5415 W Planada Way – Westslope Dr – East	7	0	2	N	
8	Jeremiah Dr Jeremiah Dr – 5415 S – North	6	0	0	N	
9	Jeremiah Dr Jeremiah Dr – 5415 S – South	10	1	0	N	
10	Westslope Dr 5600 W – 5415 W - North	10	1	2	N	
11	Westslope Dr 5600 W – 5415 W - South	10	1	4	N	
Section 3						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Jeremiah Dr 5415 W – La Brea St - North	1	1	0	N	
2	Jeremiah Dr 5415 W – La Brea St - South	2	0	0	N	
3	La Brea St Farah Dr – Westslope Dr - West	8	0	6	N	
4	La Brea St Farah Dr – Westslope Dr - East	3	0	3	N	
5	Farah Dr Planada Way – Westslope Dr - West	13	0	0	N	
6	Farah Dr Planada Way – Westslope Dr – East	8	1	3	N	
7	Stockton St Heath Ave – Westslope Dr - West	5	1	1	N	
8	Stockton St Heath Ave – Westslope Dr – East	5	0	2	N	
9	Westslope Dr La Brea St – Heath Ave - North	9	0	3	N	
10	Westslope Dr La Brea St – Heath Ave – South	17	1	2	N	
11	Heath Ave Planada Way – Westslope Ave - West	11	0	0	N	
12	Heath Ave Planada Way – Westslope Ave – East	11	0	2	N	

Section 4

#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Westslope Dr – Mountain Men Dr – East	0	0	0	N	
2	Jeremiah Dr Westslope Dr – Mountain Men Dr - West	6	0	1	N	
3	Jeremiah Dr Westslope Dr – Mountain Men Dr – East	8	0	0	N	
4	5450 W Colter Dr – Mountain Men Dr - West	2	0	4	N	
5	5450 W Colter Dr – Mountain Men Dr – East	1	0	3	N	
6	Colter Dr Jeremiah Dr – Mountain Men Dr - North	8	0	5	N	
7	Colter Dr Jeremiah Dr – Mountain Men Dr - South	11	1	5	N	
8	Mountain Men Dr 5600 W – Tuscan St - North	20	0	10	N	holiday party?
9	Mountain Men Dr 5600 W – Tuscan St - South	16	1	3	N	
10	Campbell Dr Westslope Dr – 5115 S - West	1	0	0	N	
11	Campbell Dr Westslope Dr – 5115 S - West	2	0	1	N	
12	Heath Ave Westslope Dr – Mountain Men Dr - West	2	0	0	N	
13	Heath Ave Westslope Dr – Mountain Men Dr - East	5	0	0	N	

Section 5

#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Mountain Men Dr – Henley Dr – East	0	0	0	N	
2	Lewis Clark Dr Mountain Men Dr – Campbell Dr - North	11	0	6	Y	
3	Lewis Clark Dr Mountain Men Dr – Campbell Dr – South	15	2	5	N	
4	Bailiff Dr Henley Dr – 5420 W - North	11	0	5	N	
5	Bailiff Dr Henley Dr – 5420 W – South	9	0	3	N	
6	5420 W Lewis Clark Dr – Henley Dr - West	2	0	0	N	
7	5420 W Lewis Clark Dr – Henley Dr – East	2	0	0	N	
8	Henley Dr 5600 W – 5200 W - North	7	1	1	N	
9	Henley Dr 5600 W – 5200 W – South	14	0	2	N	
10	Leprechaun Dr Henley Dr – Jededian Dr - North	11	1	4	N	
11	Leprechaun Dr Henley Dr – Jededian Dr - South	8	0	5	N	
12	5115 W Mountain Men Dr – Jededian Dr – North	3	3	3	N	
13	5115 W Mountain Men Dr – Jededian Dr – South	8	3	1	N	
14	Shalee St Leprechaun Dr – 5200 S - West	1	0	0	N	
15	Shalee St Leprechaun Dr – 5200 S – East	1	0	0	N	
16	Jededian Dr Mountain Men Dr – Leprechaun Dr - West	4	0	0	N	
17	Jededian Dr Mountain Men Dr – Leprechaun Dr – East	4	0	3	N	
18	Campbell Dr Mountain Men Dr – 5115 S - West	2	0	2	N	
19	Campbell Dr Mountain Men Dr – 5115 S - East	0	0	1	N	

Section 6						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5185 W Mountain Men Dr – Hoopes St - West	8	0	3	N	
2	5185 W Mountain Men Dr – Hoopes St – East	6	2	7	N	
3	Verde St 5185 W – Westwind Way - North	0	0	0	N	
4	Verde St 5185 W – Westwind Way - South	1	0	2	N	
5	5160 W Verde St – Hoopes St - West	2	0	2	Y	
6	5160 W Verde St – Hoopes St – East	3	0	4	Y	
7	5125 S 5160 W – Heath Ave - North	3	0	1	Y	
8	5125 S 5160 W – Heath Ave - South	9	0	4	Y	
9	Westwind Way Heath Ave – 5125 S – West	5	0	8	N	
10	Westwind Way Heath Ave – 5125 S – East	7	0	2	N	
11	Heath Ave Mountain Men Dr – Hoopes St - West	6	0	6	Y	
12	Heath Ave Mountain Men Dr – Hoopes St – East	6	0	5	Y	
13	Tuscan St – West	6	0	4	N	
14	Tuscan St – East	4	2	1	N	
Section 7						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Henley Dr – Trident Dr East	0	0	0	N	
2	Parish Dr Henley Dr – Falstaff Dr West	2	0	2	N	
3	Parish Dr Henley Dr – Falstaff Dr East	3	0	1	N	
4	Kemp Dr Henley Dr – Falstaff Dr West	1	0	0	N	
5	Kemp Dr Henley Dr – Falstaff Dr East	1	0	0	N	
6	5420 W Henley Dr – Trident Ln	2	0	0	N	
7	5420 W Henley Dr – Trident Ln	3	0	0	N	
8	Falstaff Dr Parish Dr – 5420 W North	13	0	5	N	
9	Falstaff Dr Parish Dr – 5420 W South	6	1	2	N	
10	Trident Dr 5600 W – 5420 W North	7	0	0	N	
11	Trident Dr 5600 W – 5420 W South	7	3	1	N	
Section 8						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Trident Dr – 5400 S East	0	0	0	N	
2	Nautilus Dr Trident Dr – 5420 W North	7	0	6	N	
3	Nautilus Dr Trident Dr – 5420 W South	13	1	4	N	
4	5495 W Trident Ln – Nautilus Dr West	4	0	0	N	
5	5495 W Trident Ln – Nautilus Dr East	5	0	0	N	
6	5420 W Trident Dr – 5400 S - West	3	0	0	N	
7	5420 W Trident Dr – 5400 S – East	6	0	3	N	
8	5400 S 5600 W – 5420 W - North	0	0	0	N	

Section 9						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Leprechaun Ln 5200 S – 5400 S South	4	0	5	N	
2	Leprechaun Ln 5200 S – 5400 S North	12	0	6	N	
3	5240 S 5385 W – Shalee St North	3	0	2	N	
4	5240 S 5385 W – Shalee St South	3	0	1	N	
5	Shalee St 5200 S – Leperchaun Ln West	3	0	0	N	
6	Shalee St 5200 S – Leperchaun Ln East	1	0	1	N	
7	5240 W 5200 S – 5400 S West	4	16	0	N	
8	5240 W 5200 S – 5400 S East	2	13	0	N	
9	5200 W 5200 S – 5400 S West	0	6	2	N	
10	5200 W 5200 S – 5400 S East	8	0	3	N	
11	5400 S 5120 W – 5200 W North	0	0	0	N	
Section 10						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5160 W Hoopes St – 5320 S – West	1	2	0	N	
2	5160 W Hoopes St – 5320 S - East	0	0	1	N	
3	5120 W Hoopes St – 5320 S - West	9	1	2	N	
4	5120 W Hoopes St – 5320 S - East	6	1	1	N	
5	Hoopes Cir South of Hoopes St - West	3	1	3	N	
6	Hoopes Cir South of Hoopes St - East	4	3	1	N	
7	Heath Ave Hoopes St – 5320 S - West	3	0	0	N	
8	Heath Ave Hoopes St – 5320 S – East	8	0	1	N	
9	5320 S 5160 W – Heath Ave - North	4	0	10	N	
10	5320 S 5160 W – Heath Ave - North	9	0	1	N	
11	Charlotte Ave Hoopes St – 5320 S - West	1	3	0	Y	
12	Charlotte Ave Hoopes St – 5320 S - East	4	3	0	Y	
Section 11						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5160 W 5320 S – 5415 S West	3	1	1	N	
2	5160 W 5320 S – 5415 S East	0	0	0	N	
3	5030 W 5320 S – 5415 S West	2	0	2	N	
4	5030 W 5320 S – 5415 S East	0	1	1	N	
5	Heath Ave 5320 S – 5100 S West	8	0	2	N	
6	Heath Ave 5320 S – 5100 S East	11	1	2	N	
7	5360 S 5160 W – Heath Ave North	9	2	4	Y	
8	5360 S 5160 W – Heath Ave South	7	3	6	Y	
9	5415 S 5160 W – Heath Ave North	0	0	6	N	
10	Charlotte Ave Cross St – 5415 S - West	4	1	4	N	
11	Charlotte Ave Cross St – 5415 S - East	4	0	7	N	

Section 12						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5245 W Planada Way – Dead End - West	8	1	2	Y	
2	5245 W Planada Way – Dead End - East	5	0	5	N	
3	Stockton St Heath Ave – 5245 E North	1	0	2	N	
4	Stockton St Heath Ave – 5245 E South	1	0	0	N	

Collection Time: 9:00 PM - 11:00 PM

Section 1						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Townsend Way 5600 W - Planada Way - South	7	4	7	N	
2	5600 W Townsend Way - Planada Way – East	0	0	0	N	
3	Heath Ave Townsend Way - Planada Way -- South	15	0	4	N	
4	Heath Ave Townsend Way - Planada Way – North	9	0	7	N	
5	Planada Way 5600 W - 5245 W – North	14	0	2	N	
6	Planada Way 5600 W - 5245 W – South	10	1	1	N	
7	5575 W Joaquin St – Planada Way - West	7	1	1	N	
8	5575 W Joaquin St – Planada Way - East	5	0	0	N	
9	Joaquin St 5575 W – Heath Ave - North	1	0	0	N	
10	Joaquin St 5575 W – Heath Ave - South	0	0	0	N	
Section 2						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Planada Way – Westslope Dr – East	0	0	0	N	
2	Jeremiah Dr Jeremiah Dr – Westslope Dr – West	2	1	2	N	
3	Jeremiah Dr Jeremiah Dr – Westslope Dr – East	3	0	0	N	
4	5480 W Jeremiah Dr – Westslope Dr - West	4	0	0	N	
5	5480 W Jeremiah Dr – Westslope Dr – East	5	0	0	N	
6	5415 W Planada Way – Westslope Dr - West	7	1	3	N	
7	5415 W Planada Way – Westslope Dr – East	7	0	1	N	
8	Jeremiah Dr Jeremiah Dr – 5415 S – North	6	0	3	N	
9	Jeremiah Dr Jeremiah Dr – 5415 S – South	6	0	0	N	
10	Westslope Dr 5600 W – 5415 W - North	10	0	3	N	
11	Westslope Dr 5600 W – 5415 W - South	7	0	1	N	
Section 3						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Jeremiah Dr 5415 W – La Brea St - North	1	0	0	N	
2	Jeremiah Dr 5415 W – La Brea St - South	2	0	0	N	
3	La Brea St Farah Dr – Westslope Dr - West	5	0	2	N	
4	La Brea St Farah Dr – Westslope Dr - East	3	1	0	N	
5	Farah Dr Planada Way – Westslope Dr - West	10	0	0	N	
6	Farah Dr Planada Way – Westslope Dr – East	14	1	3	N	
7	Stockton St Heath Ave – Westslope Dr - West	6	1	4	N	
8	Stockton St Heath Ave – Westslope Dr – East	4	0	0	N	
9	Westslope Dr La Brea St – Heath Ave - North	13	1	1	N	
10	Westslope Dr La Brea St – Heath Ave – South	19	0	1	N	
11	Heath Ave Planada Way – Westslope Ave - West	14	0	0	N	
12	Heath Ave Planada Way – Westslope Ave – East	10	0	0	N	

Section 4						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Westslope Dr – Mountain Men Dr – East	0	0	0	N	
2	Jeremiah Dr Westslope Dr – Mountain Men Dr - West	5	0	0	N	
3	Jeremiah Dr Westslope Dr – Mountain Men Dr – East	8	0	0	N	
4	5450 W Colter Dr – Mountain Men Dr - West	3	0	2	N	
5	5450 W Colter Dr – Mountain Men Dr – East	1	0	2	N	
6	Colter Dr Jeremiah Dr – Mountain Men Dr - North	10	0	1	N	
7	Colter Dr Jeremiah Dr – Mountain Men Dr - South	12	2	2	N	
8	Mountain Men Dr 5600 W – Tuscan St - North	21	0	5	N	
9	Mountain Men Dr 5600 W – Tuscan St - South	14	1	4	N	
10	Campbell Dr Westslope Dr – 5115 S - West	1	0	0	N	
11	Campbell Dr Westslope Dr – 5115 S - West	1	0	0	N	
12	Heath Ave Westslope Dr – Mountain Men Dr - West	6	1	0	N	
13	Heath Ave Westslope Dr – Mountain Men Dr - East	2	0	0	N	
Section 5						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Mountain Men Dr – Henley Dr – East	0	0	0	N	
2	Lewis Clark Dr Mountain Men Dr – Campbell Dr - North	12	1	6	N	
3	Lewis Clark Dr Mountain Men Dr – Campbell Dr – South	14	5	9	N	
4	Bailiff Dr Henley Dr – 5420 W - North	10	1	9	N	
5	Bailiff Dr Henley Dr – 5420 W – South	13	0	3	Y	
6	5420 W Lewis Clark Dr – Henley Dr - West	4	0	0	N	
7	5420 W Lewis Clark Dr – Henley Dr – East	1	0	0	N	
8	Henley Dr 5600 W – 5200 W - North	12	0	8	N	
9	Henley Dr 5600 W – 5200 W – South	15	0	13	N	
10	Leprechaun Dr Henley Dr – Jededian Dr - North	12	1	7	Y	
11	Leprechaun Dr Henley Dr – Jededian Dr - South	6	1	5	Y	
12	5115 W Mountain Men Dr – Jededian Dr – North	5	3	2	N	
13	5115 W Mountain Men Dr – Jededian Dr – South	14	6	6	N	
14	Shalee St Leprechaun Dr – 5200 S - West	3	0	0	N	
15	Shalee St Leprechaun Dr – 5200 S – East	2	0	0	N	
16	Jededian Dr Mountain Men Dr – Leprechaun Dr - West	2	0	1	Y	
17	Jededian Dr Mountain Men Dr – Leprechaun Dr – East	7	2	6	Y	
18	Campbell Dr Mountain Men Dr – 5115 S - West	2	0	1	Y	
19	Campbell Dr Mountain Men Dr – 5115 S - East	6	0	0	Y	

Section 6

#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5185 W Mountain Men Dr – Hoopes St - West	5	1	0	N	
2	5185 W Mountain Men Dr – Hoopes St – East	7	1	2	N	
3	Verde St 5185 W – Westwind Way - North	2	0	2	N	Y
4	Verde St 5185 W – Westwind Way - South	1	0	0	N	Y
5	5160 W Verde St – Hoopes St - West	1	0	0	N	
6	5160 W Verde St – Hoopes St – East	2	0	0	N	
7	5125 S 5160 W – Heath Ave - North	3	0	0	N	Y
8	5125 S 5160 W – Heath Ave - South	4	0	1	N	Y
9	Westwind Way Heath Ave – 5125 S – West	7	0	1	N	
10	Westwind Way Heath Ave – 5125 S – East	9	0	3	N	Y
11	Heath Ave Mountain Men Dr – Hoopes St - West	5	0	1	N	
12	Heath Ave Mountain Men Dr – Hoopes St – East	4	0	0	N	
13	Tuscan St – West	5	0	0	N	
14	Tuscan St – East	5	2	0	N	

Section 7

#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Henley Dr – Trident Dr East	0	0	0	N	
2	Parish Dr Henley Dr – Falstaff Dr West	3	0	1	N	
3	Parish Dr Henley Dr – Falstaff Dr East	3	0	0	N	
4	Kemp Dr Henley Dr – Falstaff Dr West	1	0	1	N	
5	Kemp Dr Henley Dr – Falstaff Dr East	1	0	1	N	
6	5420 W Henley Dr – Trident Ln	2	0	0	N	
7	5420 W Henley Dr – Trident Ln	4	0	4	N	
8	Falstaff Dr Parish Dr – 5420 W North	7	2	0	N	
9	Falstaff Dr Parish Dr – 5420 W South	13	0	3	N	
10	Trident Dr 5600 W – 5420 W North	9	3	1	N	
11	Trident Dr 5600 W – 5420 W South	12	0	4	N	

** harder to see yards at night**

Section 8

#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5600 W Trident Dr – 5400 S East	0	0	1	N	
2	Nautilus Dr Trident Dr – 5420 W North	18	1	4	N	
3	Nautilus Dr Trident Dr – 5420 W South	10	0	3	N	
4	5495 W Trident Ln – Nautilus Dr West	8	1	0	Y	
5	5495 W Trident Ln – Nautilus Dr East	7	1	0	Y	
6	5420 W Trident Dr – 5400 S - West	3	2	1	N	
7	5420 W Trident Dr – 5400 S – East	5	0	0	N	
8	5400 S 5600 W – 5420 W - North	0	0	0	N	

Section 9						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	Leprechaun Ln 5200 S – 5400 S South	17	2	7	N	
2	Leprechaun Ln 5200 S – 5400 S North	4	0	8	N	
3	5240 S 5385 W – Shalee St North	4	1	1	Y	
4	5240 S 5385 W – Shalee St South	3	0	5	Y	
5	Shalee St 5200 S – Leperchaun Ln West	5	0	0	N	
6	Shalee St 5200 S – Leperchaun Ln East	3	2	0	N	
7	5240 W 5200 S – 5400 S West	3	15	9	N	
8	5240 W 5200 S – 5400 S East	15	3	2	N	
9	5200 W 5200 S – 5400 S West	4	4	4	N	
10	5200 W 5200 S – 5400 S East	10	1	4	N	
11	5400 S 5120 W – 5200 W North	0	0	0	N	
Section 10						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5160 W Hoopes St – 5320 S – West	4	0	1	N	
2	5160 W Hoopes St – 5320 S - East	3	0	0	N	
3	5120 W Hoopes St – 5320 S - West	12	0	1	N	
4	5120 W Hoopes St – 5320 S - East	8	1	3	N	
5	Hoopes Cir South of Hoopes St - West	5	1	6	N	
6	Hoopes Cir South of Hoopes St - East	3	4	1	N	
7	Heath Ave Hoopes St – 5320 S - West	3	0	0	N	
8	Heath Ave Hoopes St – 5320 S – East	5	0	4	N	
9	5320 S 5160 W – Heath Ave - North	2	2	12	N	
10	5320 S 5160 W – Heath Ave - North	8	2	0	N	
11	Charlotte Ave Hoopes St – 5320 S - West	2	4	0	Y	
12	Charlotte Ave Hoopes St – 5320 S - East	4	3	0	Y	
Section 11						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5160 W 5320 S – 5415 S West	2	0	1	N	
2	5160 W 5320 S – 5415 S East	0	0	1	N	
3	5030 W 5320 S – 5415 S West	0	3	1	N	
4	5030 W 5320 S – 5415 S East	1	0	1	N	
5	Heath Ave 5320 S – 5100 S West	7	0	3	N	
6	Heath Ave 5320 S – 5100 S East	13	0	7	N	
7	5360 S 5160 W – Heath Ave North	4	1	0	N	
8	5360 S 5160 W – Heath Ave South	6	1	4	N	
9	5415 S 5160 W – Heath Ave North	0	0	4	N	
10	Charlotte Ave Cross St – 5415 S - West	3	3	5	N	
11	Charlotte Ave Cross St – 5415 S - East	5	1	3	N	

Section 12						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5245 W Planada Way – Dead End - West	10	0	2	N	
2	5245 W Planada Way – Dead End - East	5	2	2	N	
3	Stockton St Heath Ave – 5245 E North	2	0	2	N	
4	Stockton St Heath Ave – 5245 E South	1	0	0	N	



Date 12/11/2021

Counters Mitchell Hadfield, Jonah Henckel

Notes

It snowed a few days prior to data collection. More cars may have been parked on the sidewalk than normal to avoid snow plows.

If a boat, trailer, or RV was parked on the street or in a yard it was counted as a vehicle

No vehicle was double counted in data collection. Each column shows separate vehicles. If a car was on the sidewalk and the street it was counted as a sidewalk blocking vehicle.

Snow made it difficult in some situations to decide what vehicles were on driveways vs. yards

Collection Time: 8:00 AM - 10:00 AM

Section 1						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4720 W 4715 S – 4865 S - West	1	5	4	N	
2	4720 W 4715 S – 4865 S – East	0	4	5	N	
3	4715 S 4720 W – 4620 W - South	2	0	0	N	
4	4620 W 4715 S – 4865 S - West	2	0	4	N	
5	4620 W 4715 S – 4865 S - West	0	2	1	N	
6	4865 S 4620 W – 4520 W - North	2	1	1	N	
7	4865 S 4620 W – 4520 W - North	1	0	2	N	
Section 2						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4715 S 4520 W – 4420 W – South	0	0	1	N	
2	4420 W 4715 S – 4866 S - West	2	0	3	N	
3	4420 W 4715 S – 4866 S – East	1	3	3	N	
4	4865 S 4520 W – 4420 S - North	3	0	2	N	
5	4865 S 4520 W – 4420 S - South	3	0	1	N	
Section 3						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4715 S 4420 W – 4300 W – South	0	0	3	N	
2	4745 S 4420 W – 4300 W – North	0	3	3	N	
3	4745 S 4420 W – 4300 W – South	1	1	0	N	
4	4835 S 4380 W – 4300 W – North	3	3	1	N	
5	4835 S 4380 W – 4300 W – South	0	2	0	N	
6	4380 W 4745 S – 4835 S - West	3	6	5	N	
7	4380 W 4745 S – 4835 S – East	0	2	4	N	
8	4340 W 4745 S – 4835 S - West	2	0	1	N	
9	4340 W 4745 S – 4835 S – East	2	5	1	Y	
10	4300 W 4715 S – 4835 S – West	2	3	0	N	
11	4300 W 4715 S – 4835 S - East	1	0	0	N	

Section 4						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4620 W 5865 S – 5015 S – West	0	0	0	N	
2	4620 W 5865 S – 5015 S – East	1	0	3	N	
3	4895 S 4620 W – 4460 W – North	0	2	2	N	
4	4895 S 4620 W – 4460 W – South	2	0	3	N	
5	4925 S 4620 W – 4460 W – North	8	3	1	Y	
6	4925 S 4620 W – 4460 W – South	4	0	0	N	
7	4955 S 4620 W – 4460 W - North	4	8	1	Y	
8	4955 S 4620 W – 4460 W – South	2	6	0	Y	
9	4985 S 4620 W – 4460 W - North	6	6	3	N	
10	4985 S 4620 W – 4460 W – South	3	6	1	N	
11	5015 S 4620 W – 4460 W - North	5	1	6	N	
12	5015 S 4620 W – 4460 W – South	3	4	1	N	
13	4460 W 4865 S – 5015 S - West	2	0	5	N	
14	4460 W 4865 S – 5015 S - East	5	10	0	N	
Section 5						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4955 S 4460 W – 4420 W - North	0	0	0	N	
2	4955 S 4460 W – 4420 W – South	2	0	0	N	
3	4420 W 4865 S – 5015 S - West	0	1	2	N	
4	4420 W 4865 S – 5015 S – East	2	0	11	N	
5	4380 W 4865 S – 4985 S - West	6	7	6	N	
6	4380 W 4865 S – 4985 S – East	1	3	4	N	
7	4340 W 4865 S – 4985 S - West	4	5	0	N	
8	4340 W 4865 S – 4985 S – East	4	6	6	N	
9	4300 W 4865 S – 5015 S - West	4	2	6	N	
10	4300 W 4865 S – 5015 S – East	0	0	0	N	
11	4985 S 4420 W – 4300 W - North	2	3	0	N	
12	4985 S 4420 W – 4300 W – South	0	4	1	N	
13	5015 S 4460 W – 4300 W - North	5	2	1	N	
14	5015 S 4460 W – 4300 W - South	4	2	1	N	

Section 6						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5055 S 4620 W – 4460 W - North	9	2	3	N	
2	5055 S 4620 W – 4460 W – South	9	4	0	N	
3	5100 S 4620 W – 4460 W - North	3	4	2	N	
4	5100 S 4620 W – 4460 W – South	6	5	2	N	
5	5135 S 4620 W – 4420 W - North	6	9	1	N	
6	5135 S 4620 W – 4420 W – South	2	3	3	N	
7	5175 S 4620 W – 4420 W - North	2	1	2	N	
8	5175 S 4620 W – 4420 W – South	6	0	5	Y	
9	5215 S 4620 W – 4420 W - North	0	0	1	N	
10	5215 S 4620 W – 4420 W – South	0	4	1	N	
11	4520 W 5135 S – 5215 S - West	1	0	0	Y	
12	4520 W 5135 S – 5215 S – East	2	1	0	Y	
13	4460 W 5015 S – 5135 S - West	0	1	1	N	
14	4460 W 5015 S – 5135 S – East	1	1	2	N	
15	4420 W 5015 S – 5215 S - West	0	0	3	N	
16	4420 W 5015 S – 5215 S - East	0	1	2	N	
Section 7						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5055 S 4420 W – Alex St - North	0	8	4	N	
2	5055 S 4420 W – Alex St – South	2	9	1	N	
3	5100 S 4420 W – Alex St - North	1	3	1	N	
4	5100 S 4420 W – Alex St – South	1	0	0	N	
5	5135 S 4420 – Alex St - North	0	2	0	N	
6	5135 S 4420 – Alex St – South	0	3	2	N	
7	5175 S 4420 W – Alex St - North	0	3	1	N	
8	5175 S 4420 W – Alex St - South	1	10	0	N	
9	5215 S 4420 W – 4245 W - North	2	1	3	N	
10	5215 S 4420 W – 4245 W – South	0	0	1	N	

Section 8						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5255 S 4620 W – 4520 W - North	3	2	0	N	
2	5255 S 4620 W – 4520 W – South	3	2	0	N	
3	5295 S 4620 W – 4420 W - North	1	3	0	N	
4	5295 S 4620 W – 4420 W – South	8	2	3	Y	
5	5335 S 4620 W – 4460 W - North	2	4	2	N	
6	5335 S 4620 W – 4460 W – South	4	4	2	N	
7	5375 S 4620 W – 4460 W - North	0	9	0	N	
8	5375 S 4620 W – 4460 W – South	3	5	0	N	
9	4520 W 5215 S – 4295 S - West	0	2	2	N	
10	4520 W 5215 S – 4295 S – East	0	0	0	N	
11	4460 W 5295 S – 5415 S - West	1	2	1	N	
12	4460 W 5295 S – 5415 S – East	0	2	2	N	
13	4420 W 5215 S – 5415 S - West	0	0	3	N	
14	4420 W 5215 S – 5415 S – East	1	0	2	N	
15	5415 S 4620 W – 4420 W – North	0	0	5	N	
16	5415 S 4620 W – 4420 W – South	0	0	0	N	
Section 9						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5255 S 4420 W – 4320 W - North	1	2	2	N	
2	5255 S 4420 W – 4320 W – South	4	0	2	N	
3	5295 S 4420 W – 4320 W - North	0	8	2	N	
4	5295 S 4420 W – 4320 W – South	3	1	0	N	
5	5335 S 4420 W – 4320 W - North	1	0	1	N	
6	5335 S 4420 W – 4320 W – South	0	3	3	N	
7	5375 S 4420 W – 4280 W - North	0	0	1	N	
8	5375 S 4420 W – 4280 W – South	0	0	0	N	
9	4320 W 5215 S – 5415 S - West	0	0	3	N	
10	4320 W 5215 S – 5415 S – East	1	1	3	N	
11	4280 W 5215 S – 5375 S - West	0	5	6	N	
12	4280 W 5215 S – 5375 S – East	0	0	0	N	
13	5415 S 4420 W – 4220 W - North	0	0	0	N	
14	5415 S 4420 W – 4220 W - South	0	0	0	N	

Collection Time: 12:00 PM - 2:00 PM

Section 1						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4720 W 4715 S – 4865 S - West	0	2	5	N	
2	4720 W 4715 S – 4865 S – East	1	4	4	N	
3	4715 S 4720 W – 4620 W - South	2	2	0	N	
4	4620 W 4715 S – 4865 S - West	3	0	14	N	
5	4620 W 4715 S – 4865 S - West	0	3	4	N	
6	4865 S 4620 W – 4520 W - North	4	0	0	N	
7	4865 S 4620 W – 4520 W - North	1	0	2	N	
Section 2						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4715 S 4520 W – 4420 W – South	0	0	3	N	
2	4420 W 4715 S – 4866 S - West	1	0	6	N	
3	4420 W 4715 S – 4866 S – East	1	2	4	N	
4	4865 S 4520 W – 4420 S - North	6	0	3	N	
5	4865 S 4520 W – 4420 S - South	2	2	1	N	
Section 3						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4715 S 4420 W – 4300 W – South	0	0	4	N	
2	4745 S 4420 W – 4300 W – North	0	3	3	N	
3	4745 S 4420 W – 4300 W – South	0	4	0	N	
4	4835 S 4380 W – 4300 W – North	0	5	1	N	
5	4835 S 4380 W – 4300 W – South	0	2	0	N	
6	4380 W 4745 S – 4835 S - West	1	8	3	N	
7	4380 W 4745 S – 4835 S – East	0	4	4	N	
8	4340 W 4745 S – 4835 S - West	0	1	1	N	
9	4340 W 4745 S – 4835 S – East	0	6	2	Y	
10	4300 W 4715 S – 4835 S – West	1	2	1	N	
11	4300 W 4715 S – 4835 S - East	0	0	0	N	

Section 4						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4620 W 5865 S – 5015 S – West	0	0	0	N	
2	4620 W 5865 S – 5015 S – East	1	1	3	N	
3	4895 S 4620 W – 4460 W – North	0	3	1	N	
4	4895 S 4620 W – 4460 W – South	1	0	2	N	
5	4925 S 4620 W – 4460 W – North	2	8	4	N	
6	4925 S 4620 W – 4460 W – South	2	1	4	N	
7	4955 S 4620 W – 4460 W - North	5	4	1	N	
8	4955 S 4620 W – 4460 W – South	2	7	4	N	
9	4985 S 4620 W – 4460 W - North	5	3	3	N	
10	4985 S 4620 W – 4460 W – South	6	0	4	N	
11	5015 S 4620 W – 4460 W - North	7	2	1	N	Many cars parked on curb bur not on
12	5015 S 4620 W – 4460 W – South	3	4	2	N	
13	4460 W 4865 S – 5015 S - West	2	1	4	N	
14	4460 W 4865 S – 5015 S - East	3	11	1	N	
Section 5						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4955 S 4460 W – 4420 W - North	0	0	0	N	
2	4955 S 4460 W – 4420 W – South	2	0	0	N	
3	4420 W 4865 S – 5015 S - West	1	1	3	N	
4	4420 W 4865 S – 5015 S – East	1	1	14	N	
5	4380 W 4865 S – 4985 S - West	3	10	5	Y	
6	4380 W 4865 S – 4985 S – East	1	1	4	N	
7	4340 W 4865 S – 4985 S - West	8	5	0	N	
8	4340 W 4865 S – 4985 S – East	3	6	8	N	
9	4300 W 4865 S – 5015 S - West	4	4	8	N	
10	4300 W 4865 S – 5015 S – East	0	0	0	N	
11	4985 S 4420 W – 4300 W - North	3	3	0	N	
12	4985 S 4420 W – 4300 W – South	0	2	0	N	
13	5015 S 4460 W – 4300 W - North	3	7	1	N	
14	5015 S 4460 W – 4300 W - South	5	1	0	N	

Section 6						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5055 S 4620 W – 4460 W - North	6	2	1	N	
2	5055 S 4620 W – 4460 W – South	6	5	3	N	
3	5100 S 4620 W – 4460 W - North	1	7	2	N	
4	5100 S 4620 W – 4460 W – South	3	9	2	N	
5	5135 S 4620 W – 4420 W - North	3	11	3	N	
6	5135 S 4620 W – 4420 W – South	1	5	5	Y	
7	5175 S 4620 W – 4420 W - North	1	1	1	N	
8	5175 S 4620 W – 4420 W – South	5	0	5	Y	
9	5215 S 4620 W – 4420 W - North	0	0	3	N	
10	5215 S 4620 W – 4420 W – South	2	0	0	N	
11	4520 W 5135 S – 5215 S - West	0	0	0	Y	
12	4520 W 5135 S – 5215 S – East	4	0	4	N	
13	4460 W 5015 S – 5135 S - West	1	1	2	N	
14	4460 W 5015 S – 5135 S – East	1	1	3	N	
15	4420 W 5015 S – 5215 S - West	0	0	3	N	
16	4420 W 5015 S – 5215 S - East	0	1	1	N	
Section 7						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5055 S 4420 W – Alex St - North	0	9	2	N	
2	5055 S 4420 W – Alex St – South	0	12	1	N	
3	5100 S 4420 W – Alex St - North	2	2	3	N	
4	5100 S 4420 W – Alex St – South	2	0	1	N	
5	5135 S 4420 – Alex St - North	0	2	2	N	
6	5135 S 4420 – Alex St – South	0	3	1	N	
7	5175 S 4420 W – Alex St - North	0	3	4	N	
8	5175 S 4420 W – Alex St - South	5	5	0	N	
9	5215 S 4420 W – 4245 W - North	2	0	3	N	
10	5215 S 4420 W – 4245 W – South	0	0	1	N	

Section 8						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5255 S 4620 W – 4520 W - North	4	1	0	N	
2	5255 S 4620 W – 4520 W – South	3	1	0	N	
3	5295 S 4620 W – 4420 W - North	0	5	2	N	
4	5295 S 4620 W – 4420 W – South	7	8	1	Y	
5	5335 S 4620 W – 4460 W - North	0	7	3	N	
6	5335 S 4620 W – 4460 W – South	8	3	2	N	
7	5375 S 4620 W – 4460 W - North	2	10	1	N	
8	5375 S 4620 W – 4460 W – South	7	0	1	N	
9	4520 W 5215 S – 4295 S - West	1	0	2	N	
10	4520 W 5215 S – 4295 S – East	1	0	0	N	
11	4460 W 5295 S – 5415 S - West	2	2	0	N	
12	4460 W 5295 S – 5415 S – East	0	1	1	N	
13	4420 W 5215 S – 5415 S - West	1	0	2	N	
14	4420 W 5215 S – 5415 S – East	0	0	9	N	
15	5415 S 4620 W – 4420 W – North	0	0	5	N	
16	5415 S 4620 W – 4420 W – South	0	0	0	N	
Section 9						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5255 S 4420 W – 4320 W - North	1	2	2	N	
2	5255 S 4420 W – 4320 W – South	3	0	5	N	All 5 yard vehicles in one yard
3	5295 S 4420 W – 4320 W - North	1	6	2	N	
4	5295 S 4420 W – 4320 W – South	4	3	0	N	
5	5335 S 4420 W – 4320 W - North	0	2	4	N	
6	5335 S 4420 W – 4320 W – South	0	0	1	N	
7	5375 S 4420 W – 4280 W - North	0	1	1	N	
8	5375 S 4420 W – 4280 W – South	0	0	0	N	
9	4320 W 5215 S – 5415 S - West	0	1	6	N	
10	4320 W 5215 S – 5415 S – East	3	1	6	N	
11	4280 W 5215 S – 5375 S - West	0	6	6	N	
12	4280 W 5215 S – 5375 S – East	0	0	0	N	
13	5415 S 4420 W – 4220 W - North	0	0	0	N	
14	5415 S 4420 W – 4220 W - South	0	0	0	N	

Collection Time: 5:00 PM - 7:00 PM

Section 1						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4720 W 4715 S – 4865 S - West	2	3	6	N	
2	4720 W 4715 S – 4865 S – East	5	1	5	N	
3	4715 S 4720 W – 4620 W - South	3	0	2	N	
4	4620 W 4715 S – 4865 S - West	2	3	3	N	
5	4620 W 4715 S – 4865 S - West	0	3	3	N	
6	4865 S 4620 W – 4520 W - North	2	0	0	N	
7	4865 S 4620 W – 4520 W - North	1	0	3	N	
Section 2						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4715 S 4520 W – 4420 W – South	0	0	2	N	
2	4420 W 4715 S – 4866 S - West	1	2	1	N	
3	4420 W 4715 S – 4866 S – East	2	2	4	N	
4	4865 S 4520 W – 4420 S - North	5	1	3	N	
5	4865 S 4520 W – 4420 S - South	3	1	3	N	
Section 3						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4715 S 4420 W – 4300 W – South	2	2	7	N	
2	4745 S 4420 W – 4300 W – North	0	4	1	N	
3	4745 S 4420 W – 4300 W – South	2	4	2	Y	
4	4835 S 4380 W – 4300 W – North	3	1	3	Y	
5	4835 S 4380 W – 4300 W – South	0	2	0	N	
6	4380 W 4745 S – 4835 S - West	5	7	5	N	
7	4380 W 4745 S – 4835 S – East	3	6	5	N	
8	4340 W 4745 S – 4835 S - West	3	1	3	N	
9	4340 W 4745 S – 4835 S – East	0	6	0	N	
10	4300 W 4715 S – 4835 S – West	2	4	1	N	
11	4300 W 4715 S – 4835 S - East	0	0	0	N	

Section 4						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4620 W 5865 S – 5015 S – West	0	0	0	N	
2	4620 W 5865 S – 5015 S – East	0	3	6	N	
3	4895 S 4620 W – 4460 W – North	0	2	3	N	
4	4895 S 4620 W – 4460 W – South	3	0	3	N	
5	4925 S 4620 W – 4460 W – North	3	5	1	Y	
6	4925 S 4620 W – 4460 W – South	6	2	4	Y	
7	4955 S 4620 W – 4460 W - North	5	14	5	N	
8	4955 S 4620 W – 4460 W – South	5	7	1	N	
9	4985 S 4620 W – 4460 W - North	5	8	2	N	
10	4985 S 4620 W – 4460 W – South	8	5	4	N	
11	5015 S 4620 W – 4460 W - North	3	4	4	N	
12	5015 S 4620 W – 4460 W – South	1	7	4	N	
13	4460 W 4865 S – 5015 S - West	3	2	6	N	
14	4460 W 4865 S – 5015 S - East	5	6	0	N	
Section 5						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4955 S 4460 W – 4420 W - North	1	0	1	Y	
2	4955 S 4460 W – 4420 W – South	1	5	0	Y	
3	4420 W 4865 S – 5015 S - West	1	0	2	N	
4	4420 W 4865 S – 5015 S – East	4	0	7	N	
5	4380 W 4865 S – 4985 S - West	3	11	6	N	
6	4380 W 4865 S – 4985 S – East	1	3	4	N	
7	4340 W 4865 S – 4985 S - West	0	9	3	Y	
8	4340 W 4865 S – 4985 S – East	1	4	6	N	
9	4300 W 4865 S – 5015 S - West	6	3	8	N	
10	4300 W 4865 S – 5015 S – East	0	0	0	N	
11	4985 S 4420 W – 4300 W - North	4	4	0	Y	
12	4985 S 4420 W – 4300 W – South	5	2	0	N	
13	5015 S 4460 W – 4300 W - North	4	7	1	N	
14	5015 S 4460 W – 4300 W - South	4	1	1	N	

Section 6						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5055 S 4620 W – 4460 W - North	8	9	1	N	
2	5055 S 4620 W – 4460 W – South	7	16	4	N	
3	5100 S 4620 W – 4460 W - North	3	3	3	N	
4	5100 S 4620 W – 4460 W – South	1	9	4	N	
5	5135 S 4620 W – 4420 W - North	9	5	5	N	
6	5135 S 4620 W – 4420 W – South	4	5	6	N	
7	5175 S 4620 W – 4420 W - North	0	4	2	N	
8	5175 S 4620 W – 4420 W – South	3	2	7	N	
9	5215 S 4620 W – 4420 W - North	3	3	4	N	
10	5215 S 4620 W – 4420 W – South	5	2	3	N	
11	4520 W 5135 S – 5215 S - West	0	0	0	N	
12	4520 W 5135 S – 5215 S – East	2	1	2	N	
13	4460 W 5015 S – 5135 S - West	0	2	1	N	
14	4460 W 5015 S – 5135 S – East	1	1	2	N	
15	4420 W 5015 S – 5215 S - West	0	0	3	N	
16	4420 W 5015 S – 5215 S - East	0	1	2	N	
Section 7						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5055 S 4420 W – Alex St - North	0	11	1	N	
2	5055 S 4420 W – Alex St – South	0	8	3	N	
3	5100 S 4420 W – Alex St - North	1	3	3	N	
4	5100 S 4420 W – Alex St – South	2	1	0	N	
5	5135 S 4420 – Alex St - North	1	1	2	N	
6	5135 S 4420 – Alex St – South	0	3	1	N	
7	5175 S 4420 W – Alex St - North	1	6	4	N	
8	5175 S 4420 W – Alex St - South	0	6	2	N	
9	5215 S 4420 W – 4245 W - North	2	2	4	N	
10	5215 S 4420 W – 4245 W – South	0	1	2	N	

Section 8						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5255 S 4620 W – 4520 W - North	1	1	0	N	
2	5255 S 4620 W – 4520 W – South	5	3	0	N	
3	5295 S 4620 W – 4420 W - North	4	4	2	Y	
4	5295 S 4620 W – 4420 W – South	7	6	2	N	
5	5335 S 4620 W – 4460 W - North	2	4	5	N	
6	5335 S 4620 W – 4460 W – South	6	6	3	N	
7	5375 S 4620 W – 4460 W - North	0	9	3	N	
8	5375 S 4620 W – 4460 W – South	2	4	0	N	
9	4520 W 5215 S – 4295 S - West	1	1	2	N	
10	4520 W 5215 S – 4295 S – East	0	0	0	N	
11	4460 W 5295 S – 5415 S - West	1	4	2	Y	
12	4460 W 5295 S – 5415 S – East	0	2	0	N	
13	4420 W 5215 S – 5415 S - West	2	0	4	N	
14	4420 W 5215 S – 5415 S – East	0	1	10	N	
15	5415 S 4620 W – 4420 W – North	0	0	5	N	
16	5415 S 4620 W – 4420 W – South	0	0	0	N	
Section 9						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5255 S 4420 W – 4320 W - North	0	0	2	N	
2	5255 S 4420 W – 4320 W – South	4	0	3	N	
3	5295 S 4420 W – 4320 W - North	0	6	2	N	
4	5295 S 4420 W – 4320 W – South	4	1	0	N	
5	5335 S 4420 W – 4320 W - North	0	1	1	N	
6	5335 S 4420 W – 4320 W – South	0	2	4	N	
7	5375 S 4420 W – 4280 W - North	0	1	2	N	
8	5375 S 4420 W – 4280 W – South	3	1	0	N	
9	4320 W 5215 S – 5415 S - West	0	3	3	N	
10	4320 W 5215 S – 5415 S – East	0	5	5	N	
11	4280 W 5215 S – 5375 S - West	0	7	7	N	
12	4280 W 5215 S – 5375 S – East	0	0	0	N	
13	5415 S 4420 W – 4220 W - North	0	0	0	N	
14	5415 S 4420 W – 4220 W - South	0	0	0	N	

Collection Time: 9:00 PM - 11:00 PM

Section 1						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4720 W 4715 S – 4865 S - West	0	6	4	N	
2	4720 W 4715 S – 4865 S – East	0	7	4	N	
3	4715 S 4720 W – 4620 W - South	3	0	3	N	
4	4620 W 4715 S – 4865 S - West	1	2	5	N	
5	4620 W 4715 S – 4865 S - West	1	2	3	N	
6	4865 S 4620 W – 4520 W - North	4	0	0	N	
7	4865 S 4620 W – 4520 W - North	1	0	2	N	
Section 2						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4715 S 4520 W – 4420 W – South	0	0	2	N	
2	4420 W 4715 S – 4866 S - West	0	0	4	N	
3	4420 W 4715 S – 4866 S – East	3	2	0	N	
4	4865 S 4520 W – 4420 S - North	0	4	2	N	
5	4865 S 4520 W – 4420 S - South	4	1	1	N	
Section 3						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4715 S 4420 W – 4300 W – South	1	5	2	N	
2	4745 S 4420 W – 4300 W – North	2	1	2	N	
3	4745 S 4420 W – 4300 W – South	3	2	0	Y	
4	4835 S 4380 W – 4300 W – North	1	3	2	Y	
5	4835 S 4380 W – 4300 W – South	0	2	0	N	
6	4380 W 4745 S – 4835 S - West	2	10	4	N	
7	4380 W 4745 S – 4835 S – East	0	4	3	N	
8	4340 W 4745 S – 4835 S - West	0	3	1	N	
9	4340 W 4745 S – 4835 S – East	0	6	2	N	
10	4300 W 4715 S – 4835 S – West	3	4	1	N	
11	4300 W 4715 S – 4835 S - East	1	0	0	N	

Section 4* Count Missed						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4620 W 5865 S – 5015 S – West					
2	4620 W 5865 S – 5015 S – East					
3	4895 S 4620 W – 4460 W – North					
4	4895 S 4620 W – 4460 W – South					
5	4925 S 4620 W – 4460 W – North					
6	4925 S 4620 W – 4460 W – South					
7	4955 S 4620 W – 4460 W - North					
8	4955 S 4620 W – 4460 W – South					
9	4985 S 4620 W – 4460 W - North					
10	4985 S 4620 W – 4460 W – South					
11	5015 S 4620 W – 4460 W - North					
12	5015 S 4620 W – 4460 W – South					
13	4460 W 4865 S – 5015 S - West					
14	4460 W 4865 S – 5015 S - East					
Section 5						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	4955 S 4460 W – 4420 W - North	0	0	0	Y	
2	4955 S 4460 W – 4420 W – South	3	1	0	Y	
3	4420 W 4865 S – 5015 S - West	2	1	1	N	
4	4420 W 4865 S – 5015 S – East	6	2	7	N	
5	4380 W 4865 S – 4985 S - West	3	13	3	N	
6	4380 W 4865 S – 4985 S – East	0	4	7	N	
7	4340 W 4865 S – 4985 S - West	3	8	2	Y	
8	4340 W 4865 S – 4985 S – East	3	4	7	Y	
9	4300 W 4865 S – 5015 S - West	5	3	7	Y	
10	4300 W 4865 S – 5015 S – East	0	0	0	N	
11	4985 S 4420 W – 4300 W - North	5	1	3	N	
12	4985 S 4420 W – 4300 W – South	5	0	3	N	
13	5015 S 4460 W – 4300 W - North	4	4	1	N	
14	5015 S 4460 W – 4300 W - South	2	4	0	N	

Section 6						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5055 S 4620 W – 4460 W - North	14	5	1	N	
2	5055 S 4620 W – 4460 W – South	17	2	2	N	
3	5100 S 4620 W – 4460 W - North	4	5	3	N	
4	5100 S 4620 W – 4460 W – South	6	6	2	N	
5	5135 S 4620 W – 4420 W - North	3	10	3	N	
6	5135 S 4620 W – 4420 W – South	1	7	2	N	
7	5175 S 4620 W – 4420 W - North	1	3	3	N	
8	5175 S 4620 W – 4420 W – South	2	4	7	N	
9	5215 S 4620 W – 4420 W - North	4	1	5	N	
10	5215 S 4620 W – 4420 W – South	10	0	1	N	
11	4520 W 5135 S – 5215 S - West	0	0	0	N	
12	4520 W 5135 S – 5215 S – East	3	0	1	N	
13	4460 W 5015 S – 5135 S - West	2	2	2	N	
14	4460 W 5015 S – 5135 S – East	2	0	2	N	
15	4420 W 5015 S – 5215 S - West	0	0	2	Y	
16	4420 W 5015 S – 5215 S - East	2	0	1	N	
Section 7						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5055 S 4420 W – Alex St - North	1	9	2	N	
2	5055 S 4420 W – Alex St – South	3	9	1	N	
3	5100 S 4420 W – Alex St - North	0	3	3	N	
4	5100 S 4420 W – Alex St – South	0	3	2	N	
5	5135 S 4420 – Alex St - North	1	4	1	N	
6	5135 S 4420 – Alex St – South	0	5	2	N	
7	5175 S 4420 W – Alex St - North	1	7	6	N	
8	5175 S 4420 W – Alex St - South	0	9	2	N	
9	5215 S 4420 W – 4245 W - North	3	1	2	N	
10	5215 S 4420 W – 4245 W – South	0	0	1	N	

Section 8						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5255 S 4620 W – 4520 W - North	1	2	0	N	
2	5255 S 4620 W – 4520 W – South	4	3	0	N	
3	5295 S 4620 W – 4420 W - North	3	7	2	N	
4	5295 S 4620 W – 4420 W – South	6	8	2	N	
5	5335 S 4620 W – 4460 W - North	5	3	3	N	
6	5335 S 4620 W – 4460 W – South	7	4	2	N	
7	5375 S 4620 W – 4460 W - North	2	3	2	N	
8	5375 S 4620 W – 4460 W – South	0	6	3	N	
9	4520 W 5215 S – 4295 S - West	1	1	3	N	
10	4520 W 5215 S – 4295 S – East	0	0	0	N	
11	4460 W 5295 S – 5415 S - West	2	0	3	N	
12	4460 W 5295 S – 5415 S – East	2	2	1	N	
13	4420 W 5215 S – 5415 S - West	2	0	3	N	
14	4420 W 5215 S – 5415 S – East	0	0	13	N	
15	5415 S 4620 W – 4420 W – North	0	0	7	N	
16	5415 S 4620 W – 4420 W – South	0	0	0	N	
Section 9						
#	Location	# of Parked Veh	# of Veh Parked on SW	# of Veh Parked in Yard	Veh Parked on Corner (Y/N)	Conflict Notes (parked vehicles and other street users)
1	5255 S 4420 W – 4320 W - North	1	0	2	N	
2	5255 S 4420 W – 4320 W – South	2	3	4	N	
3	5295 S 4420 W – 4320 W - North	1	5	3	N	
4	5295 S 4420 W – 4320 W – South	1	4	0	N	
5	5335 S 4420 W – 4320 W - North	1	0	3	N	
6	5335 S 4420 W – 4320 W – South	0	2	2	N	
7	5375 S 4420 W – 4280 W - North	2	0	0	N	
8	5375 S 4420 W – 4280 W – South	0	0	0	N	
9	4320 W 5215 S – 5415 S - West	0	0	5	N	
10	4320 W 5215 S – 5415 S – East	0	2	6	N	
11	4280 W 5215 S – 5375 S - West	0	8	7	N	
12	4280 W 5215 S – 5375 S – East	0	0	0	N	
13	5415 S 4420 W – 4220 W - North	0	0	1	N	
14	5415 S 4420 W – 4220 W - South	0	0	0	N	



APPENDIX B

Technical Memo #2: Community Engagement



DRAFT

Kearns On-Street Parking Study

Memorandum 2: Community Engagement

August 2022

Prepared for:



*Wasatch Front Regional Council
41 North Rio Grande Street
Salt Lake City, Utah 84101*



G R E A T E R S A L T L A K E
**Municipal Services
District**

*Greater Salt Lake Municipal Services District
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August 2022
098347001



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Executive Summary

The purpose of Technical Memorandum #2 is to provide an overview of the public engagement that has been performed as part of the Kearns On-Street Parking Study. Included in the memorandum is a brief overview of the public engagement efforts and a summary of the survey responses.

The intent of the public outreach and survey was to allow the public an opportunity to react to the technical analysis, which assesses the current condition of on-street parking in the Kearns Metro Township based on data collected directly from the designated study areas. Social media content in the form of six infographics and a brief online survey invited the public to provide any additional comments they feel would be beneficial in the development of recommended changes to on-street parking. The Kearns On-Street Parking Study Survey opened for responses on June 20, 2022 and closed on July 18, 2022.

The online survey received 229 comments across the six survey questions from 108 respondents. The complete results of the survey can be viewed in **Appendix A**. In addition to the survey responses, an additional 114 comments were left on the Facebook posts advertising the survey from June 20, 2022 to July 11, 2022. Those comments can be found in **Appendix B**.

This survey was promoted on the Greater Salt Lake Municipal Services District's Facebook page, K-Town (Kearns Township Facebook Page), and MSD's July 2022 e-Newsletter with a link to the survey.

The following are a few key observations and takeaways identified from the Kearns On-Street Parking Study Survey:

- 62% of respondents do not park in their yards.
- 75% of respondents were in favor of eliminating on-street parking that impedes views near intersections.
- Respondents left 32 comments expressing interest in stronger enforcement of existing parking rules, 13 of which were requests for stronger enforcement of winter parking rules. This included better signage to advertise the parking rules.
- Respondents left 86 comments across the survey expressing safety concerns related to on-street parking and neighborhood intersections in the Kearns Metro Township.
- 60 % of respondents are in favor of eliminating on-street parking on one side of the roadway for narrow roads.
- Multiple comments expressed the need to limit the types of vehicles that can park on the street including RVs, large trucks with trailers, semi-trucks, and inoperable vehicles.



Introduction

The purpose of Technical Memorandum #2 is to provide an overview of the public engagement that has been performed as part of the Kearns On-Street Parking Study. The intent of the public outreach and survey was to communicate the results and allow the public an opportunity to react to the technical analysis, which assesses the current condition of on-street parking in the Kearns Metro Township based on data collected directly from the designated study areas. The study areas for the Kearns On-Street Parking Study consists of two residential neighborhoods as seen in **Figure 1**.

The Kearns On-Street Parking Study survey took place from June 20, 2022 to July 18, 2022. The effort consisted of one overview/introduction graphic along with five individual infographics to inform the community about the results of the Kearns On-Street Parking Study and encourage them to participate in the survey by providing a direct link. Included in this memorandum is an overview of the developed infographics, a summary of survey responses, and key takeaways to be utilized in the development of on-street parking recommendations for the Kearns Metro Township.



Figure 1 – Kearns On-Street Parking Study Areas

Infographic Overview

Infographics were developed as part of the public engagement plan to disseminate information to the community and encourage individuals to participate in the survey. The Greater Salt Lake Municipal Services District posted



Kearns On-Street Parking Study

Memorandum 2: Community Engagement

the series of infographics on their Facebook page on June 20, June 23, June 27, June 30, July 6, and July 11. These posts were shared on the K-Town (Kearns Township) Facebook page on June 20, June 27, June 30, July 6, and July 11. Five individual infographics were developed to convey information related to vehicles parking in yards, crashes involving parked vehicles, vehicles parking on sidewalks, intersection-related crashes, and on-street parking availability. These infographics can be seen in **Figure 2**. The introduction graphic, as seen in **Figure 3**, was also shared in the MSD e-Newsletter on July 6. Full details of the Facebook posts and comments can be found in **Appendix B**.

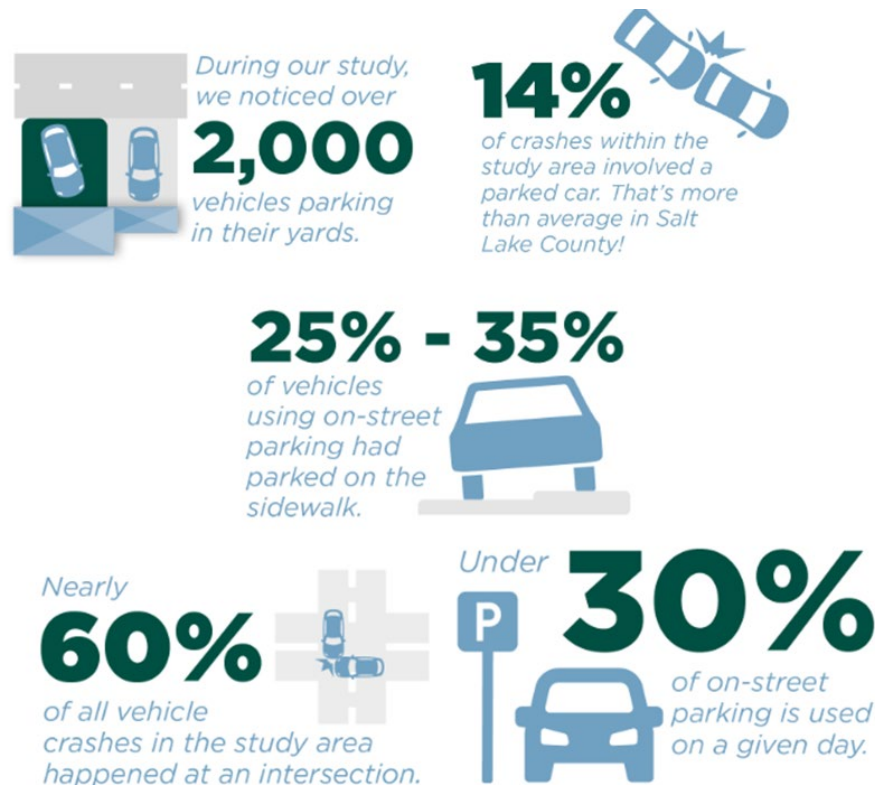


Figure 2 – Summary Infographics



Kearns On-Street Parking Study

Memorandum 2: Community Engagement

Kearns On-Street Parking Study

Kearns Metro Township and the Salt Lake Municipal Services District have conducted an On-Street Parking Study to evaluate parking availability, demand for parking, and parking habits.

Our study chose two areas in Kearns that represented the diverse homes and neighborhoods in Metro Township. We studied parking habits on one weekday and one weekend day in the morning, afternoon, evening, and nighttime. Our team used these findings to draft some potential recommended changes that we are aiming to implement by 2025.

Over the next two weeks, we will be sharing some key findings from the study with you. Follow along and let us know what you think of our results and recommendations by taking our survey. Our complete report will be available later this summer.

Tell us more about your experience parking in Kearns Metro Township here:

<https://tinyurl.com/KearnsParking>

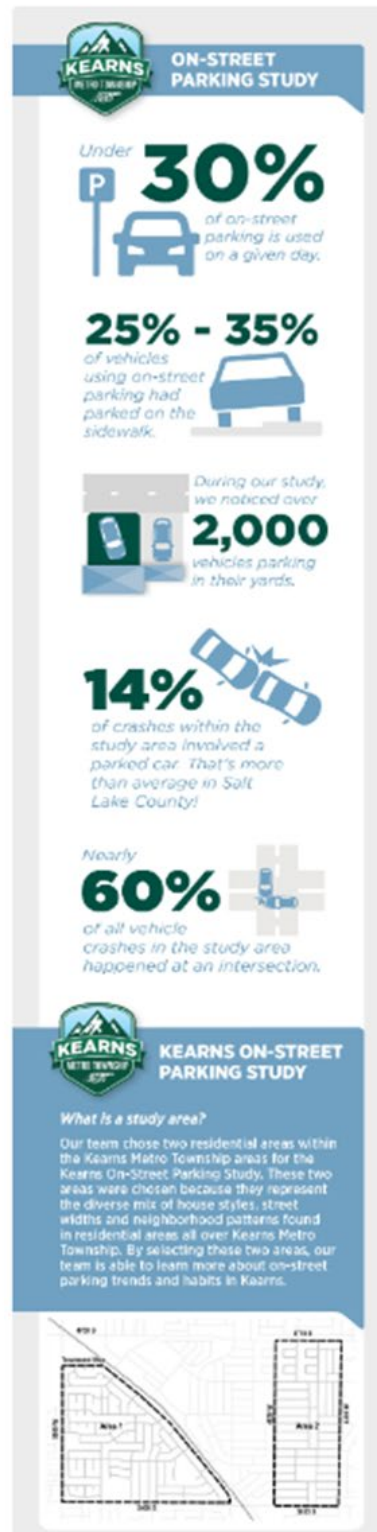


Figure 3 – MSD Newsletter Post (July 2022)



Survey

The Kearns On-Street Parking Study survey took place from June 20, 2022 to July 18, 2022. A total of six survey questions were developed based on the technical analysis found in Technical Memorandum #1. The following subsections introduce each of the survey questions, outline the results of the survey questions, and provide an overview of the respondents' comments.

Question #1

The survey asked residents their opinion why they park their vehicles in their yard with the following question. A summary of responses is provided in **Figure 4** and **Table 1**.

QUESTION #1: Our study found that many vehicles were parked in yards, despite there being street parking available. If you park your vehicle in your yard, what is the reason for your decision?

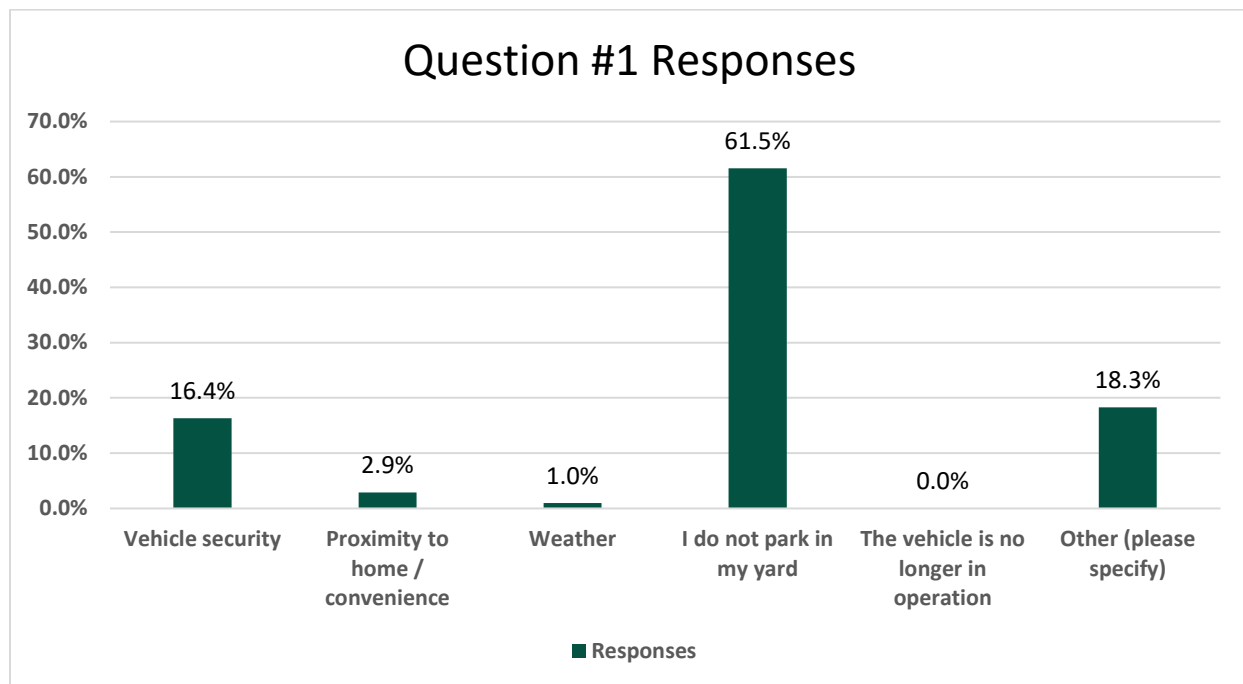


Figure 4 – Question #1 Responses

Table 1 – Question #1 Response Breakdown

Answer Choices	Percent	# of Responses
Vehicle security	16.4%	17
Proximity to home/convenience	2.9%	3
Weather	1.0%	1
I do not park in my yard	61.5%	64
The vehicle is no longer in operation	0.0%	0
Other (please specify)	18.3%	19
Total		104
Skipped		4



104 respondents participated in Question 1, and 19 left a comment related to Question #1. The following is a summary of the respondents that reported “Other” reasons for parking in their yards:

- One respondent mentioned poor street lighting.
- Two reported that the street they live on is too narrow.
- Two respondents reported they were trying to get concrete installed.
- Five respondents cited their neighbors as a reason for parking in their yard.
- Two respondents stated they got into the habit due to winter parking restrictions.
- Two respondents indicated they park in the yard to avoid getting hit by a passing car.
- One respondent explained they do not have enough room in the driveway for all the vehicles in the household.

Question #2

The survey asked residents their opinion on potential on-street parking changes with the following question. A summary of responses is provided in **Figure 5** and **Table 2**.

QUESTION #2: Our project team has drafted some potential recommended changes. Which recommendations would you like to see?

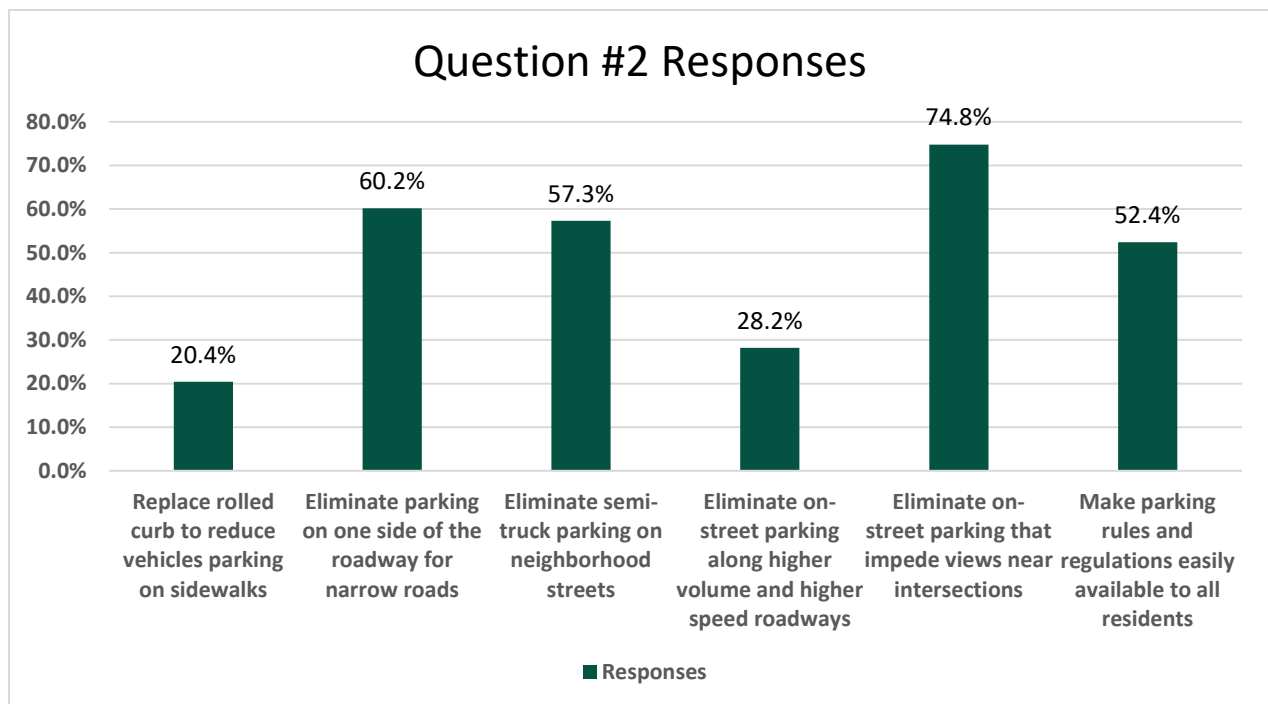


Figure 5 – Question #2 Responses



Table 2 – Question #2 Response Breakdown

Answer Choices	Percent	# of Responses
Replace rolled curb to reduce vehicles parking on sidewalks	20.4%	21
Eliminate parking on one side of the roadway for narrow roads	60.2%	62
Eliminate semi-truck parking on neighborhood streets	57.3%	59
Eliminate on-street parking along higher volume and higher speed roadways	28.2%	29
Eliminate on-street parking that impedes views near intersections	74.8%	77
Make parking rules and regulations easily available to all residents	52.4%	54
Total		103
Skipped		5

103 respondents participated in Question #2. The most popular proposed improvements were:

- Eliminate on-street parking that impedes views near intersections, which received 77 votes (74.8%).
- Eliminate parking on one side of the roadway for narrow roads, which received 62 votes (60.2%).
- Eliminate semi-truck parking on neighborhood streets, which received 59 votes (57.3%).
- Make parking rules and regulations easily available to all residents, which received 54 votes (52.4%).

Question #3

The survey asked residents for their input of what recommendations they have to improve on-street parking with the following question. **Figure 6** displays key comments associated with the survey responses for Question #3.

QUESTION #3: What recommendations do you have to improve on-street parking in Kearns Metro Township?

"More enforcement. Rules don't mean much if no one enforces them."

"No broken down cars, no trailers or boats."

"Install curbing between gutter and sidewalk.""

"Signs posted at regular intervals to remind community to remove cars from roadsides during snow storms."

"There should be a limit to how long a vehicle can park on the street. Additionally, on narrow roads, vehicles should only be allowed to park on one side of the street."

Figure 6 – Question #3 Key Comments



60 respondents left a response to Question #3. The following is a summary of those responses.

- Eleven respondents expressed that they wanted to see better enforcement of existing parking rules.
 - Two respondents expressed concerns about theft/burglary.
- Eight respondents said something about limiting the types of vehicles on the street—specifically concerned with RVs, large trucks, trailers, and cars that are inoperable.
- Four respondents wanted to limit the number of vehicles allowed per household.
- Four respondents want to see a limitation on the number of vehicles a household can park on the street.
- Seven respondents wanted to see parking restricted to one side of the street, and two respondents expressed safety concerns when parking is permitted on both sides of the road.
- Six respondents want stronger enforcement of winter parking restrictions, with one suggesting better signage to advertise the rules.
- Four respondents expressed interest in better signage to better promote existing parking rules.
- Four respondents expressed concern with parking near intersections and corners, with one suggesting road/curb striping to prevent cars from parking too close.
- Three respondents supported a change in regulation to allow cars to park in yards.
- One respondent expressed interest in one-on-one meetings with certain properties and neighborhood meetings to discuss ideas.

Question #4

The survey asked residents their opinion on additional items the project team should think about when considering on-street parking with the following question. **Figure 7** displays the key comments associated with the survey responses for Question #4.

QUESTION #4: What additional items do you think the project team needs to know about on-street parking in Kearns Metro Township?

"I think rules and regulations should take into consideration that some households have a lot of cars for a reason. It's expensive to park your car elsewhere, to move around your work car or work tools, and that some households have a lot of families."

It happens because our neighborhoods are old and only allow for one car in the driveway, two if you put them one in front of the other. We don't all have garages. The street is the only other option; people will not NOT park in the street."

"I think it would be good if people could be more aware that they should avoid parking in a way that obstructs the sidewalk. This is especially important for pedestrians with disabilities."

"Trailers parked too close to intersections and make it difficult to see oncoming traffic. Or they are parked too close to crosswalks making it a hazard to see pedestrians."

Figure 7 – Question #4 Key Comments



42 respondents left a comment on Question 4. The following is a summary of those comments.

- Eleven respondents commented that there are many homes with multiple generations or multiple families living there.
 - Two respondents were against restricting on-street parking for this reason.
 - One comment expressed feeling it would be unfair to restrict parking on both sides of the street.
 - One comment suggested requiring rental properties to supply adequate off-street parking.
 - Three comments suggested the lack of on-street parking was due to older properties not being built to accommodate multiple vehicles.
- Eleven respondents commented that on-street parking comes with safety concerns—such as narrow streets, visibility, lighting, and parking in unsafe locations (near intersections, speedbumps, and sidewalks).
 - Two comments suggested that people park on sidewalks to avoid getting hit by passing cars.
- Six comments suggested that vehicles parking on sidewalks is a big issue, and one respondent suggested painting the curbs to prevent a parked car from touching the yard.
- Four comments explained that enforcement is lacking in neighborhoods and three comments suggested winter parking rules should be enforced or better advertised.
- Three comments pointed to larger vehicles, such as RVs and trailers reducing visibility and taking up much of the available on-street parking.

Question #5

The survey asked residents their opinion on safety concerns at intersections with the following question. A summary of responses is provided in **Figure 8** along with the key comments associated with the survey responses in **Figure 9**.

QUESTION #5: Do you feel there are safety concerns at intersections in your neighborhood

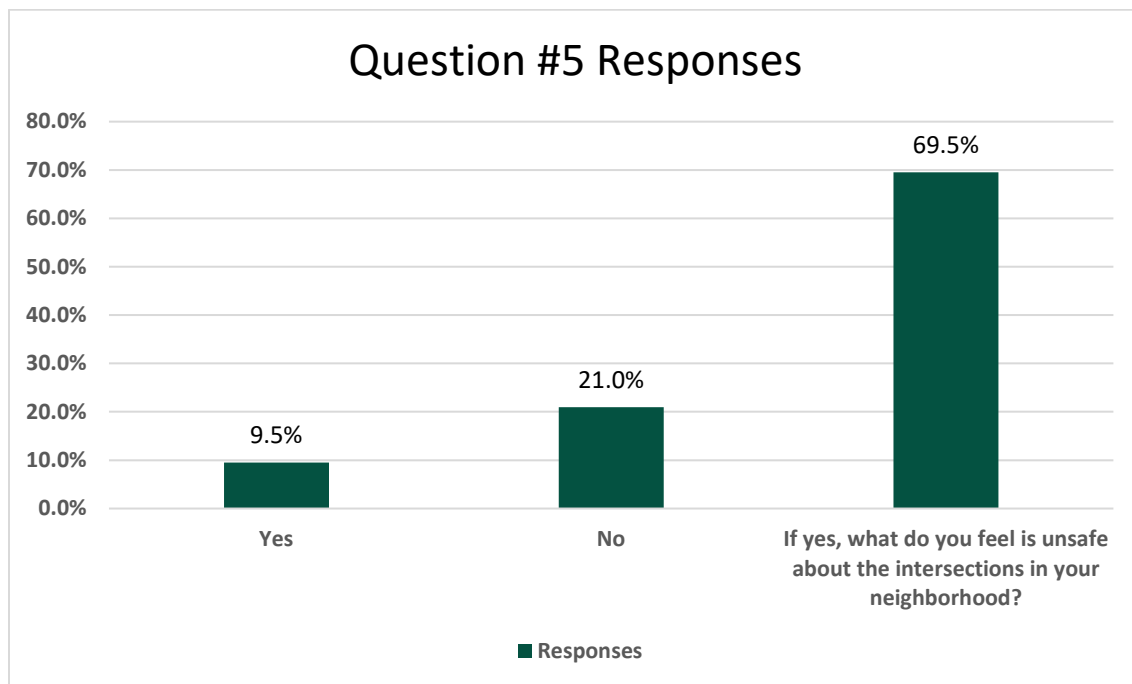


Figure 8 – Question #5 Responses

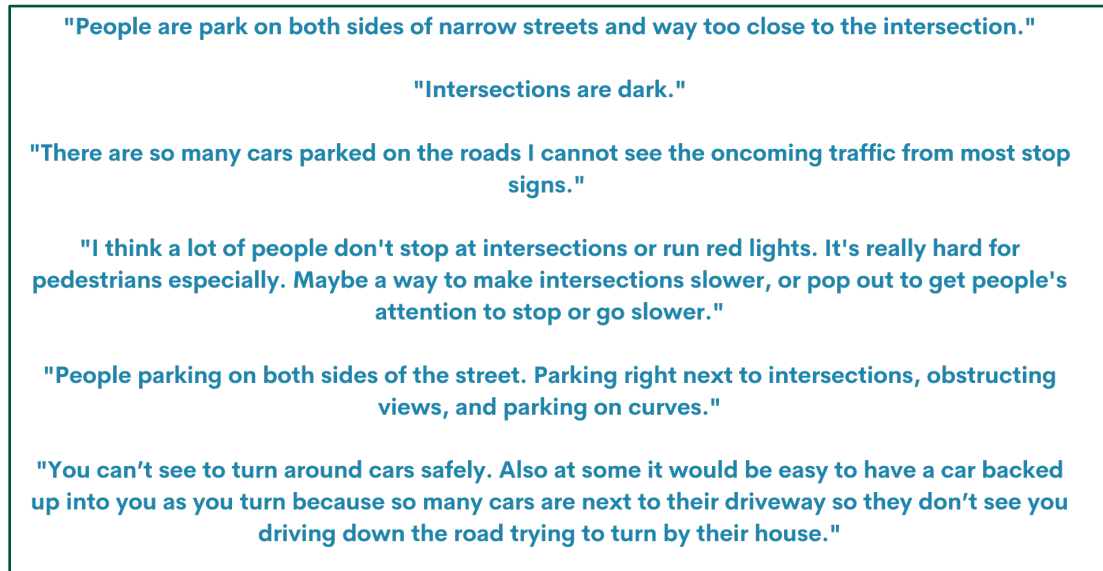


Figure 9 – Question #5 Key Comments

79% of the respondents had some type of safety concern at neighborhood intersections. 73 respondents left a comment on Question #5 related to what they felt was unsafe about intersections in their neighborhoods. The following is a summary of those safety concern comments.

- 35 comments suggested that cars parked on-street pose a safety concern.
- 26 comments explicitly state that cars parked too close to the intersection reduce visibility.
- 22 comments mentioned visibility as a concern.
 - Four comments mentioned overgrown trees and bushes as the cause of visibility issues.
- 11 comments expressed concern with signage in the area.
 - Ten of those comments identified corners and intersections with no stop signs as being a major concern.
 - One comment stated that the intersection of 6200 South and Impressions Drive needs a traffic signal.
- Eight comments expressed concerns with vehicles speeding on neighborhood streets.
- Seven comments identified cars running stop signs and lighting as a safety concern.
 - One comment pointed to 4420 West as an area of concern.
- Six comments pointed to the built environment as a source of concern. Specific comments include:
 - Curvature of roads make 2-way stops dangerous.
 - The cement blocks at 4620 South and 5215 West are scuffed up from people hitting them and could pose a hazard in the winter.
 - Narrow streets can't accommodate larger vehicles like trucks.
 - Concerns with visibility issues at the intersections of:
 - 4015 West and 5780 South
 - 4300 West 4700 South
 - Trowbridge Way and 6200 South



Question #6

The survey asked residents if they had any additional comments about on-street parking with the following question. **Figure 10** displays the key comments associated with the survey responses for Question #6.

QUESTION #6: Do you have any other comments about on-street parking in Kearns Metro Township?

"I believe we have too many cars parked on the roads. I feel it's dangerous with blocked view of kids and animals."

"Concert traffic and parking is a problem in some neighborhoods."

"There's a lot of areas in Kearns where the neighborhood is older and the streets are really narrow. I think it would be super beneficial if you were only allowed to park on one side."

"More parking is better... unless it makes the road dangerous."

"Most of the cars parked in yards don't run and aren't licensed. We should encourage people to clean up their junk so they can pull off the roads to increase visibility, thus protecting our kids, not turning every road into a single lane, and making our neighborhoods look cleaner and more open."

Figure 10 – Question #6 Key Comments

35 respondents left a comment on Question #6. The following is a summary of those comments

- Seven respondents suggested that there were too many cars parking on the street in their neighborhood.
 - Of these respondents, two expressed their distaste for seeing neighbors parking vehicles in their yards.
- Four comments expressed interest in stronger enforcement of winter parking rules, and four expressed interest in stronger enforcement of other existing parking rules.
- One commenter stated, "people are going to park where they park."
- Five comments stated they have noticed inoperable or unlicensed vehicles using on-street parking in their neighborhood.
- Three commenters stated that vehicles using on-street parking blocks their driveway or mailbox.
- Five comments expressed interest in new parking regulations or other changes, such as:
 - No overnight parking
 - Limiting the number of cars allowed per house
 - Limiting parking to one side of the road
 - No parking within 5' of a driveway
 - Cars parking in the same direction as traffic



Summary

The online survey received 229 comments across the six survey questions from 108 respondents. The complete results of the survey can be viewed in **Appendix A**. In addition to the survey responses, an additional 114 comments were left on the Facebook posts advertising the survey. Those comments can be found in **Appendix B**.

Based on the survey responses and comments the following key observations and takeaways were found and should be considered when developing recommendations related to on-street parking changes in the Kearns Metro Township.

- 62% of respondents do not park in their yards.
- 75% of respondents were in favor of eliminating on-street parking that impedes views near intersections.
- Respondents left 32 comments expressing interest in stronger enforcement of existing parking rules, 13 of which were requests for stronger enforcement of winter parking rules. This included better signage to advertise the parking rules.
- Respondents left 86 comments across the survey expressing safety concerns related to on-street parking and neighborhood intersections in the Kearns Metro Township.
- 60 % of respondents are in favor of eliminating on-street parking on one side of the roadway for narrow roads.
- Multiple comments expressed the need to limit the types of vehicles that can park on the street including RVs, large trucks with trailers, semi-trucks, and inoperable vehicles.

Next Steps

The following are the next steps of the Kearns On-Street Parking Study:

- Preliminary recommendations and work program tables
- Transportation network analysis based on recommendations
- Stakeholder Meeting #2
- Draft Report
- Final Report

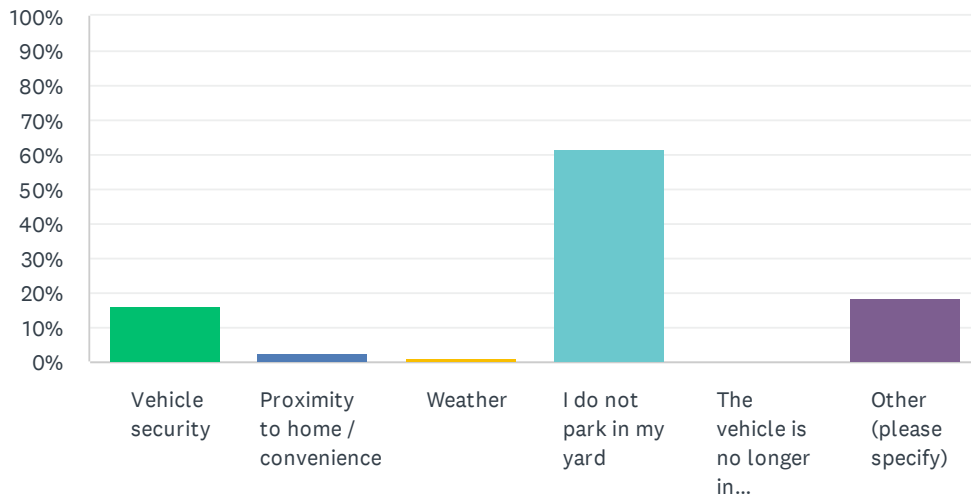


APPENDIX A

Complete Survey Results

Q1 Our study found that many vehicles were parked in yards, despite there being street parking available. If you park your vehicle in your yard, what is the reason for your decision?

Answered: 104 Skipped: 4



ANSWER CHOICES	RESPONSES	
Vehicle security	16.35%	17
Proximity to home / convenience	2.88%	3
Weather	0.96%	1
I do not park in my yard	61.54%	64
The vehicle is no longer in operation	0.00%	0
Other (please specify)	18.27%	19
TOTAL		104

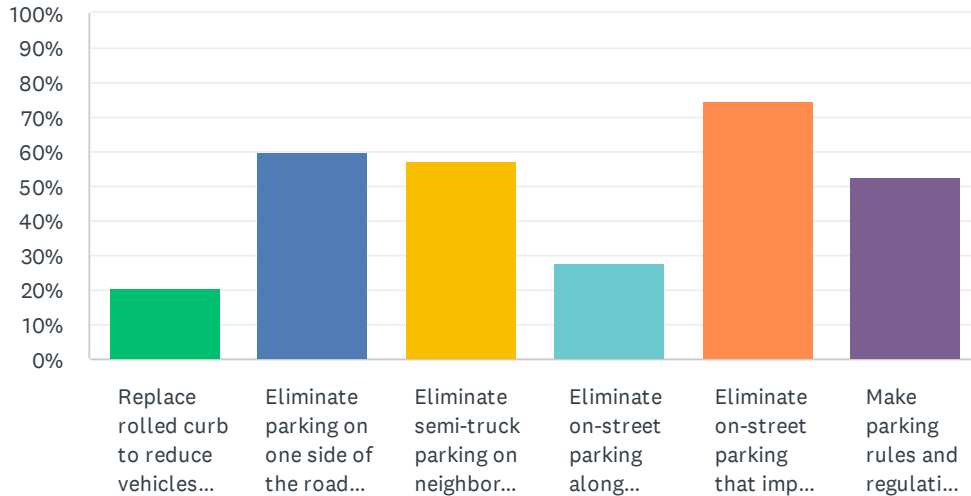
#	OTHER (PLEASE SPECIFY)	DATE
1	We park in our drive way	7/11/2022 9:07 PM
2	I don't have enough space for all of the cars in my household. Each adult in my family needs their own car to get to work because we work in different areas. Our cars are how we move around our work supplies like vacuums, trash bins, ladders, and other heavy work tools.	7/11/2022 8:53 PM
3	They tow in the winter. Also easier to see on the house cameras when they are in the yard	7/11/2022 4:48 PM
4	I park in my yard because there are tons of people breaking into cars, also because people don't know how to drive and I don't want my car getting hit on the side of the road. Also there is no street lights on my street. Mildred street. My neighbors park on the street across from me and if I park on the street there would only be room for one car to pass at a time.	7/11/2022 7:36 AM
5	I have a concrete pad to park my drivable truck	7/8/2022 8:27 AM
6	Our neighbors take all the spots in front of our house	7/6/2022 11:09 AM
7	We park our 3 cars in our driveway. If yards means on the lawn, we do not do that.	7/6/2022 8:02 AM

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8	We park in our yard due to no street room as we live in a circle and our neighbors tend to hit/ding our cars and park in ways of which we cannot park on the street.	6/30/2022 12:04 PM
9	Habit after winter from no parking on the streets for snow plows	6/30/2022 8:48 AM
10	Do you guys not see how many people complain about the street being full of cars? I don't park car in the street but I wouldn't either way because neighbors complain.	6/30/2022 7:33 AM
11	I have parking on the side of my driveway. So I am not blocking the sidewalk	6/29/2022 9:45 PM
12	I live on a corner lot, other cars take over the street parking causing me to park on my grass.	6/27/2022 11:32 AM
13	No room the way driveway is designed due to cable box in my yard. Don't park on sidewalk but in gutter	6/27/2022 10:20 AM
14	We have a long tandem driveway. Sometimes we are shuffling cars around. Trying to add a parking pad, but no concrete contractors will return our calls.	6/27/2022 8:09 AM
15	To provide more room in between cars	6/21/2022 7:27 PM
16	We park our truck on our lawn because our neighbors tend to take up the on-street parking in our circle, including constantly blocking our mailbox.	6/21/2022 8:58 AM
17	Not room for my trailer in my driveway or the street	6/20/2022 12:24 PM
18	I'm in the process of putting in concrete so I have my truck parked on the dirt what is going to be landscape rocks when finished	6/20/2022 11:31 AM
19	its my yard I can park cars in it if I want to	6/20/2022 11:19 AM

Q2 Our project team has drafted some potential recommended changes. Which recommendations would you like to see?

Answered: 103 Skipped: 5



ANSWER CHOICES	RESPONSES	
Replace rolled curb to reduce vehicles parking on sidewalks	20.39%	21
Eliminate parking on one side of the roadway for narrow roads	60.19%	62
Eliminate semi-truck parking on neighborhood streets	57.28%	59
Eliminate on-street parking along higher volume and higher speed roadways	28.16%	29
Eliminate on-street parking that impede views near intersections	74.76%	77
Make parking rules and regulations easily available to all residents	52.43%	54
Total Respondents: 103		

Q3 What recommendations do you have to improve On-Street parking in Kearns Metro Township?

Answered: 60 Skipped: 48

#	RESPONSES	DATE
1	Maybe allowing yard parking, more bus stops (not sure about trax). Actual safe areas to park work vehicles. I'm not sure if paving yards for parking would make it hotter, but maybe doing that or some kind of green/xeriscape parking. Maybe doing one-on-one meetings with people that park in their yard to talk about it. Or community meetings for people that live on a certain street to come up with ideas.	7/11/2022 8:53 PM
2	Make those that own a business not be able to park their stuff on the street. There are trucks, and trailers and attachments on the road and the roads aren't wide enough.	7/11/2022 12:21 PM
3	Perhaps use paint to show where people can park near intersections. So many people park too close and obscure the view of traffic. If red lines were painted to show where it is wrong to park and police followed up with citations, the crashes at intersections would likely decrease.	7/11/2022 9:41 AM
4	Street light so people feel more secure about parking on the street,	7/11/2022 7:36 AM
5	No parking signs on streets that come in from busier streets such as 6200 and 5400 So for at least 50 feet. Ask to limit parking to just the front of your property or limit it to 1 car on the street.	7/10/2022 12:40 AM
6	Parking only on one side of the street and actual monitoring of neighborhoods	7/8/2022 7:32 PM
7	Better enforcement I have reported people parking on sidewalks in my neighborhood many times and nothing ever happens I understand people having to park on the street sometimes but blocking sidewalks is very unsafe and the people that do it in my neighborhood don't even live on the street they are parking on	7/7/2022 7:53 PM
8	Limit parking to one side of the street. No parking on both sides.	7/7/2022 2:26 PM
9	People using their driveways	7/6/2022 1:09 PM
10	More signs for no parking on snow days. Ours have been removed for a few years now. They need to be placed more frequently, not just at entrance to the neighborhood. I'd love to see one on our circle	7/6/2022 11:14 AM
11	The churches have lawn areas, turn them into parking.	7/6/2022 10:43 AM
12	If it doesn't fit on the driveway or back yard it has to go. We have way to many single family dwellings housing 3/4 families. This causes problems with trash and debris on the yards and streets. We also have had an influx of drug trafficking	7/6/2022 8:38 AM
13	People are impatient and don't "take turns" when parking asking the street impedes traffic.	7/6/2022 8:32 AM
14	Stop parking on both sides of road and turning it into a one lane street.	7/6/2022 8:31 AM
15	How about enforcing existing parking rules? I've got a 12 ft trailer and 3/4 ton p.u. parked on my neighbors front for months and months.	7/6/2022 8:19 AM
16	What about even and odd one day you park on one side the other the next then the street sweeper can plan his day and do the job not play leap frog around cars. And get the cars off the road that never move. Most don't have a current registration	7/6/2022 8:13 AM
17	No broken down cars, no trailers or boats	7/6/2022 8:02 AM
18	Enforce a limit on how many people can be in one house. enforce a limit on how many cars can belong to a house. We have got to do something because our streets are crowded because there's too many people with too many vehicles in one single house.	7/6/2022 7:57 AM
19	Don't allow on street parking. Too many hit and runs. Reduces visibility in neighborhood for	7/6/2022 7:50 AM

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	children playing. More difficulty for snow plows and garbage collection	
20	Already listed	7/6/2022 7:42 AM
21	More enforcement. Rules don't mean much if no one enforces them.	7/6/2022 7:35 AM
22	Take stone bluff if people park on both sides you have to play chicken to get down it when someone is coming the other way same as ridge flower	7/6/2022 7:33 AM
23	Fix everyone's driveways	7/1/2022 11:32 PM
24	Stiffer penalties for violations in winter months when plows cant get through.	7/1/2022 8:40 AM
25	One side of street or at least use driveway	6/30/2022 10:35 PM
26	Not sure how to stop theft and hit and runs, but that is usually my biggest concern about parking on the streets.	6/30/2022 9:45 PM
27	Actually enforce parking violations, have someone patrol for parking, no semis and limit cul-de-sac/circle street parking to 1-2 vehicles per home.	6/30/2022 12:04 PM
28	Cars need to park closer to the curb. So many cars are parked more in the road then on the curb. There are also a lot of bigger semi like trucks parked that take up a majority of the street not to mention it really causes damage to our roads with the weight of them.	6/30/2022 10:40 AM
29	enforcement of state statue laws regarding parking distance from streets, mail boxes, etc.	6/30/2022 9:16 AM
30	I wish I knew, we live on a circle and the opening is filled with cats in both sides of the road to the point is dangerous.	6/30/2022 8:47 AM
31	Actually start ticketing people who park on the street in the winter.	6/30/2022 8:24 AM
32	Honestly let people have cars on their yard, who is it hurting??	6/30/2022 7:33 AM
33	Provide more patrols at night and better lighting	6/30/2022 7:16 AM
34	If someone has space in their driveway and still parks on the road, they should be ticketed	6/30/2022 3:42 AM
35	Parking blocking the sidewalk is an issue also I think you shouldn't park in front of your neighbors house on a regular basis. Also only one or two cars per house hold should be able to park on the street. Some thing about parking should be done.	6/29/2022 9:45 PM
36	I've seen some large trailers and RVs parked outside homes on the streets for weeks/months. One was a corner house that had a two car length trailer so it was a pain trying to maneuver around and get into the cult de sac every day for weeks. It periodically comes back and sits for weeks again.	6/28/2022 8:08 AM
37	Stop parking on sidewalks	6/27/2022 4:00 PM
38	Have people park in their driveways	6/27/2022 3:45 PM
39	Install curbing between gutter and sidewalk	6/27/2022 1:33 PM
40	More officers for enforcement. None of the questions above will affect a change unless there is strict enforcement.	6/27/2022 1:30 PM
41	Too many burglaries!	6/27/2022 11:40 AM
42	Place signs that help understand parking laws. Ex: don't park in front of driveways which happens to me all the time, must be at least 5 feet. Signs/painted curb for not parking next to fire hydrant, signs/painted curb for corners not supposed to park.	6/27/2022 11:32 AM
43	Remove cars that don't run	6/27/2022 8:24 AM
44	Enforce parking issues to promote safety, especially in winter and near curves or intersections	6/27/2022 8:22 AM
45	Parking too close to corners and stop signs make visibility low and dangerous.	6/21/2022 7:27 PM
46	Give tickets for parking on corners they will park right on the corner	6/21/2022 4:31 PM
47	Make stricter rules for blocking mailboxes. Don't allow street parking on narrow roads especially on the smaller neighborhood roads like Jeremiah Drive, it's always a maze to get through there and the people clearly have driveways.	6/21/2022 8:58 AM

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48	In our neighborhood the problem is trailers parked on the street that are not moved very often. Some if them are wider than a normal vehicle. These should be required to be parked off street	6/20/2022 7:00 PM
49	Not having parking on both sides of street	6/20/2022 3:27 PM
50	Don't allow it overnight	6/20/2022 2:47 PM
51	Signs posted at regular intervals to remind community to remove cars from roadsides during snow storms.	6/20/2022 1:32 PM
52	Make all vehicles parked in the street are licensed and registered. There are so many inoperable vehicles parked on our streets.	6/20/2022 12:24 PM
53	Make it legal to park in your yard.	6/20/2022 12:24 PM
54	Enforce the rules and laws hold people accountable.	6/20/2022 12:05 PM
55	I don't think people should park their boats in the road for the entire summer!	6/20/2022 11:57 AM
56	Remove vehicles that never move, that are not licensed.	6/20/2022 11:31 AM
57	enforce the 'no parking on the road when theres snow on the ground' rule	6/20/2022 11:19 AM
58	No parking within 100 feet of intersection.	6/20/2022 11:08 AM
59	Is there a way to limit how often someone parks in front of another person's home? I'm fine with people who are visiting or otherwise temporarily using the space in front of my house, however I feel my neighbors use that space in a permanent fasion, which causes my visitors to park farther away.	6/20/2022 10:46 AM
60	There should be a limit to how long a vehicle can park on the street. Additionally, on narrow roads, vehicles should only be allowed to park on one side of the street.	6/20/2022 10:17 AM

Q4 What additional items do you think the project team needs to know about on-street parking in Kearns Metro Township?

Answered: 42 Skipped: 66

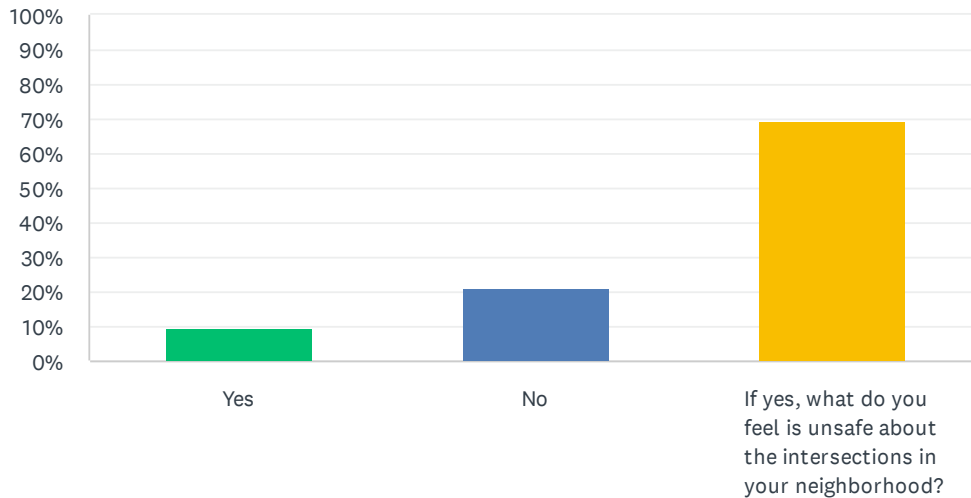
#	RESPONSES	DATE
1	I think rules and regulations should take into consideration that some households have a lot of cars for a reason. It's expensive to park your car elsewhere, to move around your work car or work tools, and that some households have a lot of families.	7/11/2022 8:53 PM
2	It happens because our neighborhoods are old and only allow for one car in the driveway, two of you put them one in front of the other. We don't all have garages. The street is the only other option; people will not NOT park in the street. It would be unfair to make it illegal to park on both sides.	7/11/2022 9:41 AM
3	There needs to be more reinforcement/ticketing during the winter months when plows are in use.	7/10/2022 12:40 AM
4	Not parking but speed humps on copper city would be great.	7/8/2022 8:27 AM
5	There are many houses with multi generation families, each with their own car.	7/7/2022 2:26 PM
6	I wish would paint some side straps red on curb. People park against my yard, deal drugs, throw trash, smoke etc	7/6/2022 8:38 AM
7	Get trailers out of the road.	7/6/2022 8:31 AM
8	Limit how many cars can be parked in front of houses except for gatherings/parties. Example there is a house that has 8+ cars (usually on average 12) everyday blocking views from 2 intersections.	7/6/2022 8:17 AM
9	If a home has more than one family, must have adequate off-street parking.	7/6/2022 8:02 AM
10	It's a horrible idea; don't allow	7/6/2022 7:50 AM
11	Courtesy when taking turns to drive between parked cars.	7/6/2022 7:42 AM
12	There is no room no matter how you look at it!	7/6/2022 7:35 AM
13	It's a pain in the butt	7/1/2022 11:32 PM
14	It is very dangerous	6/30/2022 10:35 PM
15	Plows need to be able to operate effectively in the winter, not sure that on-street parking is the way to go for that either. Maybe if we didn't need so many cars to get around in this city in a timely fashion?	6/30/2022 9:45 PM
16	Trailers parked to close to intersections and make it difficult to see oncoming traffic. Or they are parked to close to crosswalks making it a hazard to see pedestrians.	6/30/2022 10:40 AM
17	Have the rules enforced, they say no parking on streets when it snows but there have been many times they can't get into our street because of the narrowness.	6/30/2022 8:47 AM
18	If on street parking is used it creates the roads to be so narrow that you cannot get through. An emergency vehicle would not be able make it up or down some of our roads that have cars parked on both sides of the street. A regular car barely fits.	6/30/2022 8:26 AM
19	If I ever need to pull my car off my driveway and onto the street, I have to park two houses away because my neighbors have so many cars they take up the street in front of my house. In my opinion, of people want to have 5 cars that don't run, they should park them in their yard, and not jam up the road. If a kid runs out to cross the street in my road, I will not see them coming because the cars on my street block visibility and give no where to swerve if anything out of the ordinary happens.	6/30/2022 8:24 AM
20	In the winter better enforcement of no parking is needed	6/30/2022 7:16 AM

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21	It is an issue, something needs to be done. Cars are taking over other peoples view and or their own parking in front of their own homes.	6/29/2022 9:45 PM
22	I know of one house that does because several vehicles have taken the corner too aggressively and run into their fence, tree, and car. They park on the sidewalk to hopefully reduce the chance their car is hit, and they use the car as a buffer for their house.	6/28/2022 8:08 AM
23	My neighbors don't park in their driveway they park all 5 cars in the street super rude	6/27/2022 3:45 PM
24	Many people are running business out of there homes for car repair. My backyard neighbor had 6 cars in his backyard at somepoint. Not sure what the regulation is on this, but again, I only think more officers can really bring a change to this issue.	6/27/2022 1:30 PM
25	Some homes have multiple vehicles and do not have off street parking for all the vehicles. Denying parking on street would be a hardships	6/27/2022 1:12 PM
26	Brighter street lights	6/27/2022 11:40 AM
27	Signs explaining winter parking hours.	6/27/2022 11:32 AM
28	Can't see oncoming traffic when at intersections that have people parked on the street.	6/27/2022 10:59 AM
29	More enforcement. I have neighbors that have cars on jackstands missing tires/wheels ON the sidewalk.	6/27/2022 8:09 AM
30	Some people park in the middle of the road and others clear up on the side walk	6/21/2022 4:31 PM
31	We should have some kind of restrictions for people that are throwing parties but they have to have their guests Park all on one side of the road and not fill up Circles or park within 5 ft. of a driveway.	6/21/2022 8:58 AM
32	People think they own the street in front of their house.	6/20/2022 7:00 PM
33	I think it would be good if people could be more aware that they should avoid parking in a way that obstructs the sidewalk. This is especially important for pedestrians with disabilities.	6/20/2022 3:45 PM
34	Park to close to intersections off 6209 south	6/20/2022 3:27 PM
35	Width limits for on-street parking for trailers, RV's, etc.	6/20/2022 1:32 PM
36	Keep cars off the sidewalk so I can keep out of the street when walking.	6/20/2022 12:24 PM
37	It's a bigger issue of people not be self sufficient, not living within their means, too many people living in a house with cars. People not taking care of their homes. No values instilled and not caring about neighbors.	6/20/2022 12:05 PM
38	people need to MOVE their vehicles in the winter time so our roads get plowed!	6/20/2022 11:57 AM
39	Some homes have more than 3 drivers and need the on street parking. Don't take that away	6/20/2022 11:48 AM
40	Limit time parking on sidewalks. I only do it with my trailer. Has been hit on the road. So I keep it on the sidewalk no more than 12 hours.	6/20/2022 11:08 AM
41	I think many of the single family homes are serving multiple families and businesses as well. This increases the demand for parking.	6/20/2022 10:46 AM
42	I feel that if someone rents a portion of their home to someone else, the owner should be required to provide off-street parking for the renters. Most of the homes in Kearns weren't built to accommodate the number of vehicles that the occupants currently have.	6/20/2022 10:17 AM

Q5 Do you feel there are safety concerns at intersections in your neighborhood?

Answered: 105 Skipped: 3



ANSWER CHOICES	RESPONSES	
Yes	9.52%	10
No	20.95%	22
If yes, what do you feel is unsafe about the intersections in your neighborhood?	69.52%	73
TOTAL		105

#	IF YES, WHAT DO YOU FEEL IS UNSAFE ABOUT THE INTERSECTIONS IN YOUR NEIGHBORHOOD?	DATE
1	There is a 4 way intersection no stop signs kids walk to school and have to cross the street one of the directions there are no speed bumps and cars fly by putting other cars a people kids at risk. At the very least one stop sign and speed bumps. Also adding to the problem cars parked on street to close to 4 way stop block the view for other cars and can not always Dee kids going across. I see this every day .	7/11/2022 9:07 PM
2	I think a lot of people don't stop at intersections or run red lights. It's really hard for pedestrians especially. Maybe a way to make intersections slower, or pop out to get people's attention to stop or go slower.	7/11/2022 8:53 PM
3	I live on a narrow street. My spouse owns a diesel truck Ford F360 with a full size bed (which you don't see to often anymore) He has to pull in and out of the driveway a few times to pull out at an angle so, that he doesn't hit vehicles that are parked on the street.	7/11/2022 5:09 PM
4	Speeding in neighborhood intersections with cars parked on the road makes a blind intersection and potential wrecks.	7/11/2022 4:48 PM
5	There is one intersection in my neighborhood, Jeremiah dr and 5415 W that doesn't have a stop sign on either corner. No one know who has right of way. Also the corner of Jeremiah Dr and Coulter Dr. No stop signs and traffic coming off of Coulter don't stop	7/11/2022 12:21 PM
6	People parking too close to them, blocking the view of traffic.	7/11/2022 9:41 AM
7	At the intersection of 4620 south and 5215 west they added cement blockers on the road. They claim to reduce vehicle speed but as you can see they are already marked up with tire	7/11/2022 7:36 AM

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marks from people hitting them. When it snows they are going to be invisible and people are going to total there cars and then sue kearns for adding those.

8	The cars and post officers park too close to the intersection so you can't see around them.	7/10/2022 12:40 AM
9	People not yielding it stopping	7/8/2022 7:32 PM
10	Driving to fast and not stopping at signs	7/8/2022 8:27 AM
11	People parking on both sides of the street. Parking right next to intersections, obstructing views, and parking on curves.	7/7/2022 2:26 PM
12	People drive to fast in residential areas, more speed bumps needed to slow them down.	7/7/2022 9:25 AM
13	too many people race through neighborhood intersections without slowing down or stopping to take any caution on other cars or people walking	7/6/2022 9:15 PM
14	Curvatures of roadways make for certain 2-way stop only iffy	7/6/2022 1:09 PM
15	Cougar is super busy. It's a pain to get out of our neighborhood during soccer season	7/6/2022 11:09 AM
16	cars too near/blocking views of intersection	7/6/2022 9:57 AM
17	People park too close to the corner where you turn onto leprechaun. You can never see if someone is coming.	7/6/2022 9:24 AM
18	People parking where your view is blocked.	7/6/2022 9:22 AM
19	You can't see to turn around cars safely. Also at some it would be easy to have a car backed up into you as you turn because so many cars are next to their driveway so they don't see you driving down the road trying to turn by their house.	7/6/2022 8:40 AM
20	People race down Dewdrops Dr. All day long. No speed bumps, no patrol.	7/6/2022 8:38 AM
21	There are two intersections that need to be four way stops because you can't see oncoming traffic.	7/6/2022 8:32 AM
22	Can't see when cars are on top of the intersection.	7/6/2022 8:31 AM
23	Some have to many cars parked around them can't see to pull out. Others some people don't stop before pulling out.	7/6/2022 8:17 AM
24	Parking too close to corners/intersections	7/6/2022 8:17 AM
25	They park to close to them. And right next to stop signs	7/6/2022 8:13 AM
26	People driving through stop signs. Cars parked too close to intersection	7/6/2022 8:02 AM
27	People are parking on both sides of narrow streets and way too close to the intersection.	7/6/2022 7:57 AM
28	Trowbridge Way and 6200 south is a terrible intersection	7/6/2022 7:50 AM
29	Cars, RV's, and trailers parked too near intersections reducing visibility	7/6/2022 7:50 AM
30	Parking too close to intersection	7/6/2022 7:42 AM
31	Too many trees! Can't see the stop signs.	7/6/2022 7:35 AM
32	No stop signs	7/1/2022 11:32 PM
33	Vehicles parking too close to corners.	7/1/2022 8:40 AM
34	6200 S Impressions needs a stop light. Stop signs on Impressions need flashing lights.	6/30/2022 10:35 PM
35	People driving on the neighborhood streets like they are freeways (speeding)	6/30/2022 9:45 PM
36	People park to close to the end of the street so you can't see oncoming traffic when exiting	6/30/2022 12:45 PM
37	I almost pulled out in front of a suburban this morning because of a truck parked on the side blocked my view. It also had the hood up and passenger door open. Been there for days. It is on 4620 West by Caplan Street.	6/30/2022 12:14 PM
38	There are no stop signs and with hills/blind corners it's really hard to see other cars, as well as no speed limit signs in the neighborhood.	6/30/2022 12:04 PM

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39	Aww	6/30/2022 11:45 AM
40	5600 west needs another light in-between Jefferson Jr high and 5400 south. It's terrible and would help slow the traffic there.	6/30/2022 8:47 AM
41	We have so many cars parked on the streets that you cannot see if another car is coming to safely pull out	6/30/2022 8:26 AM
42	Too many cars parked in the street make visibility almost non existent to see oncoming cars.	6/30/2022 8:24 AM
43	Many cars parked on the side, and you can't turn correctly.	6/29/2022 10:09 PM
44	You turn the corner and boom a car it's hard to see then at night. When you have two cars parked on opposite sides of the street it's hard to maneuver around them in the dark.	6/29/2022 9:45 PM
45	There are some intersections that have a stop one direction and no stop the other. Sometimes the one with a stop will have people ignore the stop and almost hit those without the stop sign.	6/28/2022 8:08 AM
46	Cars park to close to 5400 so and 5240 west big dip hard to see	6/27/2022 3:45 PM
47	People treating stop signs as an opportunity to see how fast they can accelerate. I would love dips at every stop sign to slow people down. Dips seems like an easy effective way to control speeds on residential roads.	6/27/2022 1:30 PM
48	People park right up to the corner.	6/27/2022 1:12 PM
49	Intersections are dark	6/27/2022 11:40 AM
50	They do not have stop signs. Cars parked illegally blocking view of roads, blocking view for pedestrians.	6/27/2022 11:32 AM
51	People parked on street close to intersections makes it hard to see if there's incoming traffic. More stop signs in some high traffic intersections are needed as well.	6/27/2022 10:59 AM
52	Bushes on the corner so you can't see. People parking trailers in street make it hard to see up atreet	6/27/2022 10:20 AM
53	Multiple crashes on 54th south around 48-62 west due to turning into neighborhoods.	6/27/2022 8:22 AM
54	Parking too close to the corners, reducing visibility.	6/27/2022 8:09 AM
55	Parking too close and making visibility difficult.	6/21/2022 7:27 PM
56	People run them or do California stops or don't know how a four way goes	6/21/2022 4:31 PM
57	Parking too close to the intersection. Running the stop sign and speeding	6/20/2022 7:00 PM
58	No stop signs on streets that enter on main roads so cars don't stop	6/20/2022 3:27 PM
59	6200 S Impressions needs a stop light so many crashes there	6/20/2022 2:47 PM
60	Many trees and weeds are not trimmed back, and many vehicles parked on street too close to intersections. You cannot see the intersection without pulling out further into the intersection to see if vehicles are coming.	6/20/2022 12:30 PM
61	Vehicles parked where you can't see traffic coming on 5400 s coming out of the neighborhood	6/20/2022 12:24 PM
62	There are many intersection without stop signs in any direction.	6/20/2022 12:18 PM
63	People don't follow the stop sign and speeding in a residential area.	6/20/2022 12:05 PM
64	People are running the lights at 4420 west all day long!	6/20/2022 12:01 PM
65	There are so many cars parked on the roads I cannot see the oncoming traffic from most stop signs	6/20/2022 11:57 AM
66	There's a tall fence you can't see down the road because the road bends	6/20/2022 11:31 AM
67	4300w 4700s its hard to see oncoming traffic when merging going northbound	6/20/2022 11:19 AM
68	Can't see around parked cars and some of the duos in the road are very very deep people don't expect them and it causes problems.	6/20/2022 11:06 AM
69	No one stops. Don't know how to treat a 4 way stop.	6/20/2022 10:36 AM

Kearns On-Street Parking Study

70	intersection of 4015 W and 5780 S: please restrict parking close to the intersection, particularly on the northwest corner. Due to the curve & hill it's hard to see oncoming traffic when people park close to the intersection.	6/20/2022 10:34 AM
71	Vehicles parked too close to intersections. Too many cars parked on both sides of the street to safely navigate intersections.	6/20/2022 10:17 AM
72	Cars park too close to intersection so you can't see to turn.	6/20/2022 9:59 AM
73	We have a 4 way stop that people just fly through!	6/20/2022 9:28 AM

Q6 Do you have any other comments about on-street parking in Kearns Metro Township?

Answered: 35 Skipped: 73

#	RESPONSES	DATE
1	There is a sign right across the street from my house no parking when snow or ice is present and yet still do and it's a very narrow street and the snow plows can not do their jobs. Sometime after the plows have come by due to cars parked they are unable to move the snow away from the front of our house. It makes it really hard for us. The plow only can push what he can and it usually ends up on our side of the street in front of our house due to cars parked on both sides of a very narrow street. Please come and look at this and the 4 way intersection with no stop sign.	7/11/2022 9:07 PM
2	Cars that aren't driven are parked on the road	7/11/2022 4:48 PM
3	People are going to park where they park no matter what you change.	7/11/2022 7:36 AM
4	It would be nice to see some enforcement of existing ordinances.	7/7/2022 2:26 PM
5	Speed bumps are needed, especially on the main neighborhood roads	7/6/2022 1:09 PM
6	No	7/6/2022 9:24 AM
7	I built here in 1996 and we have never been repaved. We have more and more potholes all the time	7/6/2022 8:38 AM
8	People blocking driveways	7/6/2022 8:02 AM
9	I believe we have too many cars parked on the roads. I feel it's dangerous with blocked view of kids and animals.	7/6/2022 7:42 AM
10	I think it should be limited we have one family that doesn't use their driveway but parks 4 on the street	7/6/2022 7:33 AM
11	Good luck	7/1/2022 11:32 PM
12	Concert traffic and parking is a problem in some neighborhoods.	7/1/2022 8:40 AM
13	It can be nice for gatherings to have city parks nearby with large areas of curb that don't block houses or driveways. Parks are expensive to maintain though... Xeriscape parks??	6/30/2022 9:45 PM
14	There are far too many cars parked on the streets and in yards. We live in a circle and cars are parked all around it making it extremely difficult to enter and exit. There are also large work trucks that make driving extremely difficult as they block most of the road	6/30/2022 12:45 PM
15	Cars should be facing the way traffic goes.	6/30/2022 12:14 PM
16	It's definitely a big problem in our area. I live on Alpine Point Circle and nights and weekends are a joke. Including unlicensed vehicles.	6/30/2022 8:47 AM
17	Yes. We should not be encouraging more of it. Most of the cars parked in yards don't run and aren't licensed. We should encourage people to clean up their junk so they can pull off the roads to increase visibility, thus protecting our kids, not turning every road into a single lane, and making our neighborhoods look cleaner and more open.	6/30/2022 8:24 AM
18	Stop punishing people for using their property.	6/30/2022 7:33 AM
19	Crack down hard core during winter because of people who park on the street when it's snowing	6/30/2022 3:42 AM
20	It's frustrating having neighbors with vehicles that are on their driveway, lawn, and street that monopolize all the parking and make their home a blight on the neighborhood. My neighbors and I have been improving our homes. Our one neighbor has a broken fence for a couple years (refuses assistance with repairs), parks cars on their driveway, street, and grass (the son	6/28/2022 8:08 AM

Kearns On-Street Parking Study

collects old vehicles and works on them in the driveway or street), and has a two car trailer parked at the corner so we have to maneuver around it. The neighbor also made it difficult when another neighbor was having a shed delivered to access the street.

21	No more then a few hours no over night street parking	6/27/2022 3:45 PM
22	Clearly marking where street parking is unsafe will help.	6/27/2022 8:22 AM
23	Limit the # of cars per household, 1 per bedroom could be a place to start. My neighbors have 4 drivers and 6 cars- 3 parked in their yard.	6/27/2022 8:09 AM
24	Big problem in my neighborhood glad your looking into it	6/21/2022 4:31 PM
25	There's a lot of areas in Kearns where the neighborhood is older and the streets are really narrow. I think it would be super beneficial if you were only allowed to park on one side and no closer than 5 ft. to a driveway.	6/21/2022 8:58 AM
26	Why do people think it is okay to park in front of their neighbors mailbox.	6/20/2022 7:00 PM
27	Get the trailers, campers and un-registered vehicles off the roads. Stop people parking cars across from each other and turning the road to one lane. Do some code enforcement and get vehicles and trailers off the dirt.	6/20/2022 5:59 PM
28	Don't allow it overnight	6/20/2022 2:47 PM
29	No on street parking in winter needs to be enforced.	6/20/2022 12:30 PM
30	Enforce the rules and laws if people are unhappy they can discuss it and see it's not about them individually we are a society.	6/20/2022 12:05 PM
31	more parking is better... unless it makes the road dangerous (for high speed high traffic roads)	6/20/2022 11:19 AM
32	Issue more parking tickets.	6/20/2022 11:08 AM
33	It's really hard to drive down the street because cars are lining both sides of the street and you can hardly fit another car. It would be nice to change that. However I'm not sure where people will put their cars because that's a lot of cars to move, especially if it's just someone visiting for a few hours. Though I realize most of the problem is that it's consistently parked cars on the street more than a visitor.	6/20/2022 11:06 AM
34	Should be allowed except during a snow storm.	6/20/2022 10:36 AM
35	Be more strict about parking on streets on snowy days!	6/20/2022 9:28 AM



APPENDIX B

Facebook Comments



Kearns Metro Township and the Greater Salt Lake Municipal Services District have conducted an On-Street Parking Study to evaluate parking availability, demand for parking, and parking habits.

Our study chose two areas in Kearns that represented the diverse homes and neighborhoods in Metro Township. We studied parking habits on one weekday and one weekend day in the morning, afternoon, evening, and nighttime. Our team used these findings to draft some potential recommended ch... See more



on a given day.

25% - 35%

*of vehicles
using on-street
parking had
parked on the
sidewalk.*



*During our study,
we noticed over*

2,000

*vehicles parking
in their yards.*

14%

*of crashes within the
study area involved a
parked car. That's more
than average in Salt*



**Julie Anderson**

I appreciate the effort that goes into understanding issues and trying to find solutions! Thank you for sharing the finding with us

Like Reply Share 4 w

**Chris Chamberlain**

2000 cars parked in yards? Seems high... There can't be much more than 2000 homes in those two areas...

Like Reply Share 4 w

**Jared Smith**

Chris Chamberlain this doesn't seem too high, when you have some homes with multiple drivers. I have 3 drivers in my home with 1 of the 3 parking directly on our lawn so we aren't in the street. And the other parking in an tv pad. Between myself and th... See more

Like Reply Share 4 w



2

**Chris Chamberlain**

Jared Smith thanks for the insight.

Like Reply Share 4 w

**Tabitha Hernandez**

Chris Chamberlain I passed one the other day that had 6 cars parked in their lawn!

Like Reply Share 4 w

**Reeve Chidester**

I ride my battery-powered mobility vehicles in the street because of the many cars parked on the sidewalks.

Like Reply Share 4 w



3

**Cherolyne Jardine**

Wintertime is bad.



Like Reply Share 4 w

**Scott Osborn**

Maybe let's look at tightening up the regulations on the sheer numbers of people that can be in one house. My neighbors often have around eight vehicles that need to be parked. They don't have that much space at their own house so the surrounding neigh... See more

Like Reply Share 4 w Edited



10

**Lisa Fredrickson**

Scott Osborn I agree this is our problem as well I know all if my neighbors feel the same way.

Like Reply Share 4 w

**Jennifer Jackowski**

Scott Osborn many people have had to move back in with family due to the shy rocketing cost of housing. Maybe some compassion would help. I have two cars. My neighbors have several. In the past when my son and his family lived with me I had two more. D... See more

Like Reply Share 4 w



15

↳ 6 replies

↳ View 1 more reply

**Darla Riley Denning**

I know kearns doesn't have the rules Wvc does, I wish it did. Vehicles, rv's, garbage yards etc. I don't like it and it doesn't help kearns look like a nice place to live. So many people don't take any pride in their yards or homes here so it makes i... See more

Like Reply Share 4 w



10

Darla Riley Denning

I know kearns doesn't have the rules Wvc does, I wish it did. Vehicles, rv's , garbage yards etc. I don't like it and it doesn't help kearns look like a nice place to live. So many people don't take any pride in their yards or homes here so it makes i... See more

Like Reply Share 4 w

10

Debbie Davis Wall

Darla Riley Denning the County does but no one seems to enforce the rules.

Like Reply Share 4 w

Chrissy Marie

Darla Riley Denning that's the difference between being a township and not a city.

Like Reply Share 4 w

Saa Lou

You can't park on your yard?

Like Reply Share 4 w

Trav E Fry

Correct. It must be slab or tight gravel with weed blocker underneath. Also that can not cover more than 50% of your yard. I just recently found this out due to the bulb out blocking front parking space for visitors on road and an entrance to my RV gat... See more

Like Reply Share 4 w

Grace Burton

Alfonso Guerra-Urcino

Like Reply Share 4 w

Heather Smith

I would like to know why those chose 2 areas North of 54th, what about South of 54th. Vista Ridge is a joke

Like Reply Share 4 w

Susan Young

So glad that the biggest issue to study in Kearns is parking...😄

Like Reply Share 4 w

Whitney Dimova

Susan Young as opposed to...? They have to start somewhere!

Like Reply Share 4 w

Susan Young

Whitney Dimova how about all vehicle break ins, the vandalism, and not to mention the speeders. I understand they start somewhere but to be honest the problem is mostly due to multiple people having to live together due to finances. I don't see how tha... See more

Like Reply Share 4 w

Kelly Fashimpaur Bush

Susan Young this is a municipal service study that was conducted based off of multiple things that the community named off as top concerns. There will be more results coming.

Like Reply Share 4 w

Gayle Rider

I would rather people park on the lawn than to park in the street. Our streets are too narrow to having parking on the street. They are way to narrow especially when cars are parking on both sides of the street, you can barely fit your car down the street.

Like Reply Share 5 d

Gayle Rider

Kyle Dr on the corner and on the bend are horrible

Like Reply Share 5 d



KEARNS RESIDENTS - Many residents in the study area park on the sidewalk. Do you park on the sidewalk? Have you noticed cars in your neighborhood parking on the sidewalk? [Kearns Metro Township](#) Tell us more here: <https://tinyurl.com/KearnsParking>

25% - 35%

*of vehicles
using on-street
parking had
parked on the
sidewalk.*



6

4 comments

Like

Comment

Share

Top comments



Write a public comment...



Laura Thomson Privett

Some of the roads aren't wide enough for people not to park there.

Like Reply Share 3



Jared Smith

I don't think people understand all the parking laws. Ex: must park 5 feet away from a driveway. One of my driveways gets blocked often due to the high amount of on street parking.

Like Reply Share 2



Stephanie Scholin

I have to go into the road with my daughter in the stroller because people are literally on the sidewalk.

Like Reply Share 1



Stacey Meyers Baird

I don't park on the sidewalk but I have noticed some cars parked that way.

Like Reply Share 1

Write a comment...

KEARNS RESIDENTS - While collecting data during the various days and time, our team noticed over 2,000 vehicles parked in yards. On average, only 19%-28% of available on-street parking was used. If the vehicles parked in yards were relocated to the street, we'd see about 40% of available on-street parking used. Have you noticed this trend in your neighborhood? [Kearns Metro Township](#) Tell us more here: <https://tinyurl.com/KearnsParking>



*During our study,
we noticed over*

2,000

*vehicles parking
in their yards.*

👍👍👎 15

50 comments 4 shares

👍 Like

💬 Comment

➦ Share

All comments ▼



Kevin Rider

And over half don't run or have a current plate! And now you want them on the street! How about they just get rid of them! You should try driving a street sweeper or a snow plow!

Like Reply Share 3 w



7



Ashley Davis

I park in my yard and driveway. Less reason for wandering eyes and fingers messing with our stuff than if it were on the street.

Like Reply Share 3 w



7



Ben Kittrell

how about we enforce the no parking on the street when theres snow on the ground? thats the only thing public safety related with parking

Like Reply Share 3 w



8



Cody Hirsch

So, you're wanting me to park my vehicles on the street where they're most vulnerable to get broken into? Hard pass. I'll park them on my property that I own.

Like Reply Share 3 w



9



Robert Philip Frank

Cody Hirsch exactly

Like Reply Share 3 w



Darbi Trease

Cody Hirsch even if everyone parked on the street, they would bitch about it being parked on the street to long lol. I have a neighbor that got a warning on their car for it being on the street even when it was registered and drove. Guess she just didn't drive it enough.

Like Reply Share 3 w



Cody Hirsch

Darbi, I have a neighbor whom has a trailer parked blocking the sidewalk for over a month now. And they're not doing shit about it. But let's talk about paying on the street. 🤔🤔

Like Reply Share 3 w



Darbi Trease

They aren't gonna be happy either way. I'm just saying they bitch if it's on your property and they bitch if it's on the street. You can't win with them.

Like Reply Share 3 w



Bobbie DiChellis

I already have the parking in front of my house taken up by the neighbors, let's not make it worse. Let's enforce getting rid of junk cars on the streets!

Like Reply Share 3 w



12



Derek N Hanna Gleue

Both our cars are parked in our driveway. Whenever I need to pull one out to the street, I have to park it down away from my house because neighbors already take up all the space in front of my house.

We also have two toddlers, who are still learning not to run into the road. Cars parked on the road make visibility for drivers seeing kids in the road a whole lot worse. Encouraging more street parking is unsafe. We should be encouraging getting rid of junk vehicles and getting off the roads.

Like Reply Share 3 w Edited



9

Candy Iona Wade
If we had a big enough driveway then we wouldn't be on the "lawn" area. A lot of these older homes don't have a enough room for multiple cars, because back when they were built there was usually only one car per household

Like Reply Share 3 w



6

Sherry Talbert

Candy Iona Wade Maybe you have to many cars or park some out back. I had four cars and a big motor home and you did not see any of them in my drive way or on the street

Like Reply Share 3 w



2

Jodene Paulsen Valle

Candy Iona Wade I can't fit any vehicles in my backyard because of the way the pre-existing House was built. There is no possible way to get in our backyard.

Like Reply Share 3 w

Candy Iona Wade

I only have two cars and they are in working order and driven daily

Like Reply Share 3 w Edited

Candy Iona Wade

if I park behind the other car, or vice versa, our trunks would be blocking the sidewalk

Like Reply Share 3 w Edited

Amanda Nicole Cordova Tovar

Are you talking about summit County? Because they don't like "tire marks" on their radiant heat 1/4 mile driveways. But in kearns? My cars go in my driveway on my property so they don't get messed with or ran into. Small streets need room for visibility especially in the summer and to be cleared in winter for the plow! What kinda stupid question is this?

Like Reply Share 3 w



7

Brandie Smith

Its probably because if on street parking is used it creates the roads to be so narrow that you cannot get through. An emergency vehicle would not be able make it up or down some of our roads that have cars parked on both sides of the street. A regular car barely fits.

Like Reply Share 3 w



8

Vickie Parry

Brandie Smith you are right. We live on a cul-de-sac and sometimes are barely able to drive one car up the street. As you said, emergency vehicles would have a hard time getting to homes.

Like Reply Share 3 w



3

Ryan Rawlings

Brandie Smith narrow streets are a feature, not a bug. My street, where I sometimes have to wait a second to let another car pass doesn't ever have people driving past our house at 50 miles per hour.

Like Reply Share 3 w

↳ View 1 more reply

Carolyn Kirkham Franklin

Roads are too narrow to get through or to turn into your driveway if there are cars parked in every available spot.

Like Reply Share 3 w



5



Jacqueline Diviney

I'd rather have neighbors parking on their own lawn than congesting the streets so you can't safely get down them because it becomes a one lane road when there's cars parked on both side of the streets in Hoffman Heights area. All the cars on the street also block a driver's ability to see children and pets in the neighborhood.

Since this is coming from a code enforcement point of view how about a community effort to assist residents to xeriscape or expand driveways so they have more off street parking on their own property and are also reducing water usage. With housing being unaffordable people are having to rent rooms to avoid homelessness which has increased the number of cars in the area.

Like Reply Share 3 w



Adrienne Finnas

I can barely get down my street as it is!

Like Reply Share 3 w



Tammy Griffith

There is really no available parking. The streets are too narrow except on impressions. I live in a circle and the garbage trucks and snow plows can't get in easily

Like Reply Share 3 w



Sarah Maestas Sonntag

Tammy Griffith I'm in a circle too- agreed.

Like Reply Share 3 w



Nicole Scarton

Tammy Griffith my street is pretty consistently a one way because of all of the cars parked on it... with empty driveways. It's fun.

Like Reply Share 3 w



Natalie R Grahl

We have a dirt/gravel patch to the side of our driveway and our lawn on the other. We never park on the lawn. We park 2 vehicles on the gravel patch. It looks nicer than parking them on the street, and keeps them more protected from damage.

Like Reply Share 3 w



Ashley Lehr - Mackay

We've parked our cars on the street in front of our house and they've been vandalized before. So I'm going to keep them parked behind our fenced in front yard.

Like Reply Share 3 w



Vern Fitzgerald

I'm just going to point out that the opposite is desired during the winter. When, your vehicle can get towed for parking on the street.

No solution from me. Just an observation.

Like Reply Share 3 w





Janet Nerdin

In the Western Hills neighborhood, many of the streets are too narrow to accommodate parking on both sides of the street while allowing for two lanes of traffic flow. Unfortunately, cars are parked on both sides of the street on many of these streets. This creates a hazard for both drivers and pedestrians. Additionally, it makes it difficult for emergency vehicles, garbage trucks and mail delivery drivers to navigate through these areas. It increases the risk of a head-on collision with cars going the opposite direction. For pedestrians, the parked cars make it difficult to see oncoming traffic while crossing streets. Furthermore, many of the streets are hilly and curvy which makes them poorly suited for street parking and makes it impossible to navigate, especially during during a snow storm. A few of the streets that are problematic for street parking include: Leprechaun Lane, Shalee Drive, Jedediah Drive and Crockett Drive. The number of cars parked on these streets increases during evening/early morning hours and weekends.

Like Reply Share 3 w



2



David Chidester

Janet Nerdin add Jeremiah Drive between Westslope and Mountain Men to that list.

Like Reply Share 3 w



Sarah Maestas Sonntag

I'd rather have cars parked in the yard than all up and down the streets so you can't see around the corner - that's a big issue on my street

Like Reply Share 3 w



3



Star Muniz

Yes I've noticed it's getting worse

Like Reply Share 3 w



AmberLee HB

Yup...all neighbors do it, it's tacky. At least they aren't in front Of my house or in my parking

Like Reply Share 3 w Edited



Bobbie DiChellis

AmberLee HB I never get to park in front of my yard, nor do my friends, the neighbors are always parked there.

Like Reply Share 3 w



AmberLee HB

Bobbie DiChellis That is so rude that never happens in my neighborhood perhaps we all are pretty respectful of the front of one another's home, unless of course there is some sort of an event going on in which case we usually communicate with one another!

Like Reply Share 3 w



Lizzie Berry Bugby

Confused at this survey. We were told once they did the 4700 project we wouldn't be allowed to park on the street anymore. It says that it's starting late summer 2022.

Like Reply Share 3 w



Autumn Yeates

We have a neighbor around the corner that has 12 cars out on the street, 5 in his driveway and sometimes 2 on the other side of the street. It is getting very dangerous to pull out of the street. It is a main bus line & I am not sure how it gets down the street. As trashy as it can look I would rather have the cars in there yard.

Like Reply Share 3 w





Elisabeth Morgan

Are you saying that it is better to park in our yards than on the streets?

Like Reply Share 3 w



Natalie R Grahl

What if we eliminate park strips so the gutter goes directly next to the sidewalk? Helps with our water crisis, our water bills, and widens the streets...?

Like Reply Share 3 w



2



David Chidester

Wasn't there a push to have people use their yards for parking instead of the street because of snow? Pick a side. 😊 I prefer my neighbors use their yards instead of taking up the whole street or parking on corners.

Like Reply Share 3 w



2



Brina Villalobos

Let people do what they want on their property.

Like Reply Share 3 w



JoLayne Meadows Vrazel

We have signs All over that says too not park on the streets. And if we park on the streets it's to close for emergency vehicles to get thru.

Like Reply Share 3 w



Alexander Kozik



Like Reply Share 3 w



Amy Westerfield Bingham

Heath ave is the worst for cars like that

Like Reply Share 3 w



Martia Schulze

Amy Westerfield Bingham Charlotte Ave is even worse. I drive there every week day because I work at Western Hills in the kitchen. Always have to pull over to let people pass going the other direction.

Like Reply Share 3 w



Karla Brown

Now you want them on the streets? I live in a cul de sac and that would make getting through impossible. During winter the plows will not do their jobs and actually push the snow because of this nonsense. I have a good 4' in front of my driveway that I have to remove snow that is actually pushed into by the plows. Sheeeeeezzzzz just do what the ordinances require and don't bow to karen and ken.

Like Reply Share 3 w



Hayden Llewellyn

Across the street from where I live, there is a house that has 4 vehicles parked in the yard. All of them are junk and do not run. This doesn't include the camper that is literally falling apart in the yard, or the trailer that is trashed. I think t... See more



Like Reply Share 3 w



Barbara Blackburn

Hayden Llewellyn If they are parked and don't run, there are laws and you can have them removed. Call on them.

Like Reply Share 3 w



Dawn Rowell Jennings

It's safer to park in your yard. we have had two break ins while cars were on the street..right in front of our house. While they are parked in our yard, the camera can get a close up of those that try to rob us.



Like Reply Share 3 w



Write a public comment...





6 July at 07:15 · 🌐

KEARNS RESIDENTS - We noticed a higher percentage of crashes involving a parked car in the study area compared to similar roads around Salt Lake County. Have you experienced this in your neighborhood? Has this changed your parking choices? [Kearns Metro Township](#) Tell us more here: <https://tinyurl.com/KearnsParking>

14%



of crashes within the study area involved a parked car. That's more than average in Salt Lake County!

👍 2

22 comments 2 shares

👍 Like

💬 Comment

➦ Share

All comments ▼



Miguel Ritz

Yall complain when we park in our yards yall complain when we park in the street. Yall should worry about real problems not empty cars.

Like Reply Share 2 w

👍 🤔 6



Miguel Ritz

(Like the rate of speed cars travel in the neighborhood) if you can't dodge a parked car your driving to fast for the neighborhood.

Like Reply Share 2 w Edited

👍

 Michelle Farey Lloyd

Miguel Ritz totally agree. Leave us and our property alone.

Like Reply Share 2 w

 Jason Santistevan

Yeah like all the crime here in kearns. All the auto break ins.

Like Reply Share 2 w

 Michelle Farey Lloyd

That's because your stupid tickets for residents (from code enforcement) that have four drivers so the four cars parked on the driveway hang 2" over the sidewalk. They're making us park in the streets now. Our first night on the road my sons car got hi... See more

Like Reply Share 2 w Edited




 Jenny Suppinger

It just means lots of people living in the house, due to rent being so high. They really don't have a choice


Like Reply Share 2 w



 Michelle Farey Lloyd

Jenny Suppinger we just had 5 kids.

Like Reply Share 2 w

 Jenny Suppinger


Michelle Farey Lloyd congratulations!!!

Like Reply Share 2 w

 Michelle Farey Lloyd

Jenny Suppinger thank you! All the ones that are left at home drive to jobs so they need vehicles.

Like Reply Share 2 w

 Jenny Suppinger

Michelle Farey Lloyd very true

Like Reply Share 2 w

 Janet Davis Brown

I am pretty sure the parked car didn't jump out and cause the accident. I mean I guess I could be wrong.

Like Reply Share 2 w




 Michelle Farey Lloyd

Janet Davis Brown our neighbor backed into it and has hit 7 vehicles in total in the last 15 years hence the reason we've moved them to the driveway.

Like Reply Share 2 w



 Jenny Suppinger

Michelle Farey Lloyd sounds like your neighbor needs his eyes checked, that's so many cars

Like Reply Share 2 w



 Michelle Farey Lloyd

Jenny Suppinger I know we've lived here for 40 years and the last 15 she's been here have been interesting to say the least. Our situation with the parking is very unique and frustrating.

Like Reply Share 2 w



Jenny Suppinger

Michelle Farey Lloyd that's gotta be frustrating

Like Reply Share 2 w



2



Janet Nerdin

Unfortunately, the streets in many of the residential neighborhoods in Kearns are not wide enough to allow for two-way traffic when cars are parked on both sides of the road. This increases the risk of parked cars getting hit. My house is located near... See more

Like Reply Share 2 w



2



Michelle Farey Lloyd

Janet Nerdin we have the same problem.

Like Reply Share 2 w



Hilary Anne

Janet Nerdin I hate that one of our neighbors has about 12 cars (not exaggerating) and so many of them are parked on the street because it does make the road super narrow. They live on a corner so it's hard to see around the cars when trying to turn onto the main road, and it creates a hazard of crashing into someone when turning onto the street because if anybody parks on the other side, only 1 car can drive down the road between them. If the road weren't so narrow and didn't create such a hazard, I wouldn't mind as much.

Like Reply Share 2 w



Brooke Kebschull

Yeah I had my car parked on the side of the road by my house. Not sticking out or anything, but the driver was high on pills and not paying attention. And totalled my car.

Like Reply Share 2 w Edited



Lynda Birch Jeffs

You have rent high, gas ungodly high, no food... And yet you wonder why all the houses have tons of cars? HELLO... MCFLY.... COMMON SENSE, or just big daddy government wanting more, and more, and more?

Like Reply Share 2 w



Sherry Talbert

Our streets are so narrow that the cars who park on both sides of the street makes it impossible to get through when another car is coming. You have to pull over and let one or the other get by. It was not like ten years ago or even less.

Like Reply Share 2 w



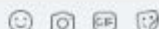
Ryan Rawlings

K, but do we have a problem with collisions involving parked cars in absolute terms, or do we just have fewer total accidents?

Like Reply Share 2 w



Write a public comment...



KEARNS RESIDENTS - The study area experienced a higher percentage of crashes involving an intersection compared to similar roads around Salt Lake County. Have you experienced a crash in a Kearns intersection? Has this changed your parking choices? [Kearns Metro Township](#) Tell us more here: <https://tinyurl.com/KearnsParking>

Nearly

60%



*of all vehicle
crashes in the study area
happened at an intersection.*



3

6 comments



Like



Comment



Share

All comments ▼



Miguel Ritz

What does intersections and parking have anything to do with each other? I bet you get payed just to post. Yall are too much. At least make sense. Yall don't care bout nothing but a pay check and it shows

Like Reply Share 2 w

 Elizabeth Thomas

Miguel Ritz People park near intersections and it makes it so drivers can't see the cross traffic. That is when crashes happen. They aren't talking about intersections with traffic lights.

Like Reply Share 2 w



Miguel Ritz

Elizabeth Thomas so is that why they are building those weird curbs all around? They need to address the actual issue (the speeding cars). And they have never enforced the 25' from a corner law anyways.

Like Reply Share 1 w



Elizabeth Thomas

Yep, that's why.

Like Reply Share 1 w



Devin LaReaux

The intersections of

5400 South and 5600 West

4700 South and 5600 West

Made my car insurance jump \$40

Just moving out here.

It's ridiculous how little people pay attention, run the light and are on their phones. Question for all who read this or want to get but hurt over anything... HOW MUCH VALUE IS THERE ON YOUR LIFE?? It's not a question of who's more valuable either. You matter as much as anyone else does to their family as to yours, if even just 1 person. So slow down and take your value serious.

Like Reply Share 2 w



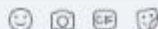
Patrick Roe

Y'all gotta do something about the intersection of 54th south and 56th west by the maverick and holiday oil. Hell 56th in general. People treat it like a freeway. I have seen people going atleast 65 on that street. Even with stop lights people still gu...
[See more](#)

Like Reply Share 1 w



Write a public comment...





20 June · 🌐

Kearns Metro Township and the Greater Salt Lake Municipal Services District have conducted an On-Street Parking Study to evaluate parking availability, demand for parking, and parking habits.

Our study chose two areas in Kearns that represented the diverse homes and neighborhoods in Metro Township. We studied parking habits on one weekday and one weekend day in the morning, afternoon, evening, and nighttime. Our team used these findings to draft some potential recommended ch... [See more](#)

 on a given day.

25% - 35%

*of vehicles
using on-street
parking had
parked on the
sidewalk.*



*During our study,
we noticed over*

2,000

*vehicles parking
in their yards.*

14%

*of crashes within the
study area involved a
parked car. That's more
than average in Salt*



KEARNS RESIDENTS - Many residents in the study area park on the sidewalk. Do you park on the sidewalk? Have you noticed cars in your neighborhood parking on the sidewalk? [Kearns Metro Township](#) Tell us more here: <https://tinyurl.com/KearnsParking>

25% - 35%

*of vehicles
using on-street
parking had
parked on the
sidewalk.*



6

4 comments



30 June at 07:08 · 🌐

KEARNS RESIDENTS - While collecting data during the various days and time, our team noticed over 2,000 vehicles parked in yards. On average, only 19%-28% of available on-street parking was used. If the vehicles parked in yards were relocated to the street, we'd see about 40% of available on-street parking used. Have you noticed this trend in your neighborhood? [Kearns Metro Township](#) Tell us more here: <https://tinyurl.com/KearnsParking>



*During our study,
we noticed over*

2,000

*vehicles parking
in their yards.*

👍👎👏 15

50 comments 4 shares

👍 Like

💬 Comment

➦ Share



KEARNS RESIDENTS - We noticed a higher percentage of crashes involving a parked car in the study area compared to similar roads around Salt Lake County. Have you experienced this in your neighborhood? Has this changed your parking choices? [Kearns Metro Township](#) Tell us more here: <https://tinyurl.com/KearnsParking>

14%



of crashes within the study area involved a parked car. That's more than average in Salt Lake County!



2

22 comments 2 shares



Like



Comment



Share



Greater Salt Lake Municipal Services District » K-TOWN (Kearns Township)

11 July at 07:08 · 🌐

...

KEARNS RESIDENTS - The study area experienced a higher percentage of crashes involving an intersection compared to similar roads around Salt Lake County. Have you experienced a crash in a Kearns intersection? Has this changed your parking choices? [Kearns Metro Township](#) Tell us more here: <https://tinyurl.com/KearnsParking>

Nearly

60%



*of all vehicle
crashes in the study area
happened at an intersection.*



3

6 comments

👍 Like

💬 Comment

➦ Share



Kearns Metro Township and the Greater Salt Lake Municipal Services District have conducted an On-Street Parking Study to evaluate parking availability, demand for parking, and parking habits.

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2,000

*vehicles parking
in their yards.*

14%

*of crashes within the
study area involved a
parked car. That's more
than average in Salt*



3

1 comment



Like



Comment



Share



Angie Jane

So... watcha wanna bet that the recommended changes will be fines and harassment etc for not following the kings orders.

Like Reply · 4



Write a comment...





KEARNS RESIDENTS - We've found that most of the available on-street parking in our study area was not used. Have you noticed this in your neighborhood? Do you park on the street? [Kearns Metro Township](https://tinyurl.com/KearnsParking) Tell us more here: <https://tinyurl.com/KearnsParking>



1 share

👍 Like

💬 Comment

➦ Share



Write a comment...





Greater Salt Lake Municipal Services District

27 June at 07:00



KEARNS RESIDENTS - Many residents in the study area park on the sidewalk. Do you park on the sidewalk? Have you noticed cars in your neighborhood parking on the sidewalk? [Kearns Metro Township](#) Tell us more here: <https://biturl.com/KearnsParking>

25% - 35%

*of vehicles
using on-street
parking had
parked on the
sidewalk.*



1 share

Like

Comment

Share



Write a comment...





KEARNS RESIDENTS - While collecting data during the various days and time, our team noticed over 2,000 vehicles parked in yards. On average, only 19%-28% of available on-street parking was used. If the vehicles parked in yards were relocated to the street, we'd see about 40% of available on-street parking used. Have you noticed this trend in your neighborhood? [Kearns Metro Township](#) Tell us more here: <https://tinyurl.com/KearnsParking>



*During our study,
we noticed over*

2,000

*vehicles parking
in their yards.*



1



Like



Comment



Share



Write a comment...





KEARNS RESIDENTS - We noticed a higher percentage of crashes involving a parked car in the study area compared to similar roads around Salt Lake County. Have you experienced this in your neighborhood? Has this changed your parking choices? [Kearns Metro Township](https://tinyurl.com/KearnsParking) Tell us more here: <https://tinyurl.com/KearnsParking>

14%



of crashes within the study area involved a parked car. That's more than average in Salt Lake County!



Like



Comment



Share



Write a comment...





KEARNS RESIDENTS - The study area experienced a higher percentage of crashes involving an intersection compared to similar roads around Salt Lake County. Have you experienced a crash in a Kearns intersection? Has this changed your parking choices? [Kearns Metro Township](https://tinyurl.com/KearnsParking) Tell us more here: <https://tinyurl.com/KearnsParking>

Nearly

60%



*of all vehicle
crashes in the study area
happened at an intersection.*

1 comment 2 shares



Like



Comment



Share



Miguel Ritz

Why would an intersection affect my parking quit playing and start addressing the speeding and lack of police. Not to mention the response time. I don't even call the police no more I just handle it my self.

Like Reply 1 in



Write a comment...



Kearns On-Street Parking Study

Kearns Metro Township and the Salt Lake Municipal Services District have conducted an On-Street Parking Study to evaluate parking availability, demand for parking, and parking habits.

Our study chose two areas in Kearns that represented the diverse homes and neighborhoods in Metro Township. We studied parking habits on one weekday and one weekend day in the morning, afternoon, evening, and nighttime. Our team used these findings to draft some potential recommended changes that we are aiming to implement by 2025.

Over the next two weeks, we will be sharing some key findings from the study with you. Follow along and let us know what you think of our results and recommendations by taking our survey. Our complete report will be available later this summer.

Tell us more about your experience parking in Kearns Metro Township here:

<https://tinyurl.com/KearnsParking>



ON-STREET PARKING STUDY

Under



30%

of on-street parking is used on a given day.

25% - 35%

of vehicles using on-street parking had parked on the sidewalk.



During our study, we noticed over

2,000

vehicles parking in their yards.

14%

of crashes within the study area involved a parked car. That's more than average in Salt Lake County!



Nearly

60%

of all vehicle crashes in the study area happened at an intersection.



KEARNS ON-STREET PARKING STUDY

What is a study area?

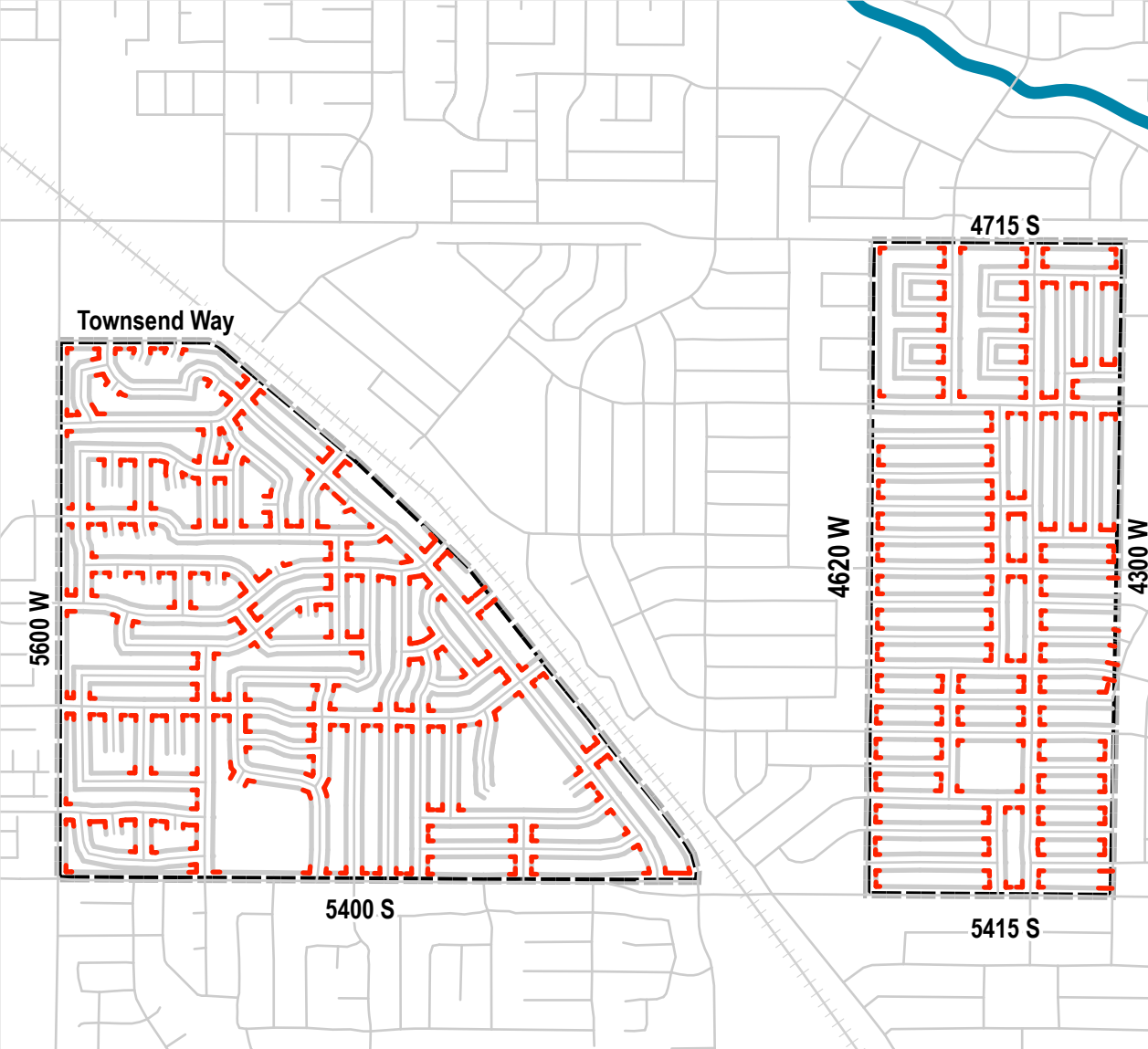
Our team chose two residential areas within the Kearns Metro Township areas for the Kearns On-Street Parking Study. These two areas were chosen because they represent the diverse mix of house styles, street widths and neighborhood patterns found in residential areas all over Kearns Metro Township. By selecting these two areas, our team is able to learn more about on-street parking trends and habits in Kearns.



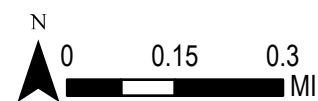


APPENDIX C

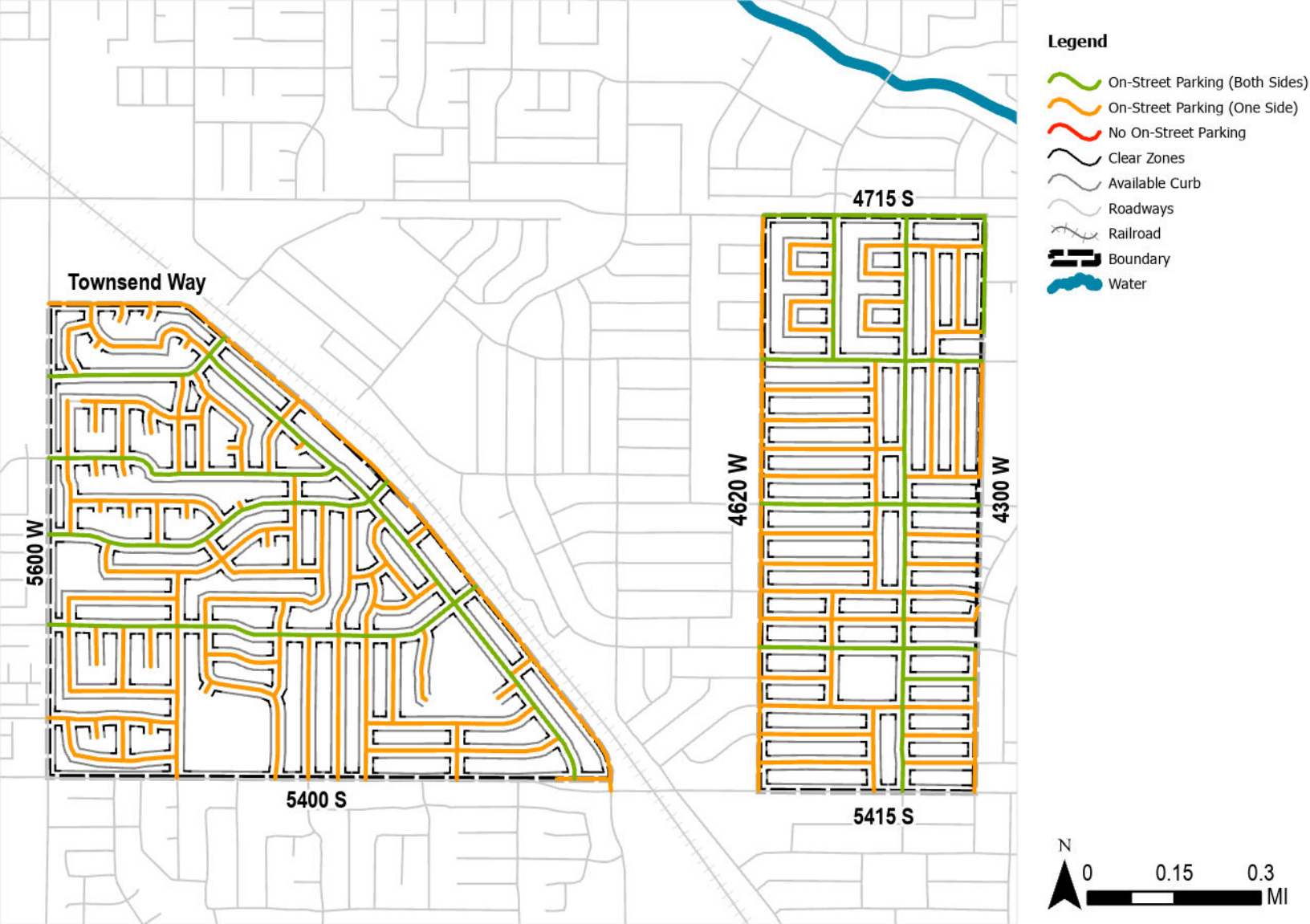
Updated Parking Occupancy Figures with Parking Restrictions



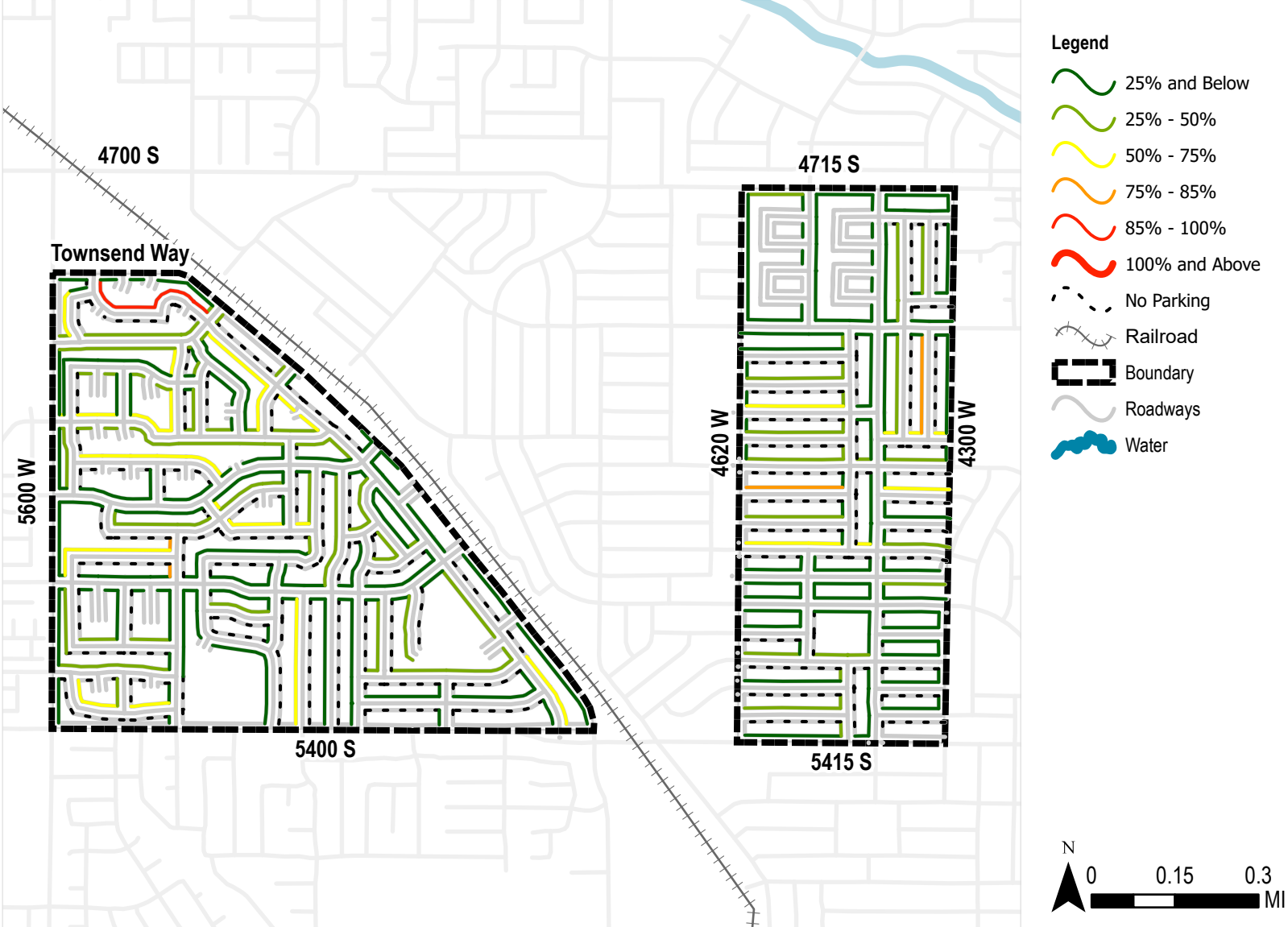
- Legend**
- Available Curb
 - No Parking
 - Railroad
 - Boundary
 - Roadways
 - Water



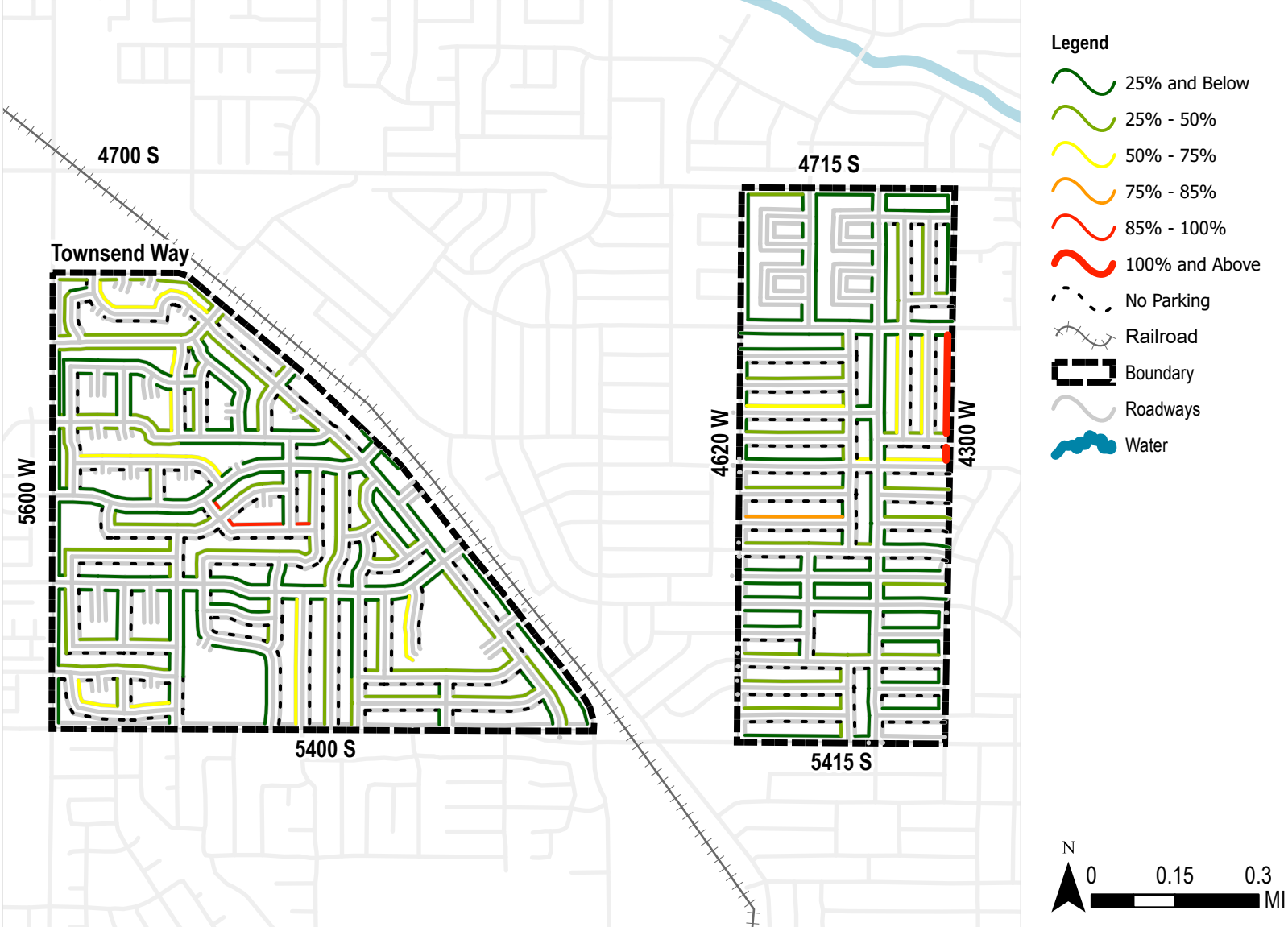
30' x 30' Clear Zones

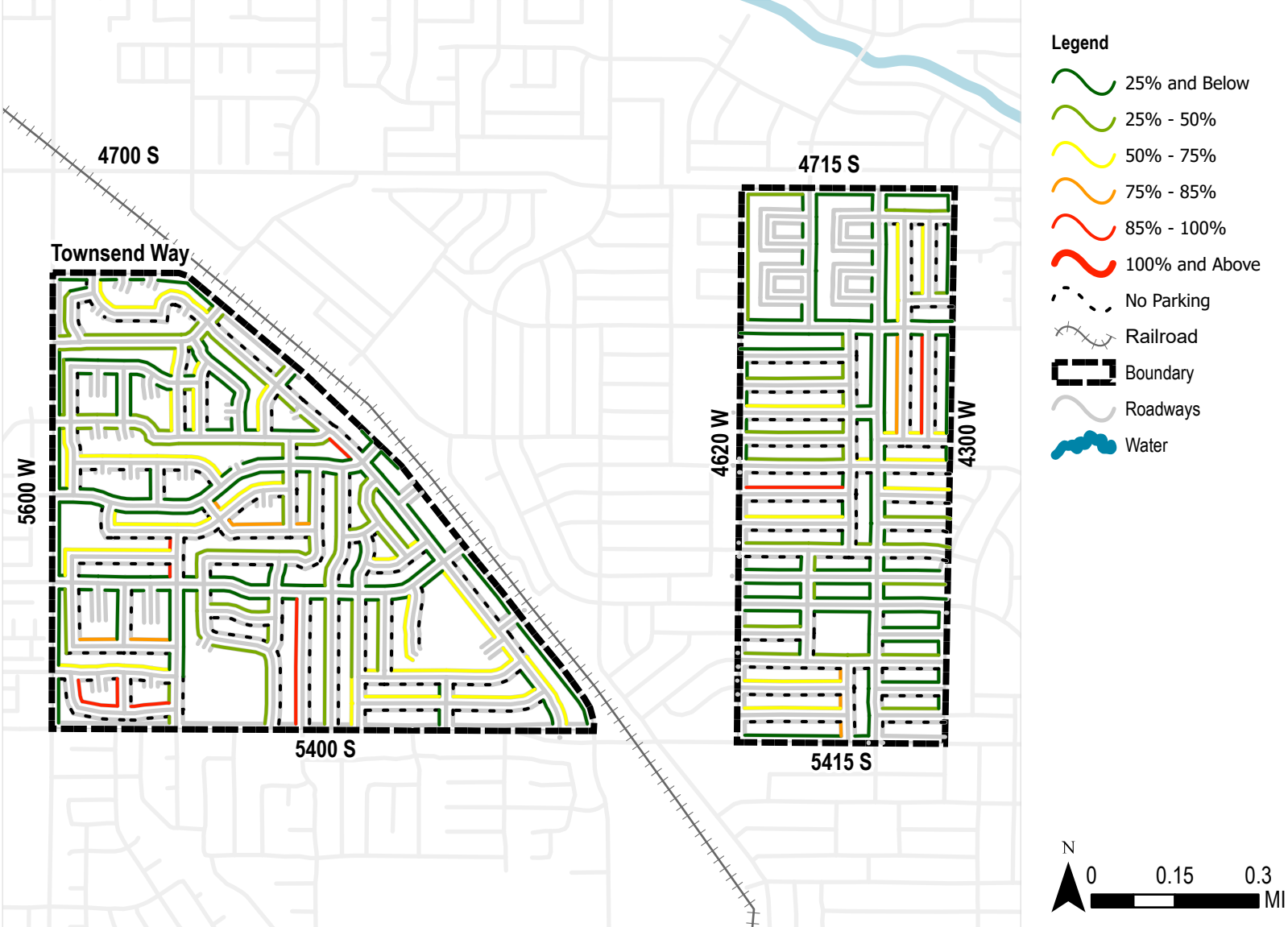


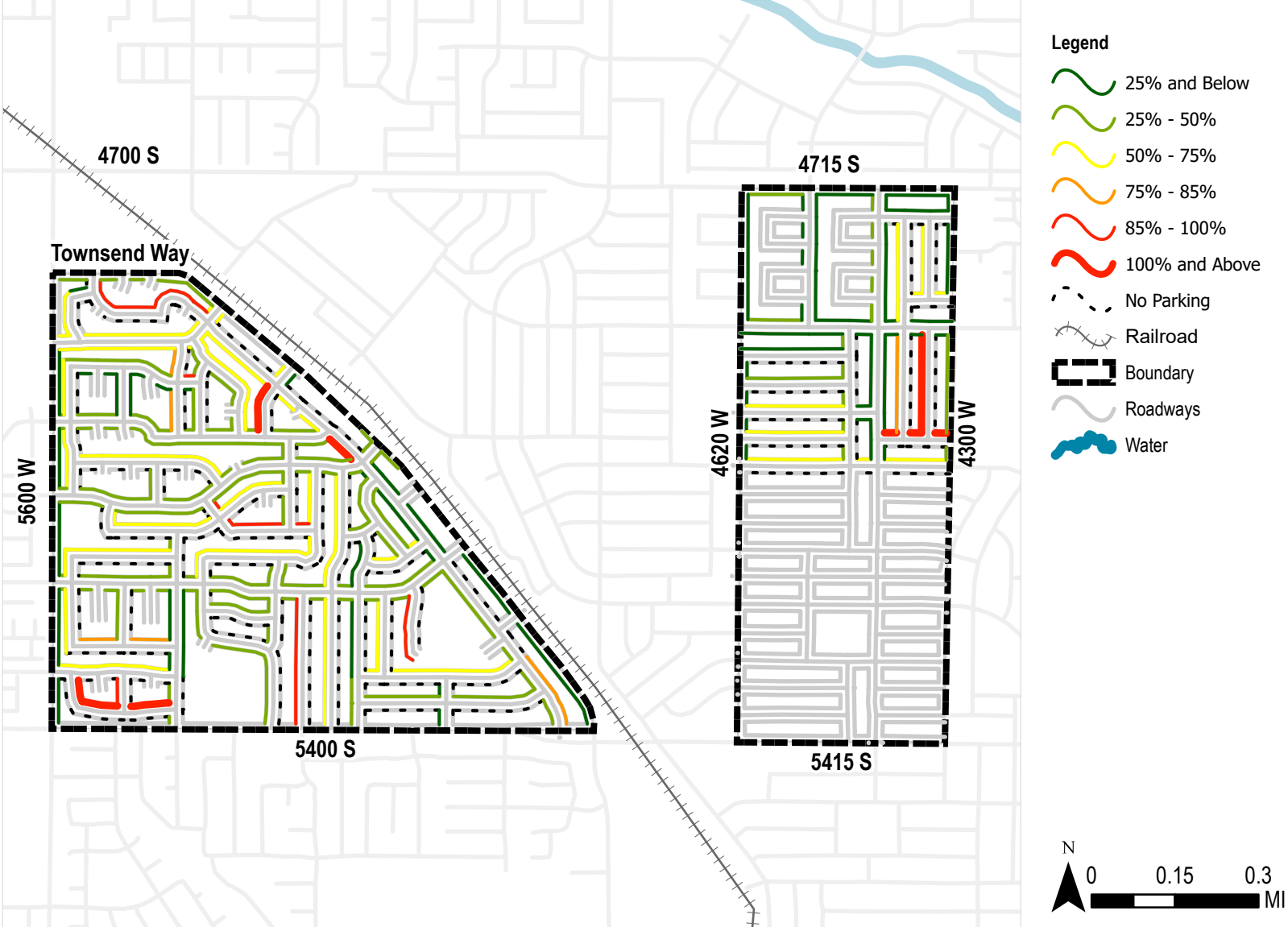
Parking by Roadway Widths



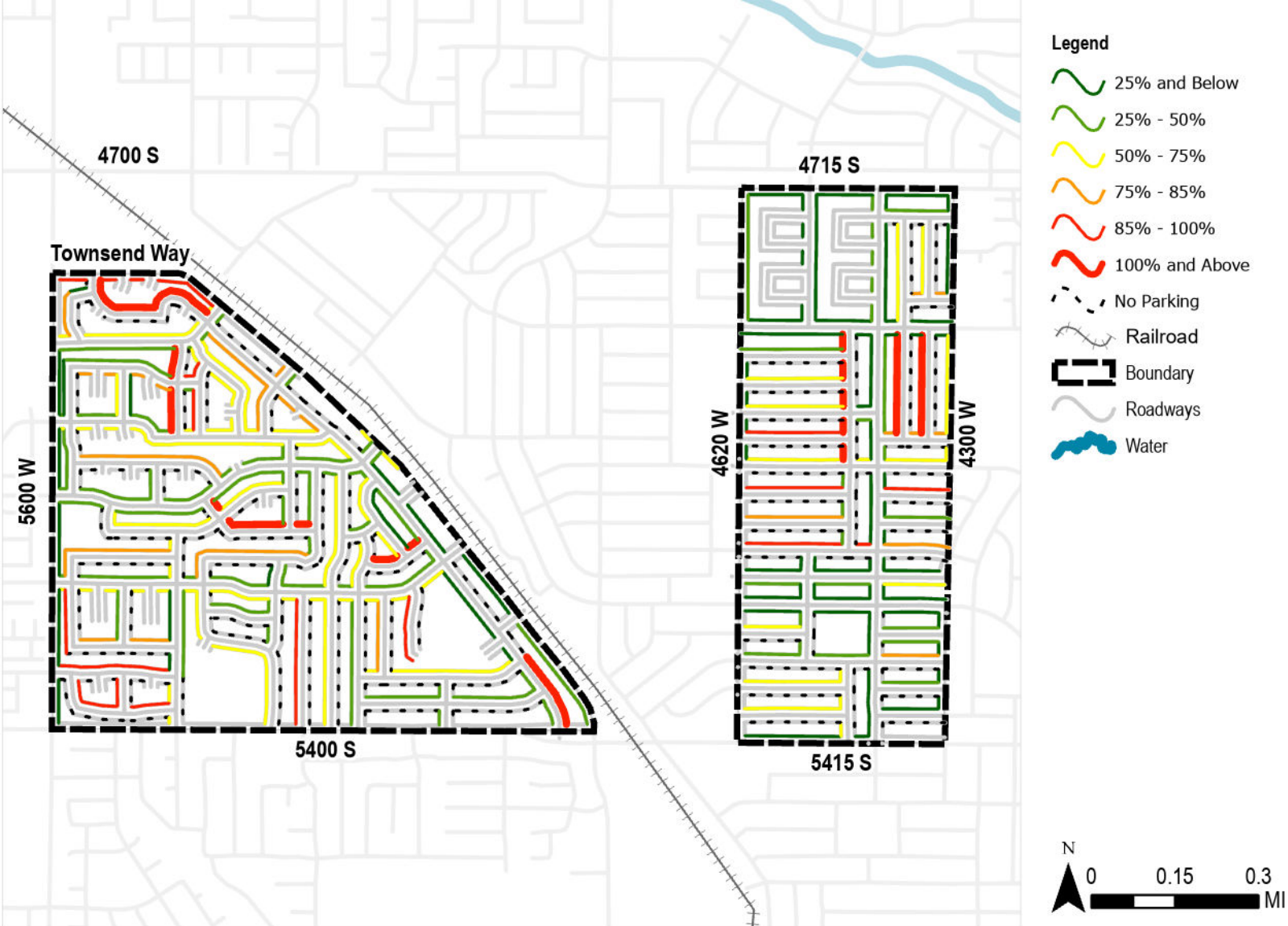
Weekday 7:00 AM - 9:00 AM Occupancy



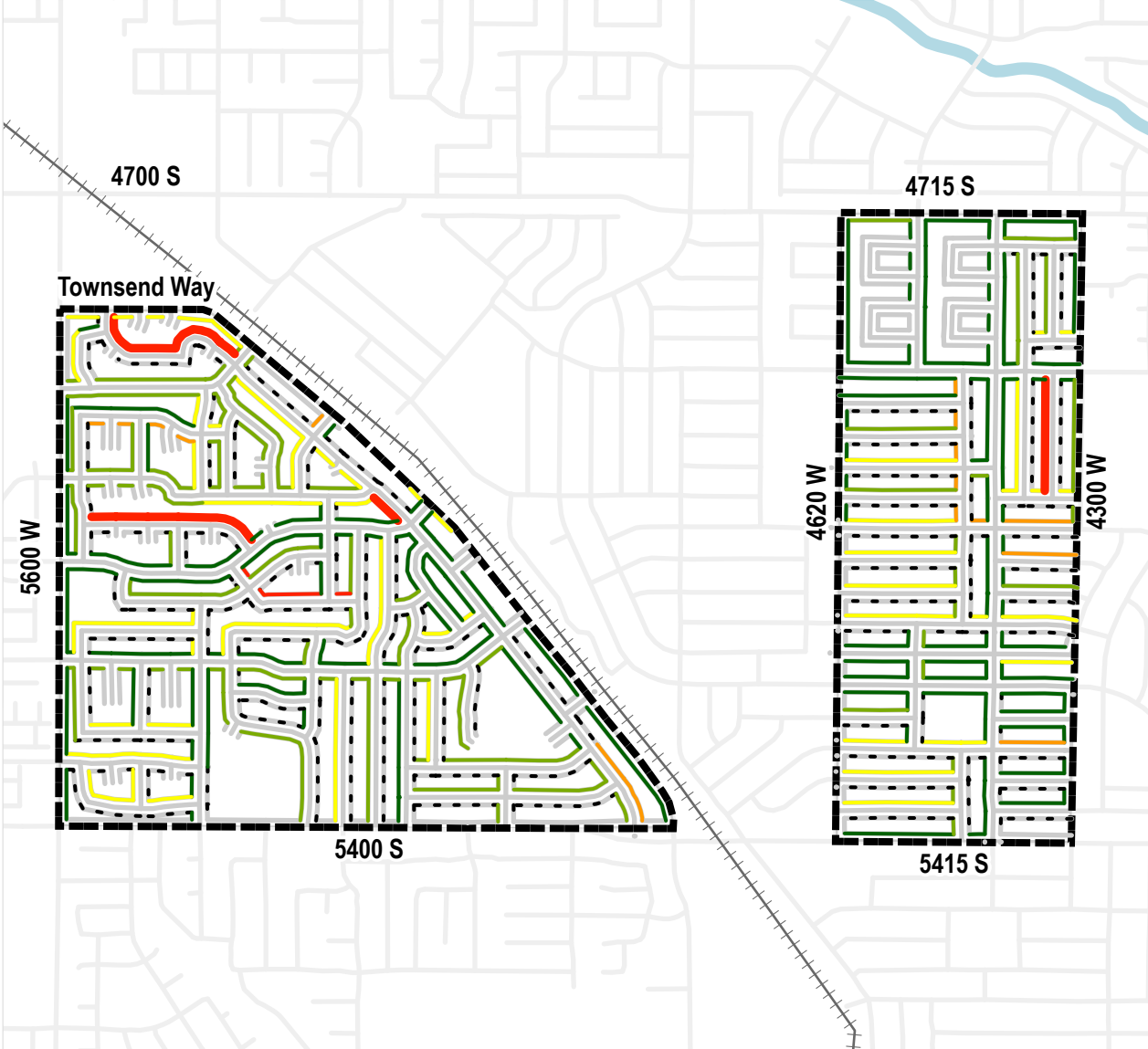




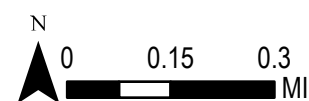
Weekday 8:00 PM - 10:00 PM Occupancy



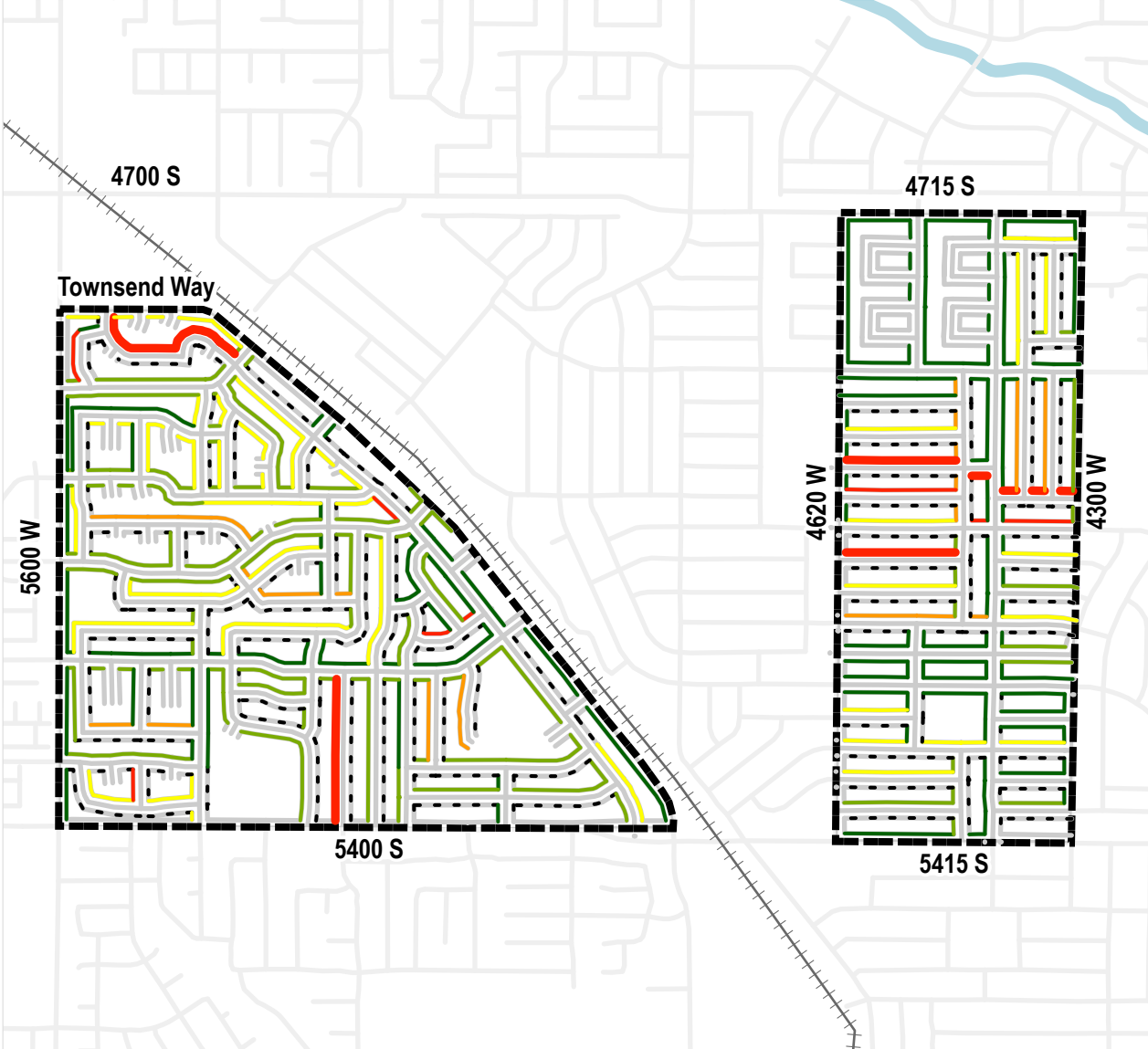
Weekend 8:00 AM - 10:00 AM



- Legend**
- 25% and Below
 - 25% - 50%
 - 50% - 75%
 - 75% - 85%
 - 85% - 100%
 - 100% and Above
 - No Parking
 - Railroad
 - Boundary
 - Roadways
 - Water

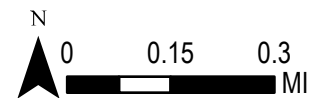


Weekend 12:00 PM - 2:00 PM

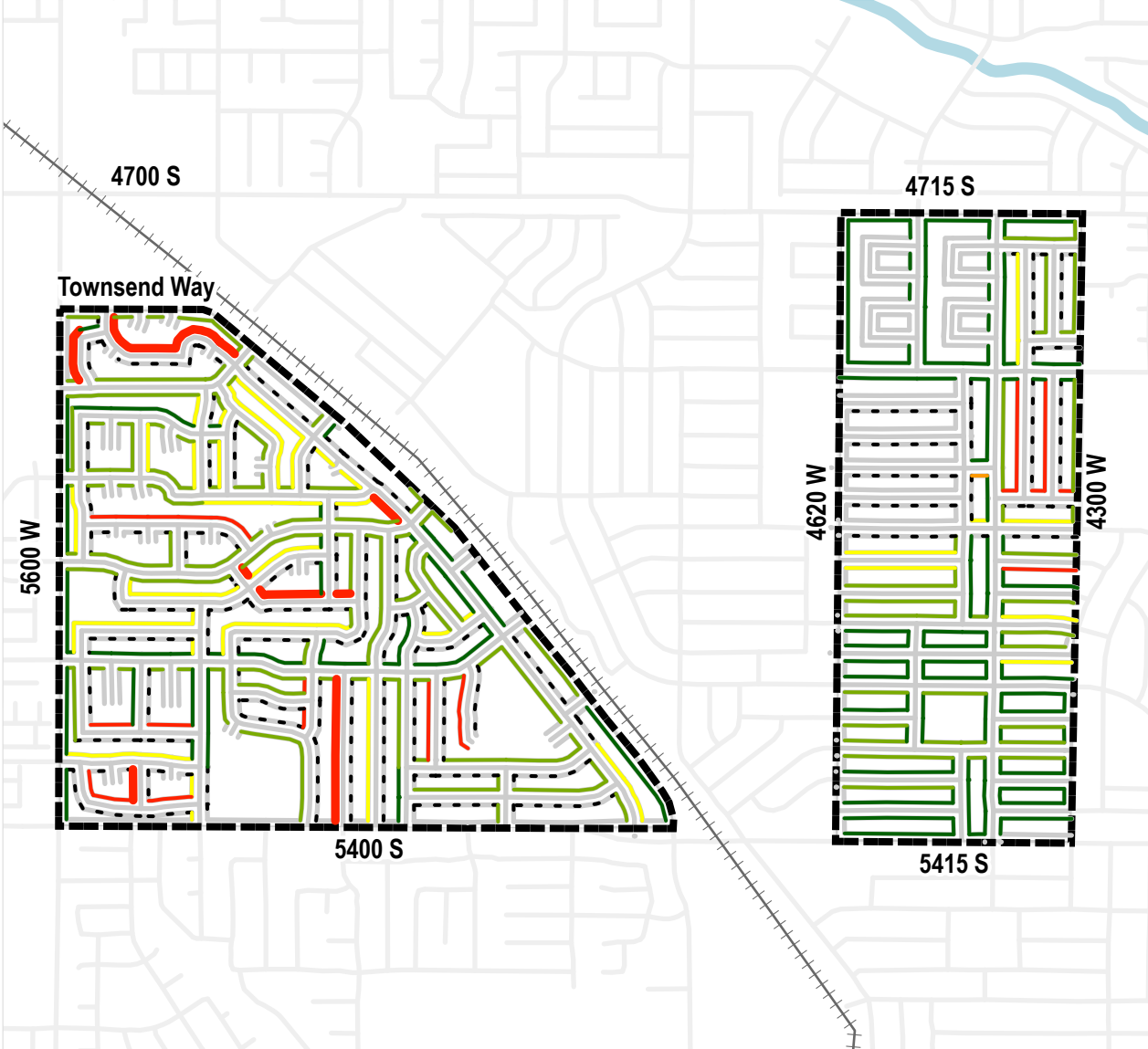


Legend

- 25% and Below
- 25% - 50%
- 50% - 75%
- 75% - 85%
- 85% - 100%
- 10% and Above
- No Parking
- Railroad
- Boundary
- Roadways
- Water

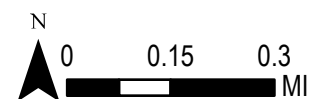


Weekend 5:00 PM - 7:00 PM



Legend

- 25% and Below
- 25% - 50%
- 50% - 75%
- 75% - 85%
- 85% - 100%
- 100% and Above
- No Parking
- Railroad
- Boundary
- Roadways
- Water

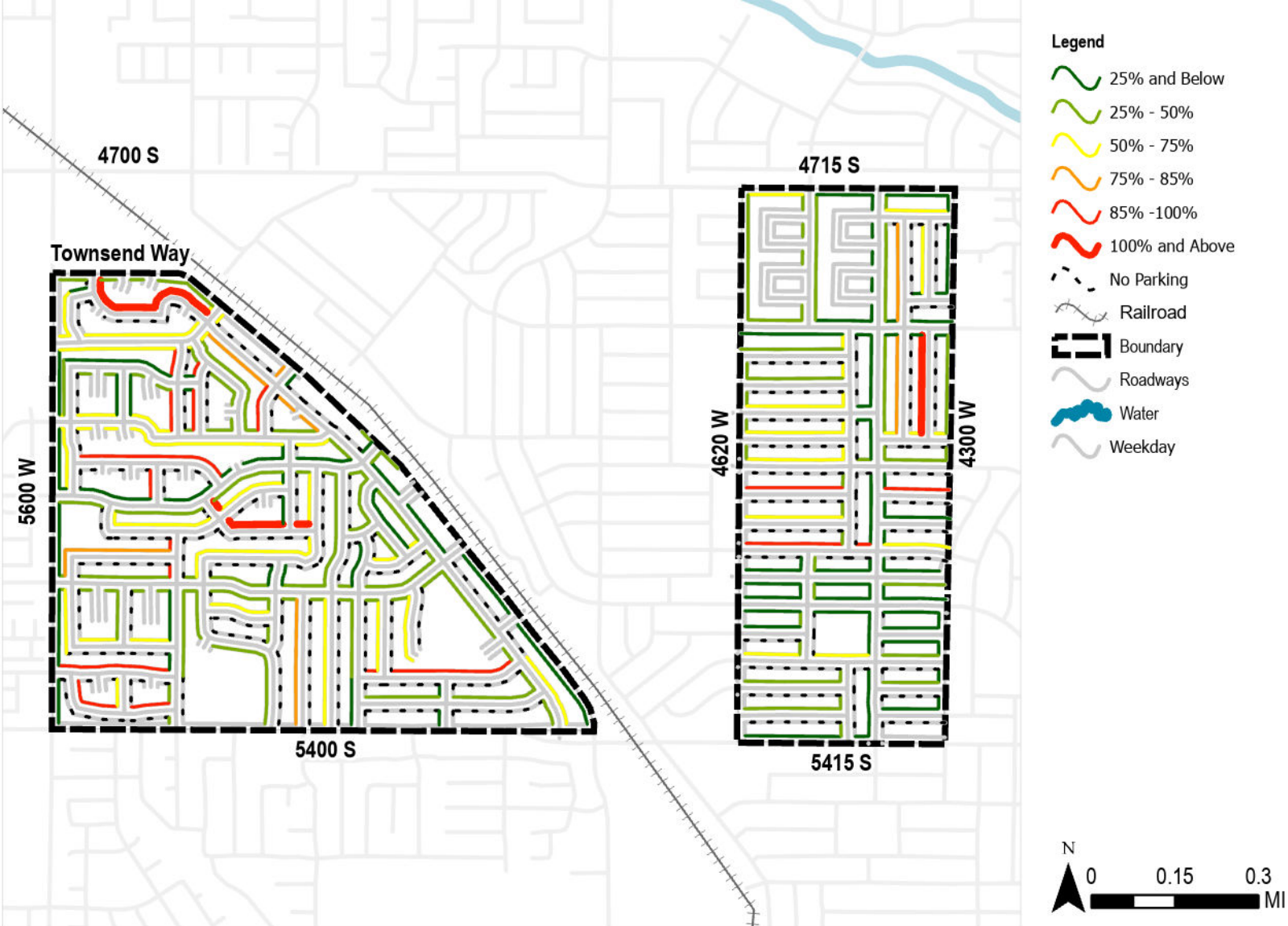


Weekend 9:00 PM - 11:00 PM

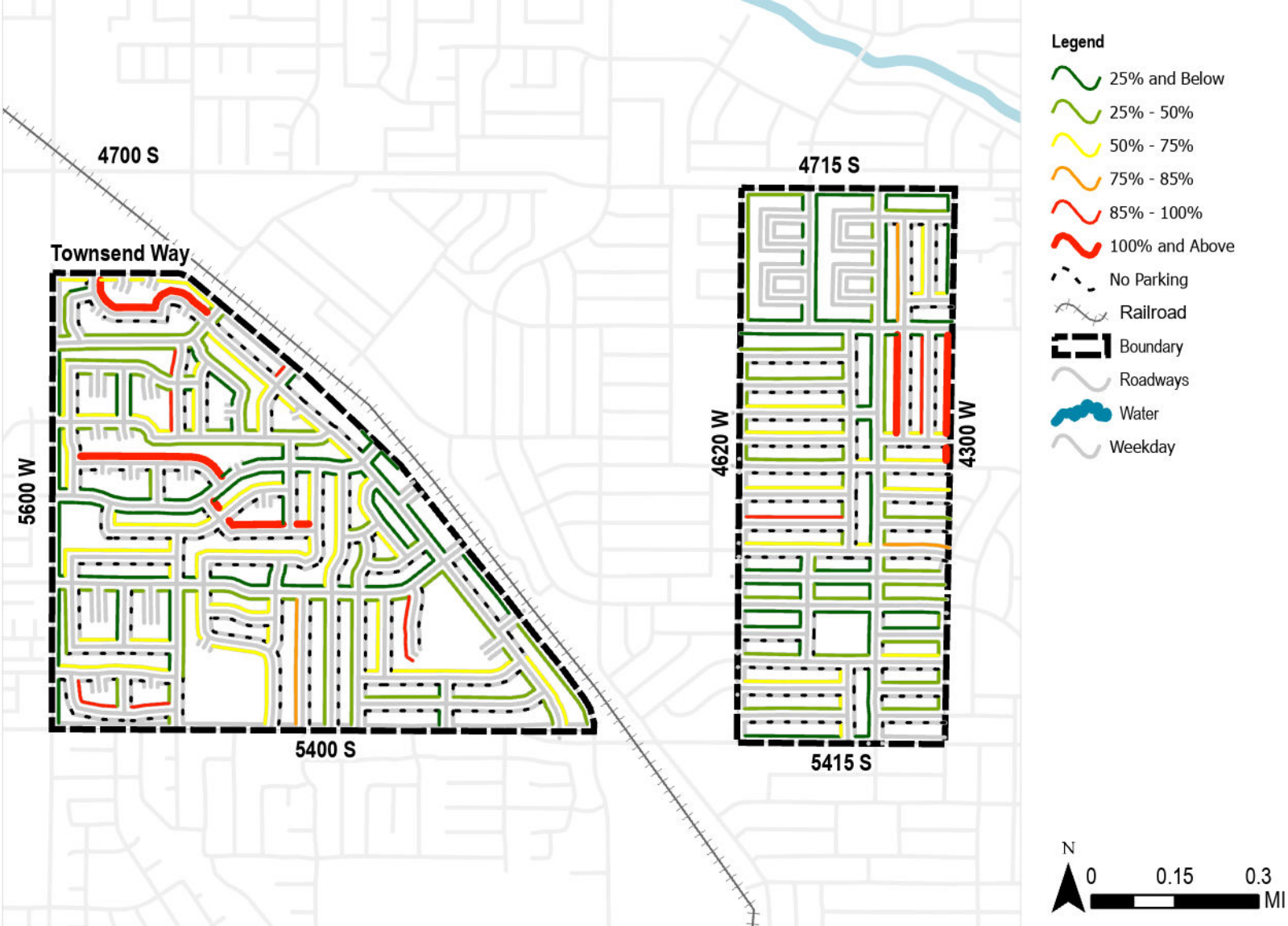


APPENDIX D

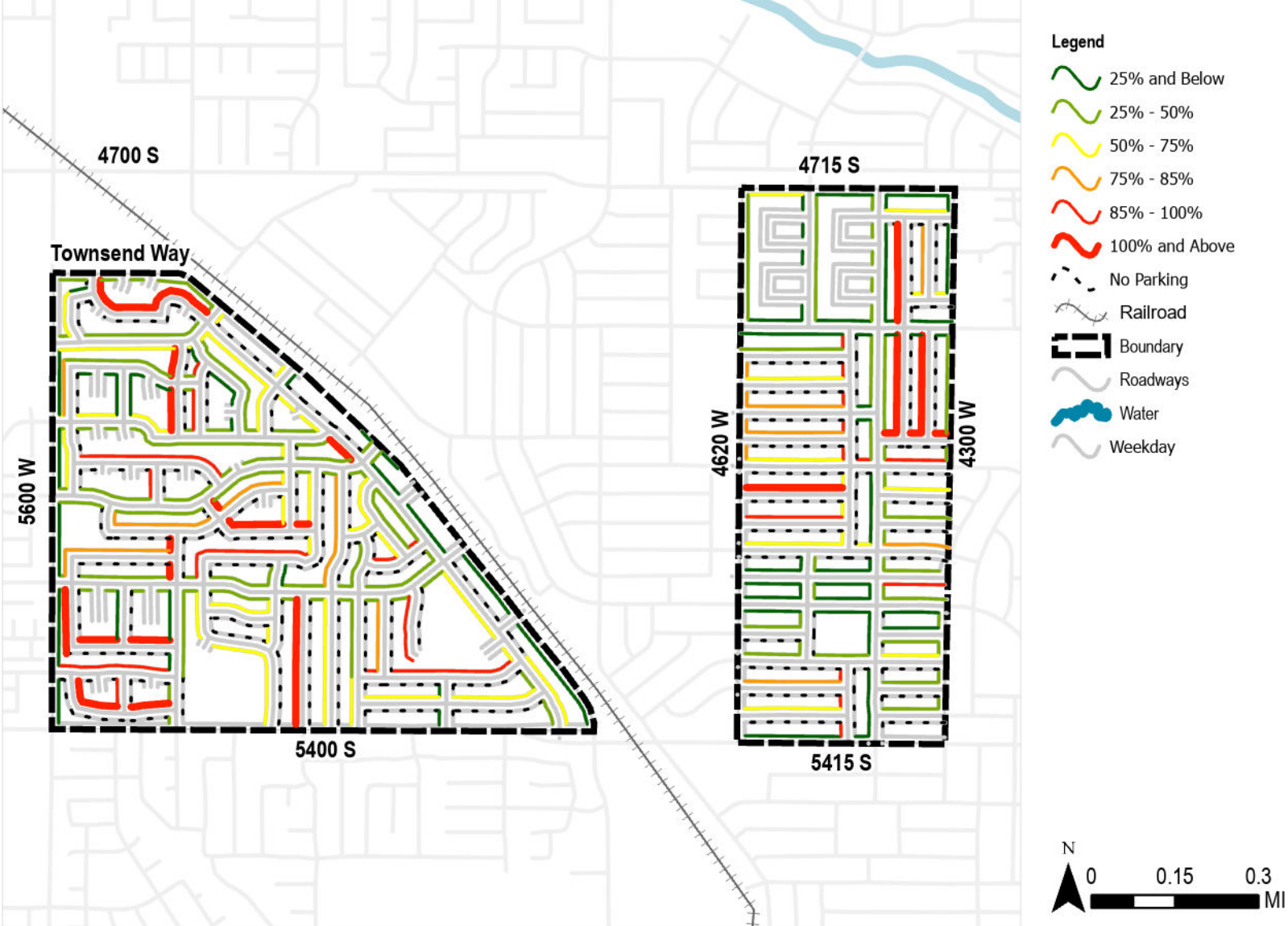
Updated Parking Occupancy Figures with Restrictions and Yard Parking



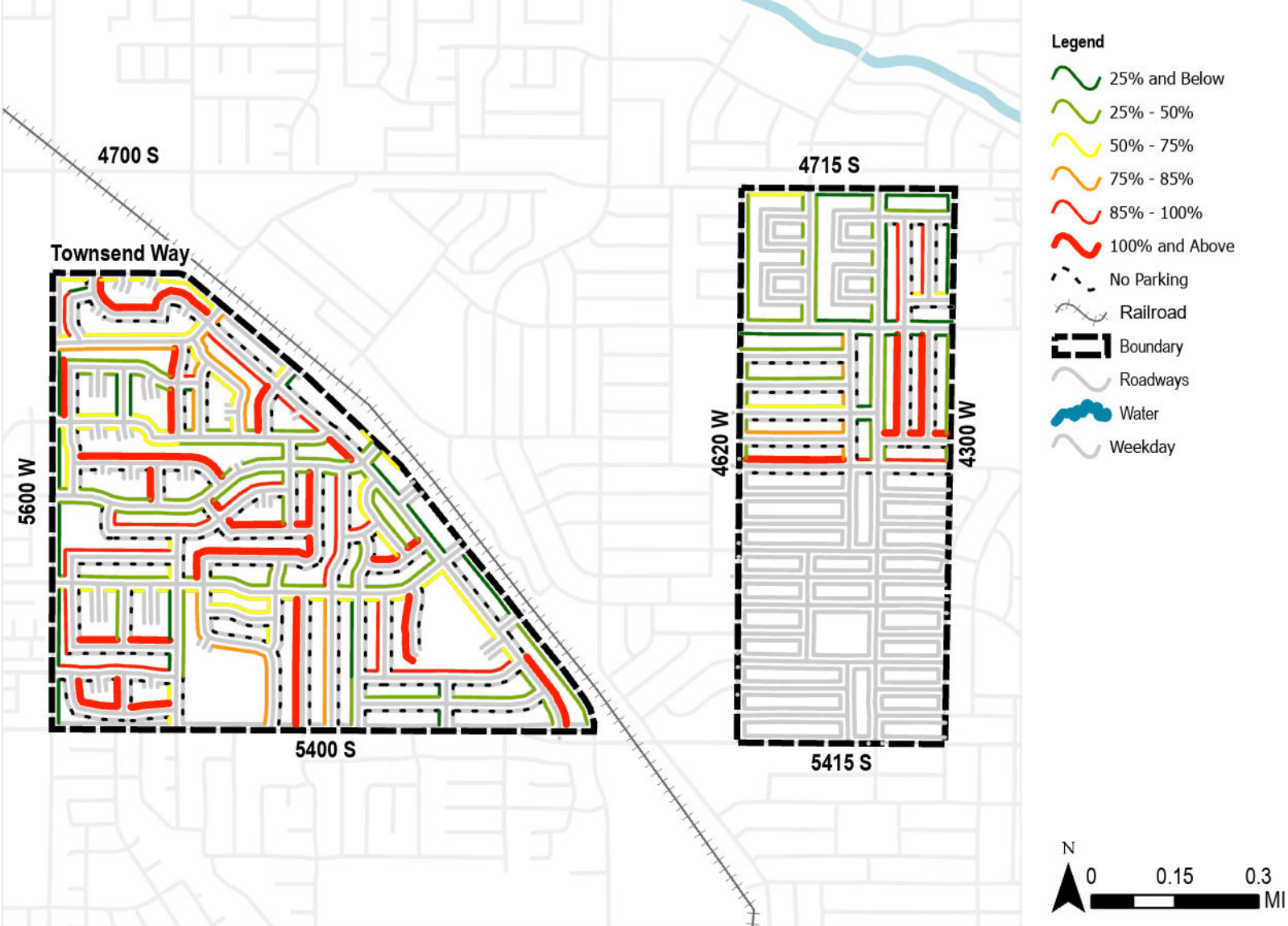
Weekday 7:00 AM - 9:00 AM Occupancy



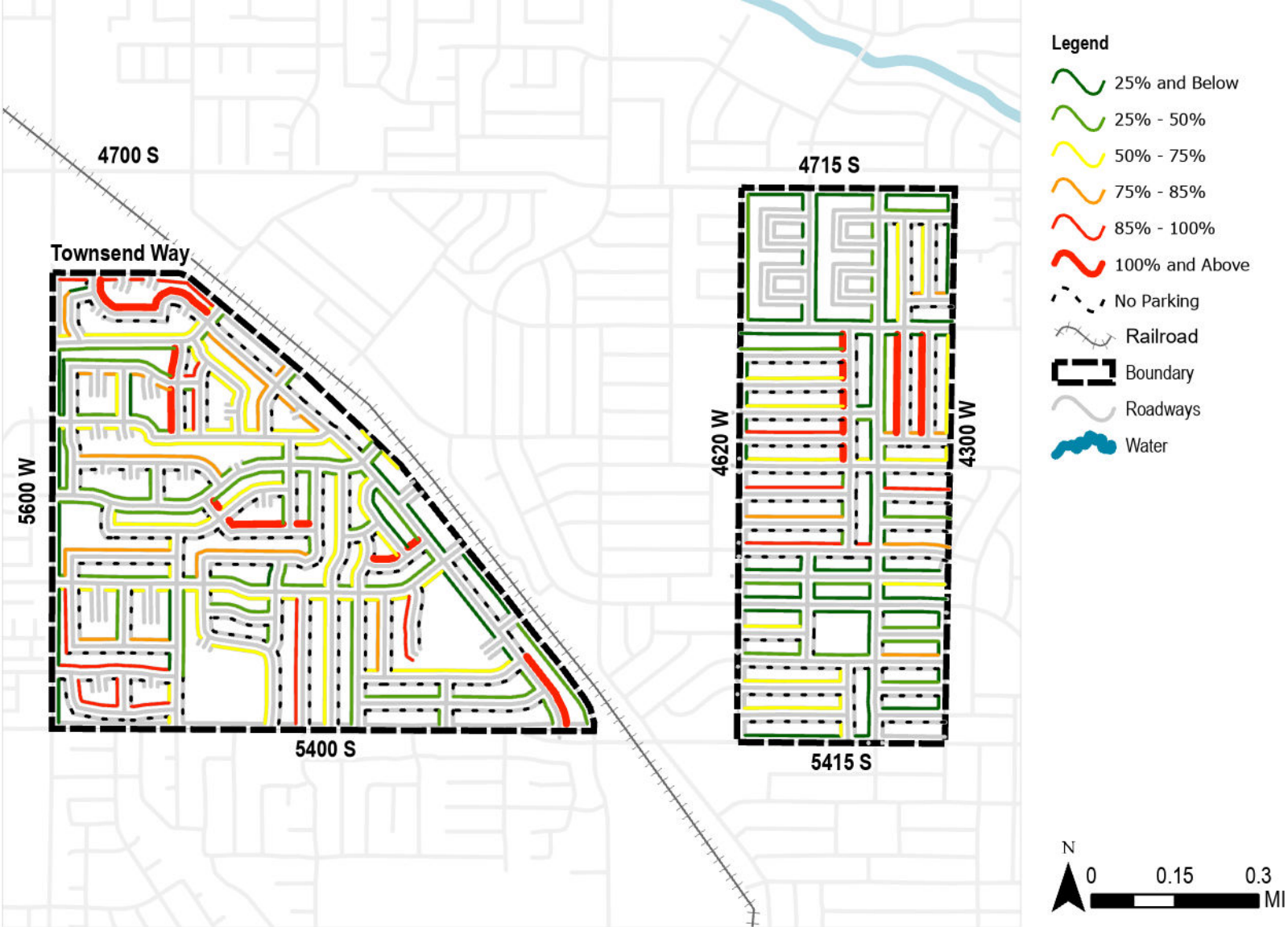
Weekday 2:00 PM - 4:00 PM Occupancy



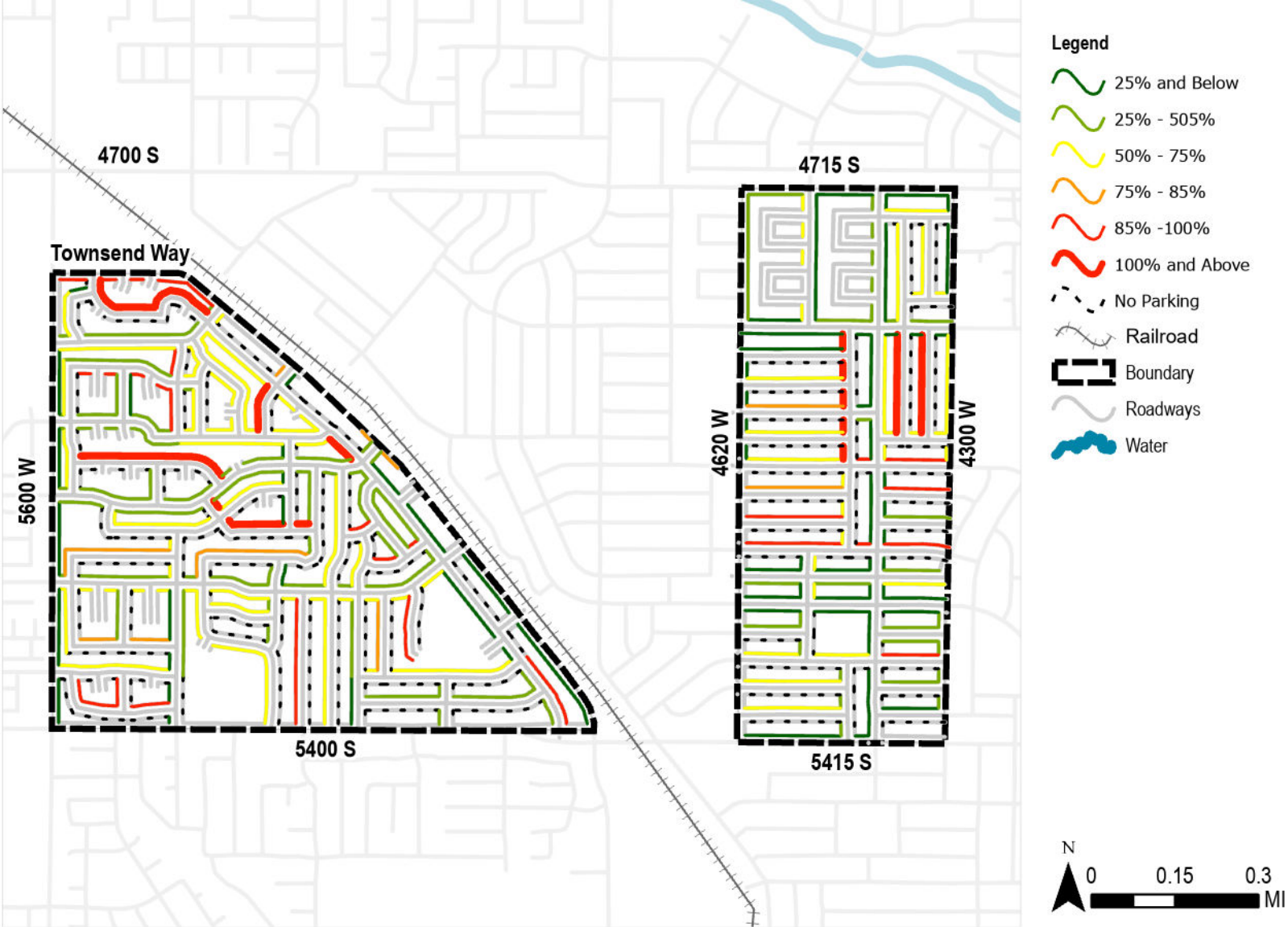
Weekday 5:00 PM - 7:00 PM Occupancy



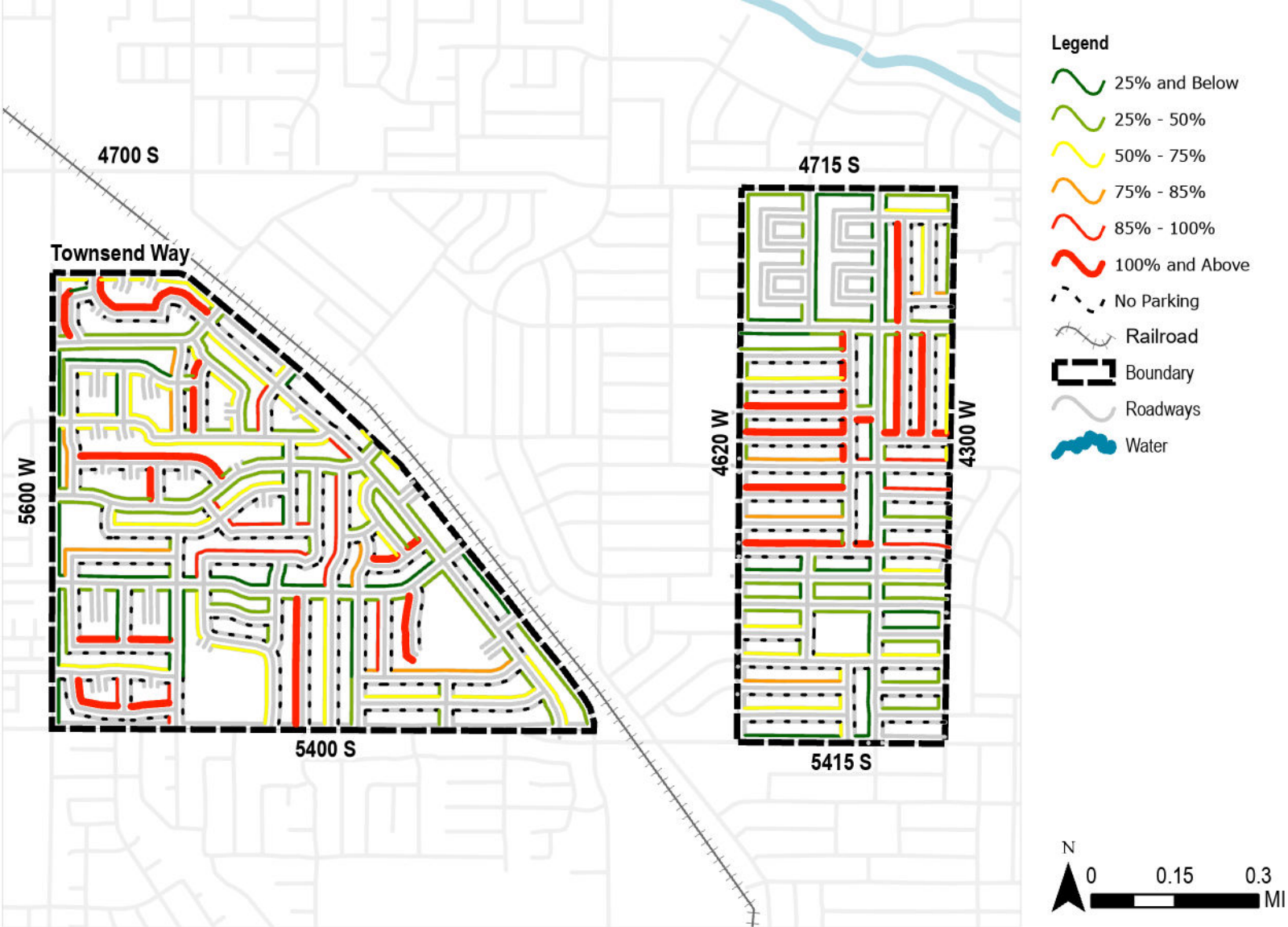
Weekday 8:00 PM - 10:00 PM Occupancy



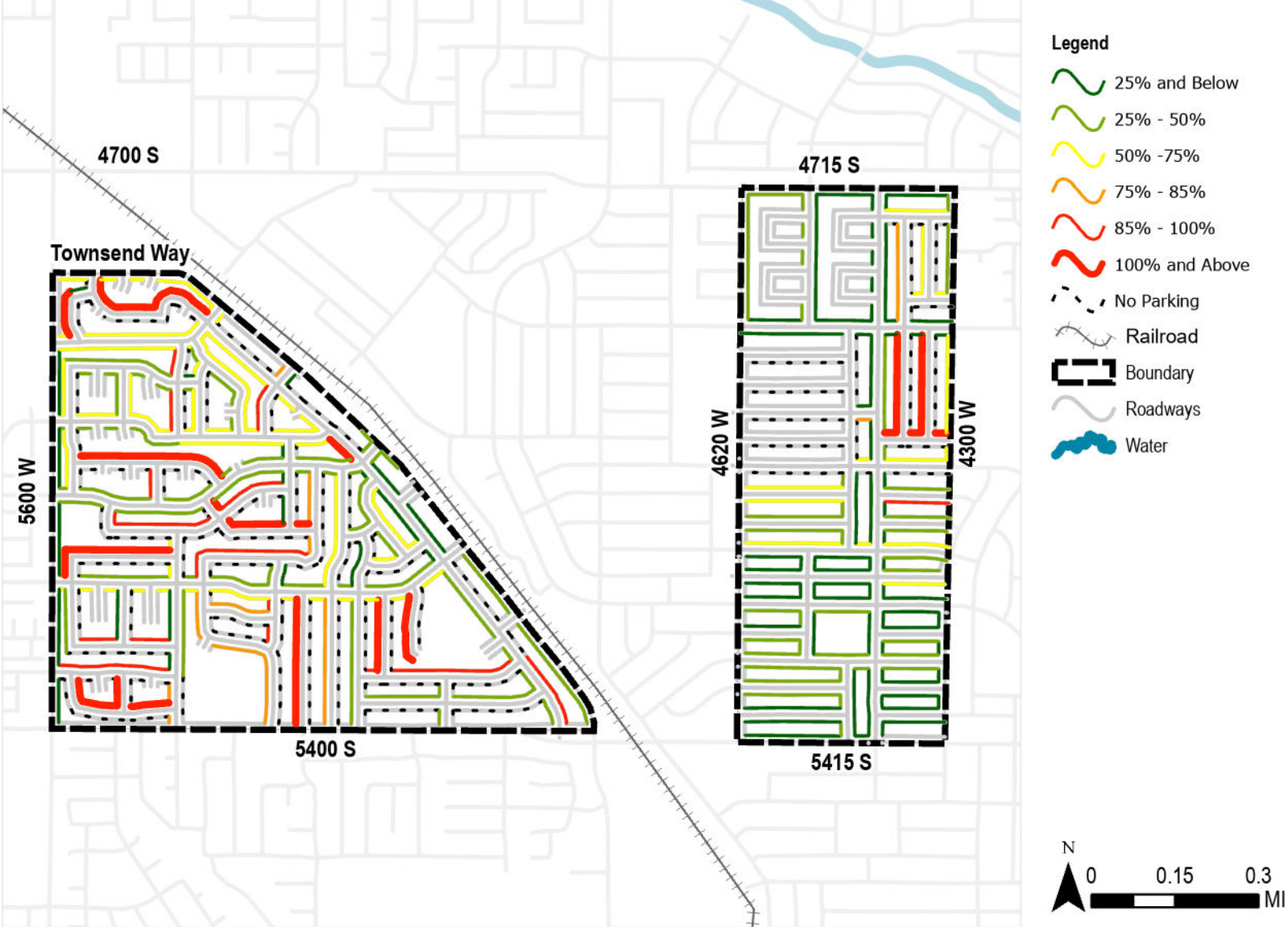
Weekend 8:00 AM - 10:00 AM



Weekend 12:00 PM - 2:00 PM



Weekend 5:00 PM - 7:00 PM



Weekend 9:00 PM - 11:00 PM



APPENDIX E

Additional Recommendations



Kearns Parking Study

Recommendations for active transportation, urban design, and land use

The following are active transportation, urban design, and land use-related recommendations for the Kearns Parking Study. These recommendations are intended to build on the parking, safety, and land use analysis work undertaken for the study, and complement the code, signage, educational, and ongoing monitoring recommendations – leveraging and linking them to broader community goals and opportunities.

The following range from simple and short-term actions to more involved long-term actions, and from proven-type solutions to more creative solutions. For the final plan, each of these areas could be explored further with graphics and additional detail.

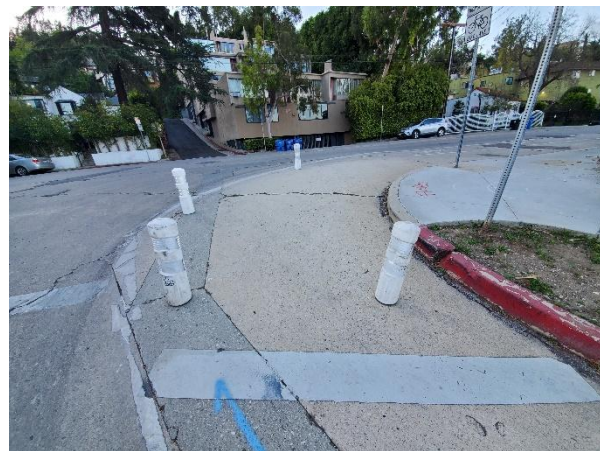
Corner treatments

The List of Recommendations identifies No Parking clear zones within 30 feet of an intersection, which will increase visibility of pedestrians (especially crossing pedestrians) by motorists and more generally of all users by motorists. We see the potential of urban design treatments to help enforce these restricted areas as well as to achieve other benefits such as creating safer, shorter crossings for pedestrians and slowing traffic.

Corner treatments could focus on extensions of the pedestrian realm within the 30-foot restricted area. These would probably best be undertaken with inexpensive materials such as paint/MMA and delineator posts, as in the examples below.

In addition, depending on the specific intersection, these treatments could also include marked crossings, a deficiency identified in the community outreach.

It may be worth finding a pilot intersection that the analysis has identified with a corner parking issue and high crash rates where this type of solution could be tested out.



Parking restrictions at bends

The analysis document identifies curves in the roads in Study Area 1 as a feature that exacerbates many of the visibility issues related to parked cars. On stretches of street with these curves, where the parking utilization analysis has shown low (below 50 percent) utilization, parking could be restricted at the curve on one side. In addition, where there is a curved corner, the length of the restriction could be extended.



Potential parking restrictions on bikeways

The Kearns Active Transportation Plan is in the process of identifying routes for bike facilities throughout the Kearns community. Many of the issues identified with parked cars throughout Kearns streets relate to conflicts with people bicycling or using other micro-mobility modes.

Identified bike routes through the study areas or other Kearns neighborhoods – whether shared roadways or designated lanes – could be priority streets for implementation of corner restrictions. And streets where parking is prohibited on one side of the street creates the opportunity for striping a bike line along the side of the roadway free from conflicts with street-parked vehicles. Where bike lanes will coexist with parallel-parked vehicles in a roadway, the bike lane should be offset from the parking location to mitigate potential car-door conflicts.

Town Center and parking

The Kearns community seeks to invest in its Town Center, the area around the intersection of 5400 South and 4000 West. The area's transformation is guided by the Kearns Town Center Master Plan. The Town Center has implications for on-street parking – and parking in general – for several reasons, and these can be leveraged into recommended strategies that seek to achieve the wider community on-street parking goals. These considerations are summarized below.

- **Shared parking resources:** The Town Center provides the opportunity for shared parking areas that can take pressure off surrounding neighborhood streets. Shared parking strategies seek to identify parking areas for uses that either have extra capacity or have peak usage times that complement peak parking needs of other uses.
- **On-street parking design:** There is the opportunity to design on-street parking in the Town Center in consideration of the Town Center goal to create a vibrant, walkable center. Parking can be designed to avoid visibility conflicts seen in other neighborhoods; to accommodate on-street parking in the context of the priority for a comprehensive pedestrian environment (such as using the parking lane for sidewalk extensions/bulb-outs first and parking where there is residual space); and ensuring that conflicts between parked/parking cars and people bicycling or using micromobility are minimized.
- **Opportunity for event transit:** Currently, the demand for parking at USANA Amphitheater can lead to event parking in Kearns neighborhoods. To help accommodate this demand, a shuttle from the Kearns Town Center to USANA Amphitheater could be considered. The shuttle could leverage the planned Kearns Town Center Mobility Hub, and/or a broader shuttle program to link other community destinations, such as the High School/Kearns-Oquirrh Park Fitness center area.

Parking on major streets and at destinations

Major streets and streets alongside community and neighborhood destinations present the opportunity for potential reconfiguration of the street or roadway to accommodate more parking or accommodate parking in ways that better achieve a broader set of community goals. These reconfigurations could create traffic calming improvements integrated with on-street parking, slowing traffic near schools, parks, and other destinations.

Examples include diagonal on-street parking alongside schools (perhaps even creating a chicane effect), parking pockets amid bulb-outs, and additional park parking on major streets currently without parking.

Pedestrian realm reconfiguration

The analysis identifies parking on sidewalk as a significant issue - of the observed periods, 25% to 35% of vehicles parked on the street were also interfering with the sidewalk. One of the recommendations is to replace the rolled gutters present on many streets with highback curb. As part of this replacement, on streets with no or a small park



strip, if it works with the overall engineering of the street, the Metro Township/MSD/Salt Lake County Engineering could seek to also add a sufficient park strip to further separate pedestrians from parked cars.

Heath Avenue focus

In the analysis, Heath Avenue was identified as a corridor with frequent crashes with parked vehicles. It also is a key corridor for the neighborhood, serves some neighborhood destinations, and has a variety of awkward/curved corner situations. It also – like much of the Study Area – has a relatively low parking occupancy rate, indicating there is curb with which to put to other uses and/or restrict parking.

It may be useful to develop an example urban design concept for Heath Avenue to study how many of these recommendations can integrate together and provide a sum to the community greater than their parts.